



SUSTAINABLE DEVELOPMENT – URBAN DESIGN & PLANNING

PLANNING & ZONING BOARD (PZB) SITE PLAN APPLICATION

Rev: 1 | Revision Date: 2/23/2017 | Print Date: 2/23/2017
I.D. Number: SPA

PLANNING & ZONING BOARD (PZB)

Site Plan Application

Cover: Deadline, Notes, and Fees
Page 1: Applicant Information Sheet
Page 2: Required Documentation / Submittal Checklist
Page 3: Sign Notification Requirements & Affidavit
Addendum: PZB Rezone with Flex Allocation <<if applicable>>
Addendum: Parking Reduction Information <<if applicable>>

DEADLINE: Submittals must be received by 4:00 PM each business day. Pursuant to Section 47-24.1(1), the Department will review all applications to determine completeness within five (5) business days. Applicants will be notified via email, if plans do not meet the submittal requirements and if changes are required.

NOTE: If your development site is separated by any public right-of-way (alley, alley reservation, or ROW easement) you must complete a separate application for each parcel.

NOTE: Optional 15-minute time slots are available during DRC meetings for scheduling to applicants, for general project inquiries or to obtain signatures on completed DRC plans (including Pre-Planning and Zoning Board, Pre- City Commission and Final DRC plans) from all representatives at one time in preference to scheduling individual appointments. Appointments are subject to availability. To make an appointment, please call 954-828-6531 latest by Friday at 12:00 noon prior to the meeting date.

FEES: All applications for development permits are established by the City Commission, as set forth by resolution and amended from time to time. In addition to the application fee, any additional costs incurred by the City including review by a consultant on behalf of the City, or special advertising costs shall be paid by the applicant. Any additional costs, which are unknown at the time of application, but are later incurred by the City, shall be paid by the applicant prior to the issuance of a development permit.

<input type="checkbox"/> Innovative Development (ID)	\$ 7,580.00
<input checked="" type="checkbox"/> Site Plan Level IV	\$ 2,730.00
<input type="checkbox"/> Site Plan Level III	\$ 2,110.00
<input type="checkbox"/> Change of Use Requiring PZB review	\$ 550.00
<input type="checkbox"/> Parking Reduction In addition to above site plan fee	\$ 750.00
<input type="checkbox"/> Site Plan Deferral	\$ 510.00
<input type="checkbox"/> Appeal of DRC Review	\$ 950.00

Page 1: PZB Site Plan - Applicant Information Sheet

INSTRUCTIONS: The following information is requested pursuant to the City's Unified Land Development Regulations (ULDR). The application must be filled out accurately and completely. Please print or type and answer all questions. Indicate N/A if does not apply.

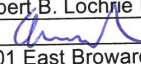
NOTE: To be filled out by Department

Case Number	
Date of complete submittal	

NOTE: For purpose of identification, the **PROPERTY OWNER** is the **APPLICANT**

Property Owner's Name	3001-18 Harbor Drive LLC
Property Owner's Signature	If a signed agent letter is provided, no signature is required on the application by the owner.
Address, City, State, Zip	1199 S. Federal Highway, Suite 384, Boca Raton, FL 33432
E-mail Address	A.Marier@consultationam.com
Phone Number	514-949-0206
Proof of Ownership	<input type="checkbox"/> Warranty Deed or <input checked="" type="checkbox"/> Tax Record

NOTE: If **AGENT** is to represent **OWNER**, notarized letter of consent is required

Applicant / Agent's Name	Robert B. Lochrie III, Esq. / Andrew J. Schein, Esq. Lochrie & Chakas, P.A.
Applicant / Agent's Signature	
Address, City, State, Zip	1401 East Broward Boulevard, Suite 303, Fort Lauderdale, FL 33301
E-mail Address	RLochrie@Lochrielaw.com
Phone Number	954-779-1101
Letter of Consent Submitted	Yes

Development / Project Name	Bahia Cabana Hotel
Development / Project Address	Existing: 3001 Harbor Drive New: Same
Legal Description	Lots 12 and 13, Ocean Harbor, Plat Book 26, Page 39 BCR
Tax ID Folio Numbers (For all parcels in development)	504212240090, 504212240100
Request / Description of Project	124-room hotel with a restaurant and 6 multifamily residential units
Applicable ULDR Sections	47-12 and 47-24.2
Total Estimated Cost of Project	\$ (Including land costs)

NOTE: Park impact fees are assessed and collected at time of permit per each new hotel room and dwelling unit type.

Estimated Park Impact Fee	\$ 15,750	Fee Calculator: http://ci.flaud.fl.us/building_services/park_impact_fee_calc.htm
Future Land Use Designation	CBRAC	
Proposed Land Use Designation	CBRAC	
Current Zoning Designation	SBMHA	
Proposed Zoning Designation	SBMHA	
Current Use of Property	Vacant	
Residential SF (and Type)	16,636 SF (multifamily)	
Number of Residential Units	6	
Non-Residential SF (and Type)	2,406 SF restaurant, 61,698 SF hotel, 8,804 SF hotel amenities	
Total Bldg. SF (include structured parking)	130,889 SF	
Site Adjacent to Waterway	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	

Dimensional Requirements	Required	Proposed
Lot Size (SF / Acreage)	None	42,646 SF / 0.978 acres
Lot Density	48 units/acre	6 units/acre
Lot Width	None	211'
Building Height (Feet / Levels)	120'	117' from grade
Structure Length	200'	180'
Floor Area Ratio	5	3.02
Lot Coverage	None	49%
Open Space	None	21,729 SF
Landscape Area	25%	10,952.5 SF (25.69%)
Parking Spaces	98	None (off-site parking agreement)

NOTE: State north, south, east or west for each yard.

Setbacks/Yards*	Required	Proposed
Front <input type="checkbox"/> S	1/2 height of building	1/2 height of building (see setback study)
Side <input type="checkbox"/> E	1/2 height of building	1/2 height of building (see setback study)
Side <input type="checkbox"/> W	1/2 height of building	1/2 height of building (see setback study)
Rear <input type="checkbox"/> N	1/2 height of building	1/2 height of building (see setback study)

Page 2: Required Documentation / Submittal Checklist

One (1) copy of the following documents:

- ☐ Original Pre-PZB signed-off set of plans and all supplemental documentation (ie. narratives, photos, etc.)
- ☐ Completed application (all pages must be filled out where applicable)
- ☐ One (1) electronic version of complete application and plans in PDF format

Two (2) original sets, signed and sealed, of Pre-PZB plans at 24" x 36"

Thirteen (13) copy sets, of Pre-PZB half-size scaled plans at 12" x 18"

- ☐ **Narrative** describing project specifics, to include but not be limited to: architectural style and important design elements, trash disposal system, security/gating system, hours of operation, dock facilities, etc. Narratives must be on letterhead, dated, and with author indicated.
- ☐ **Narrative** quoting all applicable sections of the ULDR, with point-by-point responses of how project complies with such criteria. Narratives must be on letterhead, dated, and with author indicated.
- ☐ **Land Use and Zoning maps** indicating all properties within 700 ft. of the subject property. These should be obtained from Urban Design & Planning Division. Site should be highlighted or clearly marked to identify the parcel(s) under consideration on all sets.
- ☐ **Cover sheet** on plan set to state project name and table of contents.
- ☐ **Current survey(s)** of property, signed and sealed, showing existing conditions; survey must be As-Built and Topographic with Right-of-Way and Easement Vacations Excluded. The survey should consist of the proposed project site alone. Do not include adjacent properties or portions of lands not included in the proposed project unless specifically requested by the City.
- ☐ Most **current recorded plat** including amendments, with site highlighted. This may be obtained from Broward County Public Records at 115 S. Andrews Ave. **Note: for Change of Use applications, this is not required.**
- ☐ **Aerial photo** indicating all properties within 700 ft. of the subject property. Must be clear and current with site highlighted.
- ☐ **Plans "A" thru "H".** **Note, for Change of Use applications, items asterisked (*) are only required if proposed changes affect these plans. Otherwise, these items should be obtained from Property Records if showing current conditions.**
 - A. Site Plan
 - B. Details*
 - C. Floor Plans
 - D. Building Elevations*
 - E. Additional Renderings*
 - F. Landscape Plans*
 - G. Photometric Diagram*
 - H. Engineering Plans*

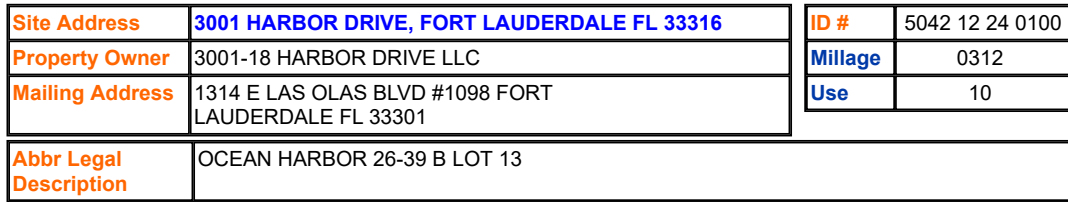
Note: All copy sets must be clear and legible. If original set is in color, copy sets must also be in color.

Note: Plans must be bound, stapled and folded to 8 1/2" x 11". All non-plan documents should be 8 1/2" x 11" and stapled or bound.

Note: Civil Engineering plans are only required at Final-DRC sign-off. Contact DRC Engineering Representative for details.

Note: For examples of project narratives, site plan data tables, and renderings required with your application, please refer to the "Submittal Reference Book" available at Urban Design & Planning.

Applicant's Affidavit	Staff Intake Review
I acknowledge that the Required Documentation and Technical Specifications of the application are met:	For Urban Design & Planning staff use only:
Print Name <u>Andrew Schein</u>	Date <u>9/11/2020</u>
Signature <u>[Signature]</u>	Received By <u>JH</u>
Date <u>9/11/20</u>	Tech. Specs Reviewed By <u>Jim Hetzel</u>
	Case No. <u>PLN-SITE-20040008</u>



* 2021 values are considered "working values" and are subject to change.						
Property Assessment Values						
Year	Land	Building / Improvement	Just / Market Value	Assessed / SOH Value	Tax	
2021	\$4,228,600		\$4,228,600	\$4,228,600		
2020	\$4,228,600		\$4,228,600	\$4,228,600		
2019	\$4,228,600		\$4,228,600	\$4,228,600	\$78,587.26	
2021 Exemptions and Taxable Values by Taxing Authority						
	County	School Board	Municipal	Independent		
Just Value	\$4,228,600	\$4,228,600	\$4,228,600	\$4,228,600		
Portability	0	0	0	0		
Assessed/SOH	\$4,228,600	\$4,228,600	\$4,228,600	\$4,228,600		
Homestead	0	0	0	0		
Add. Homestead	0	0	0	0		
Wid/Vet/Dis	0	0	0	0		
Senior	0	0	0	0		
Exempt Type	0	0	0	0		
Taxable	\$4,228,600	\$4,228,600	\$4,228,600	\$4,228,600		
Sales History				Land Calculations		
Date	Type	Price	Book/Page or CIN	Price	Factor	Type
7/30/2018	SW*-E	\$17,270,000	115241821	\$200.00	21,143	SF
8/17/2000	WD*	\$6,750,000	30825 / 194			
2/1/1973	WD	\$421,200	7940 / 667			
7/1/1969	WD	\$88,000				
				Adj. Bldg. S.F.		

* Denotes Multi-Parcel Sale (See Deed)

Special Assessments								
Fire	Garb	Light	Drain	Impr	Safe	Storm	Clean	Misc
03						F3		
L								
1						21143		



Site Address	3007 HARBOR DRIVE, FORT LAUDERDALE FL 33316	ID #	5042 12 24 0090
Property Owner	3001-18 HARBOR DRIVE LLC	Millage	0312
Mailing Address	101 NE THIRD AVE #1250 FORT LAUDERDALE FL 33301	Use	10
Abbr Legal Description	OCEAN HARBOR 26-39 B LOT 12		

The just values displayed below were set in compliance with **Sec. 193.011, Fla. Stat.**, and include a reduction for costs of sale and other adjustments required by **Sec. 193.011(8)**.

* 2021 values are considered "working values" and are subject to change.					
Property Assessment Values					
Year	Land	Building / Improvement	Just / Market Value	Assessed / SOH Value	Tax
2021	\$4,141,400		\$4,141,400	\$4,141,400	
2020	\$4,141,400		\$4,141,400	\$4,141,400	
2019	\$4,141,400		\$4,141,400	\$4,141,400	\$76,966.67
2021 Exemptions and Taxable Values by Taxing Authority					
	County	School Board	Municipal	Independent	
Just Value	\$4,141,400	\$4,141,400	\$4,141,400	\$4,141,400	\$4,141,400
Portability	0	0	0	0	0
Assessed/SOH	\$4,141,400	\$4,141,400	\$4,141,400	\$4,141,400	\$4,141,400
Homestead	0	0	0	0	0
Add. Homestead	0	0	0	0	0
Wid/Vet/Dis	0	0	0	0	0
Senior	0	0	0	0	0
Exempt Type	0	0	0	0	0
Taxable	\$4,141,400	\$4,141,400	\$4,141,400	\$4,141,400	\$4,141,400
Sales History				Land Calculations	
Date	Type	Price	Book/Page or CIN	Price	Factor
7/30/2018	SW*-E	\$17,270,000	115241821	\$200.00	20,707
8/17/2000	WD*	\$6,750,000	30825 / 194		
2/1/1973	WD	\$105,333	7940 / 667		
7/1/1970	WD	\$385,000			
2/1/1970	WD	\$70,000			
				Adj. Bldg. S.F.	

* Denotes Multi-Parcel Sale (See Deed)

Special Assessments								
Fire	Garb	Light	Drain	Impr	Safe	Storm	Clean	Misc
03						F3		
L								
1						20707		

Owner: 3001-18 HARBOR DRIVE LLC
Applicant: Adache Group Architects
Project: Bahia Cabana Hotel and Condominium
Prepared by: Adache Group Architects/JD

April 6, 2020

NARRATIVES – NEIGHBORHOOD COMPATIBILITY AND ADEQUACY

Sec. 47-25.3. Neighborhood compatibility requirements.

A. The neighborhood compatibility requirements are as follows:

1. *Adequacy requirements.* See Sec. 47-25.2.

Response: Applicant has provided a separate point-by-point narrative addressing the Adequacy Requirements.

2. *Smoke, odor, emissions of particulate matter and noise.*
 - a. Documentation from the Broward County Department of Natural Resource Protection (DNRP) or a report by a certified engineer, licensed in the State of Florida, that the proposed development will not exceed the maximum levels of smoke, odor, emissions of particulate matter and noise as regulated by Chapter 27, Pollution Control, of the Code of Broward County, and that a DNRP permit for such facility is not required.
 - b. Where a DNRP license is required in accordance with Chapter 27, Pollution Control, of the Code of Broward County, all supporting documentation and information to obtain such permit shall be submitted to the DRC as part of a site plan review.
 - c. Such DNRP licenses shall be required to be issued and copies provided to the city prior to the issuance of a building permit for the proposed development.

Response: Response: Applicant will apply and obtain all Broward County EPMGD (formerly DNRP) permits.

3. *Design and performance standards.*
 - a. *Lighting.* No lighting shall be directed from a use which is subject to the requirements of this Sec. 47-25.3 in a manner which illuminates abutting residential property and no source of incandescent or mercury vapor illumination shall be directly visible from any abutting residential property. No neon lights inside or outside structures shall be visible from any abutting residential property.
 - i. Glare. Any nonresidential operation or activity producing glare shall be conducted so that direct or indirect illumination of light shall not cause illumination in excess of one (1) foot candle on any abutting residential property except as provided in subsection iii. of this subsection a.

Response: The lighting will be designed by a licensed engineer in conformance with the above sections. Any glare produced by the Project will be conducted so that direct or indirect illumination of light will not cause illumination in excess of (1) foot-candle on any adjacent residential properties. Moreover, the Project incorporates lush landscaping to aide in the reduction of glare. All parking, garage and landscape lighting are internal to the site.

- ii. Control of effects of lights from automobiles or other sources. Where the site plan indicates potential adverse effects of parking or of other sources on the lot on which the nonresidential use is to be located, such effects shall be eliminated or at a minimum prevented so that lights do not illuminate adjacent residential property below a height of five (5) feet at the residential lot line, or from shining into any residential window if there is to be nonresidential parking on the premises after dark.

Response: The majority of the parking provided for the Project will be located in above ground parking and will not be visible. The parking garage will be located across Harbor Drive at the Bahia Cabana Garage and Apartments (see separate Narrative for this project under same ownership). On site the ground floor is screened with retail at and architectural treatments so as to eliminate imposing views of parked cars and headlights from adjacent streets and neighboring properties. There will be no on street parking, only drop off lanes for valet service and arrival. Additionally, the Project incorporates lush landscaping to diffuse any light exuding from resident automobile headlights onto adjacent streets and properties.

- iii. In addition to the above, parking lots and garages will be subject to the provisions of Sections 47-20.14 and if in conflict with the provisions of this section, the more restrictive provisions shall apply.

Response: The parking garages have been designed to comply with all applicable ULDR requirements.

- b. *Control of appearance.* The following design standards are provided to protect the character of abutting residential areas from the visual impact which may result from a use which is subject to the requirements of this Sec. 47-25.3.
 - i. *Architectural features.* The facade of any side of a nonresidential building facing the residential property shall be constructed to compliment a residential structure and shall include the following:

Response: The fenestration of the building complements and enhances a new organic architectural vernacular and encourages a new timeless design language. The project will utilize balconies, terraces, color and free form material banding. The facades of the building incorporate different design elements such as: large windows, recessed entries, green walls, louvers along the façade, glass railings and stucco finish which are consistent throughout the proposed development.

- a) Fenestration such as windows, doors and openings in the building wall; and

Response: The facades throughout the project have been designed to include windows, balconies and other architectural detailing. On the ground floor, entrances, accesses, and openings to the sidewalk reflect a pedestrian scale. See Architecture plans for detail.

- b) Shall contain a minimum of one (1) feature from each of the following architectural feature groups with a total of four (4) architectural features from the following list:
 - 1. Detail and embellishments:
 - a. Balconies,

Response: There are covered balconies or terraces utilized throughout the project. See Architecture plans for detail.

- b. Color and material banding,

Response: Color and banding is achieved through balcony railings comprised of lightly tinted glass and/ or aluminum to extend the maritime feel of Bahia Cabana.

- c. Decorative metal grates over windows,

Response: There are no metal grates over the windows of the new buildings. See Architecture plans detail.

- d. Uniform cornice heights,

Response: Cornice heights are parallel at varying heights and supplemented with organic free form stucco elements creating a variable treatment on the facades and creating a dynamic visual break on the exteriors.

- e. Awnings.

Response: Awnings are provided with overhangs and cantilevers at the ground level to shield pedestrians and encourage interaction with the amenities. See Architecture plans for detail.

- 2. Form and mass:
 - a. Building mass changes including projection and recession,

Response: Building mass varies greatly throughout the project beginning with a non-rectangular footprint and multiple step backs at various heights. Projections and recesses augment the diversity of the massing along with rounded corners throughout the building.

- b. Multiple types and angles of roofline, or any combination thereof.

Response: The roof lines throughout the project contain varied lines of banding with smooth white stucco that provide visual interest. The roofs of the buildings feature angled banding, a sloped rooftop element and architectural detailing to complement the buildings' contemporary feel further enhanced by eyebrows and canopies creating a signature look to the building, while also screening any mechanical equipment from the public view. See Architecture plans for detail.

- c) The above required facade treatment shall be required to continue around the corner onto the adjoining wall for a distance of twenty (20) feet.

Response: Façade treatments on both buildings continue around the entire building envelope.

- ii. *Loading facilities.* Loading and service facilities shall be screened so as not to be visible from abutting residential uses or vacant residential zoned property.

Response: Trash and loading facilities have also been incorporated within the building footprints and screened from Harbor Drive, A1A and all adjoining property.

- iii. *Screening of rooftop mechanical equipment.* All rooftop mechanical equipment, stair and elevator towers shall be designed as an integral part of the building volume and/or adequately screened so that they are not visible from abutting residential uses or vacant residential zoned property.

Response: All mechanical equipment will be properly screened.

- c. *Setback regulations.* When a nonresidential use which is subject to the requirements of this Sec. 47-25.3 is contiguous to any residential property, there shall be an additional setback required for any yard of that use which is contiguous to the residential property, as follows:

- i. When any side of a structure greater in height than forty (40) feet is contiguous to residential property, that portion of the structure shall be set back one (1) foot for each one (1) foot of building height over forty (40) feet up to a maximum width equal to one-half (1/2) the height of the

building, in addition to the required setback, as provided in the district in which the proposed nonresidential use is located.

Response: The property is only contiguous to residential property on the west and is stepped back accordingly.

d. *Bufferyard requirements.* When a use which is subject to the requirements of this Sec. 47-25.3 is contiguous to any residential property, the property where the use is located shall be required to have a landscaped strip area and a physical barrier between it and the residential property. Such landscape strip shall meet the following requirements:

- i. *Landscape strip requirements.* A ten (10) foot landscape strip shall be required to be located along all property lines which are adjacent to residential property. Such landscape strip shall include trees, shrubs and ground cover as provided in the landscape provisions of [Section 47-21](#), Landscape and Tree Preservation Requirements. The width of the landscape area shall extend to the property line. All required landscaping shall be protected from vehicular encroachment. When walls are required on nonresidential property abutting an alley, required shrubbery shall be installed and located within the landscape area on the exterior of the wall.

Response: The hotel property is contiguous to residential property only on the west. Ten feet of landscaping is provided on all sides except for the waterfront dock area and interrupted only by the entry drop off and loading access.

- ii. *Parking restrictions.* No parking shall be located within twelve (12) feet of the property line, within the yard area required by the district in which the proposed nonresidential use is located, when such yard is contiguous to residential property.

Response: The project does not have any parking within twelve (12) feet of the property line.

- ii. *Dumpster regulations.* All solid waste refuse containers (dumpsters) shall be set back a minimum of twelve (12) feet from any property line which is contiguous to residential property, and shall be screened in accordance with the Dumpster requirements, as provided in [Section 47-19](#), Accessory Uses, Buildings and Structures.

Response: The project does not have any refuse containers within twelve (12) feet of the property line.

- iv. *Wall requirements.* A wall shall be required on the nonresidential property, a minimum of five (5) feet in height, constructed in accordance with [Section 47-19.5](#) and subject to the following:
- a) Decorative features shall be incorporated on the residential side of such wall according to the requirements of [Section 47-19.5](#)
 - b) Shall be located within, and along the length of the property line which abuts the residential property,
 - c) When the nonresidential property is located adjacent to an alley such wall shall be located at least five (5) feet from the right-of-way line located closest to the nonresidential property,
 - d) When a utility, or other public purpose easement, on the nonresidential property precludes the construction of a wall, then an opaque fence, constructed in accordance with the standards described in [Section 47-19.5](#), may be erected in lieu of the wall required by subsection iv. above. The use of an opaque fence as a physical barrier between nonresidential and residential property shall be reviewed and recommended by the city engineer.

Response: The hotel property is contiguous to residential property only on the west. The hotel property will have cast concrete walls on the west where it is contiguous to residential and on the east where it faces the local fire station, on the property lines.

- v. *Application to existing uses.* Within five (5) years(remainder of this subsection v. is intentionally omitted).

Response: N/A

- e. *Neighborhood compatibility and preservation.* In addition to the review requirements provided in subsections A.1, A.2 and A.3.a, b, c, and d, the following review criteria shall also apply as provided below:
- i. All developments subject to this Sec. 47-25.3 shall comply with the following:
 - a) Development will be compatible with, and preserve the character and integrity of adjacent neighborhoods, the development shall include improvements or modifications either on-site or within the public rights-of-way to mitigate adverse impacts, such as traffic, noise, odors, shadow, scale, visual nuisances, or other similar adverse effects to adjacent neighborhoods. These improvements or modifications may include, but shall not be limited to, the placement or orientation of buildings and entryways, parking areas, buffer yards, alteration of building mass, and the addition of landscaping, walls, or both, to ameliorate such impacts.

Roadway adjustments, traffic control devices or mechanisms, and access restrictions may be required to control traffic flow or divert traffic as needed to reduce or eliminate development generated traffic on neighborhood streets.

Response: The redevelopment of this aging and underutilized hotel property on a spectacular Intracoastal location is compatible with, preserves, and enhances the character and integrity of adjacent neighborhoods by providing, in addition to the new, upscale lodging facilities, extensive publicly accessible pedestrian amenities. Its residential component also updates the residential neighborhood with desirable upgrades including modern condominiums, concealed off street parking, and wide sidewalks on both sides of the gateway area of the neighborhood to enhance the pedestrian experience and improve the connection to the beach on Harbor Drive.

The uniqueness of the project is derived by eliminating as much as possible, the parking on the hotel property, and placing the majority of vehicular uses concealed across Harbor Drive, thereby providing more publicly accessible open space on the intracoastal and allowing the project to reduce the building footprints in order to maximize the public open space. Specifically, this approach has allowed for a waterfront park with lushly landscaped areas and anchored by a restaurant and cafe, to function as a community-oriented open space linking directly into the public realm. These new site features encourage a balance of activity to promote a sense of community and safety.

The Bahia Cabana's ground level experience has been configured in such a way to maximize pedestrian and vehicular access to the site by utilizing multiple access points to Harbor Drive and essentially opening the site with pedestrian friendly outdoor areas and active ground floor uses. This configuration provides a balanced approach to parking which is supported by a multimodal traffic approach through bicycle parking and full valet parking service.

Bahia Cabana is envisioned to be a modern resort hotel and condominium that reflects the surrounding waterfront fabric and lifestyle of Fort Lauderdale. The residential mixed-use nature of the project is in context with the surrounding residential and commercial uses throughout the Central Beach area.

- b) Consideration shall be given to the recommendations of the adopted neighborhood master plan in which the proposed development is to be located, or which it abuts, although such neighborhood master plan shall not be considered to have the force and effect of law. When recommended improvements for the mitigation of impacts to any neighborhood, conflicts with any applicable ULDR provision, then the provisions of the ULDR shall prevail. In order to ensure that a development will be compatible with, and preserve the character and integrity of adjacent

neighborhoods, the development shall include improvements or modifications either on-site or within the public rights-of-way to mitigate adverse impacts, such as traffic, noise, odors, shadow, scale, visual nuisances, or other similar adverse effects to adjacent neighborhoods. These improvements or modifications may include, but shall not be limited to, the placement or orientation of buildings and entryways, parking areas, buffer yards, alteration of building mass, and the addition of landscaping, walls, or both, to ameliorate such impacts. Roadway adjustments, traffic control devices or mechanisms, and access restrictions may be required to control traffic flow or divert traffic as needed to reduce or eliminate development generated traffic on neighborhood streets.

Response: The project has been designed in accordance with the applicable criteria.

- iii.* All development within the RAC-TMU (RAC-EMU, RAC-SMU and RAC-WMU) district that is greater in density than twenty-five (25) dwelling units per net acre:

Response: N/A. Project is not located in RAC-TMU.

- a) In addition to meeting the review requirements of subsection A.3.e.i, building sites within the RAC-TMU (RAC-EMU, RAC-SMU and RAC-WMU) district shall be eligible to apply for additional dwelling units over and above twenty-five (25) dwelling units per net acre, provided such additional dwelling units are available for distribution in the downtown regional activity center. However, in order to obtain such additional dwelling units, a site plan level II permit must be approved. Such approval shall be based upon consideration of the number of additional dwelling units available under the city land use plan, the number of additional dwelling units requested, the impact of the proposed development on abutting residential areas, the proposed residential density of the proposed development, location of the proposed development, the sensitivity to adjacent development of the site design and proposed orientation of the proposed development (including proposed setbacks), pedestrian movements associated with the proposed development, proposed landscaping, and traffic and parking impacts of the proposed development on the transportation network. Approval for allocations of any additional dwelling units, hotel rooms or both, for multifamily dwellings, hotels and mixed-use developments shall conform to the city's land use plan and may be granted subject to approval of a site plan level II permit, subject to the considerations for such review as prescribed above. A minimum setback of twenty (20) feet from all property lines for every building used exclusively for residential purposes may be required. Such minimum setback may also be required for mixed use buildings in which residential

use exceeds fifty-nine percent (59%) of the total floor area, exclusive of parking garages.

Response: N/A. Project is not located in RAC-TMU.

iii. All development within any downtown RAC district that is within one hundred (100) feet of residential property that is located outside of any downtown RAC district and all development within the RAC-TMU (RAC-EMU, RAC-SMU and RAC-WMU) district; and all development that is located on land adjacent to the New River within the RAC-AS and RAC-CC which deviates from the New River corridor requirements as provided in [Section 47-13](#), Downtown Regional Activity Center:

a) In addition to meeting the review requirements of subsection A.3.e.i, the setbacks imposed for a development plan may be modified subject to the requirements provided as follows:

1. No structure, or part thereof, shall be erected or used, or land or water used, or any change of use consummated, nor shall any building permit or certificate of occupancy be issued therefor, unless a development plan for such structure or use shall have been reviewed and approved, where applicable, after development review as prescribed in subsection A.3.e.i. In approving such development plan, consideration shall be given to the location, size, height, design, character and ground floor utilization of any structure or use, including appurtenances; access and circulation for vehicles and pedestrians, streets, open spaces, relationship to adjacent property, proximity to New River and other factors conducive to development and preservation of a high quality downtown regional activity center district. No approval shall be given to the setbacks shown on the development plan unless a determination is made that the setbacks conform to all applicable provisions of the ULDR, including the requirements of [Section 47-13](#), Downtown Regional Activity Center Districts, that the safety and convenience of the public are properly provided for and that adequate protection and separation are provided for contiguous property and other property in the vicinity. Approval of the setbacks of a development plan may be conditioned by imposing one (1) or more setback requirements exceeding the minimum requirements.

Response: N/A. Project is not located within the Downtown RAC.

iv. All development that is located on land within the CBA zoning districts;

AND

All development that is zoned RMM-25, RMH-25 and RMH-60 east of the Intracoastal Waterway;

AND

All nonresidential development lying east of the Intracoastal Waterway.

- a) In addition to meeting the other applicable review requirements of this subsection 3., it shall be determined if a development meets the Design and Community Compatibility Criteria.

The purpose of the Community Compatibility Criteria is to define objectives for private sector development which either abuts or is readily visible from public corridors. The relationship between private and public sector development must be carefully planned to avoid negative impacts of one upon the other. The city's intent in implementing these objectives is to:

- i. Protect the investment of public funds in public corridor improvements.
- ii. Improve the visual and functional quality of both public and private development by coordinating the transition between these areas.
- iii. The ultimate goal of these objectives is to integrate buildings, vehicular circulation, pedestrian circulation, open space and site elements into a unique, pedestrian sensitive environment which stimulates revitalization.

The Community Compatibility Criteria that are required to be met are as follows:

- b) *Bulk Controls:*

Density:

Building density should be consistent with the proposed use, adjacent development, and as required under the Central Beach Area, RMM-25, RMH-25 and RMH-60 zoning districts.

Response: The hotel's SBMHA zoning provides for density of 48 units per acre. At 6 units per acre the proposed project is lower than the SBMHA development regulations for the Central Beach area, and adjacent development.

Floor Area Ratio:

Building floor area ratio (F.A.R.) should be consistent with the proposed use, and as required under the Central Beach Area.

Response: The Hotel F.A.R. is 3, and is within the maximum allowed of 5.

Maximum Height:

Building height should be consistent with the proposed use, adjacent development, and as required under the Central Beach Area, and RMM-25, RMH-25 and RMH-60 zoning districts. No portion of a structure in excess of thirty-five (35) feet in height shall exceed the prescribed Beach Shadow Ordinance setback.

Response: The building height meets all requirements of the SBMHA zoning district applicable to the Property.

Yards:

Building yards should be consistent with the proposed use, adjacent development, and as required under the Central Beach Area, RMM-25, RMH-25 and RMH-60 zoning districts. Building yards are in addition to any easements or reserve right-of-way which may be required by the city, county or state. Portions of a structure, up to thirty-five (35) feet in height, may encroach within the A1A setback if the building's street level use is predominantly pedestrian active (pedestrian-oriented retail, sidewalk cafes, etc.). No portion of any structure is permitted to extend, however, into the future right-of-way.

In the PRD, ABA and SBMHA zoning districts, to ensure continuity of the ocean front streetscape "edge" a minimum seventy-five (75%) percent of the northbound A1A frontage must be built to the setback line (or approved encroachment limit). In the Planned Resort Development (PRD) district the entire northbound A1A frontage should be built to the future right-of-way line unless otherwise approved under that district's community redevelopment plan.

Response: Bahia Cabana's building is set back consistent with the guidelines established for important pedestrian corridors in the SBMHA and RMH-60 zonings as set forth in this subsection. The applicant is not requesting any encroachments for the front yard. The building includes pedestrian friendly street level uses along Harbor Drive.

c) Massing Guidelines:

Overall Height:

Buildings should be encouraged to vary in overall height and not be contained in a single volume of continuous height.

Response: The building has been designed in such a manner to provide variety in massing, height and architectural treatments to create a positive ground level experience throughout the sites.

Vertical Plane Moderation:

Buildings exceeding thirty-five (35) feet in height should be encouraged to maintain no more than three (3) stories without horizontal moderation in vertical surface plane. This moderation should consist of a minimum four feet horizontal variation in surface plane such as brise soleil, balconies, building projections, etc. Repetitive moderations should be discouraged.

Response: All of the building includes horizontal modulation and articulating architecture in the vertical surface plane. The design includes balconies to break up the horizontal elevations.

Cornice Height:

All buildings should be encouraged to display a uniform cornice height of a maximum of thirty-five (35) feet in height. This cornice height should consist of a uniform alteration to the building massing for a minimum of twenty (20) feet perpendicular to the vertical surface.

Response: The building utilizes colonnades and overhangs between the heights of 15' to 35' to unify the project while providing shade and inviting pedestrian movement at the street, while unifying the development with a consistent architectural language.

Facade Treatment:

The first thirty-five (35) feet of exterior facade vertical plane should be encouraged to enhance the pedestrian environment by incorporating appropriate architectural features. Such features include cornice detailing, belt courses, corbelling, molding, stringcourses, ornamentation, changes in material or color, and other sculpting of the architectural surface which add special interest and are compatible with public sector site elements.

Response: The exterior facade treatment of the lower floors enhances the pedestrian environment through the use of building overhangs, providing protection for pedestrians. In addition, the facades of the buildings each provide strong architectural detail at the pedestrian level. See Architectural plans for details.

Overstreet Connections:

Connections between structures which pass over public right-of-way may be permitted providing those connections have secured legitimate air rights over the public corridor and meet all applicable codes. Connections over A1A to the beach should be limited to select locations where significant amounts of above grade pedestrian traffic will be generated. Where possible, over street connections should access the promenade/beach at or near

major beach portal features. All over street connections should be of exceptional design, which enhances the visual and functional quality of the streetscape and should be compatible with public sector site elements.

Response: N/A There are no over street connections.

d) *Street Level Guidelines:*

Active Use:

The first floor of all buildings, including structured parking, should be designed to encourage pedestrian scale activity. To stimulate pedestrian activity, buildings which front on A1A northbound should devote a majority of their first-floor area to retail activities such as restaurants, shops, galleries and similar active uses. Street level retail uses should have direct access to the adjoining public sector sidewalk in addition to any other access which may be provided.

Structured parking facilities should be designed with street level frontages consisting of either occupied retail space or an architecturally articulated facade which screens the parking area of the structure. Street level openings to parking structures should occur only on sidestreets and be minimized to accommodate necessary vehicle entrances and pedestrian access only.

Buildings which provide pedestrian active retail uses along a majority of their A1A northbound street level frontage may be permitted to exceed setbacks established under the Central Beach Area Zoning Districts (Section [47-12](#)). In addition, street level retail and restaurant uses may be permitted to use a portion of the public sector sidewalk for sidewalk displays and/or outdoor dining areas. Private use of public sector sidewalks must be temporary only and subject to all applicable codes and lease arrangements. All displays, furnishings and other elements associated with these active street level uses should be designed and maintained to enhance the visual and functional quality of the streetscape and should be compatible with public sector site elements.

Response: Bahia Cabana provides many active uses at ground level on Harbor Drive and throughout the Property. One of the key focus points of the development has been to activate the frontage along Harbor Drive and invite access on to the site through a viable mix of active ground floor uses throughout the site. These uses are designed to interact with the pedestrian experience created by wide, shaded walkways, dining opportunities, and connections to the other amenities on the Property. This continues throughout the property including development of the waterfront at the northern end of the Property, further activating the frontage along the Intracoastal.

This approach endeavors to enhance the pedestrian experience of Bahia Cabana and is enhanced by a ‘tropical modern’ landscape, decorative paving, and appropriate seating at the pedestrian level. The open areas also provide access to the marina waterfront; the

waterfront open park space and the Intracoastal restaurants provide further activation of the ground level of the site.

Fenestration:

To complement pedestrian scale activity on A1A and all People Streets, a majority of the first floor facade on these frontages should be windows, doors or other transparent architectural features. Expanses of solid wall should be minimized. Reflective surfaces on windows or doors should be discouraged. Street level windows and doors should be recessed or receive special design detailing which distinguish them from the building shaft and add variety to the streetscape.

Response: Bahia Cabana's street level facades are composed of transparent tinted glass and warm materials to encourage indoor-outdoor visibility, pedestrian movement and activity. Entry ways and protected areas near the entry way are covered by deep overhangs and dramatic cantilevers to further encouraging gathering places. Expanses of solid walls are minimized by landscaping or textured materials in order to add visual interest at the street-level. See Architecture plans for details.

Arcades/Canopies:

Buildings which border directly on A1A northbound or Las Olas Boulevard within the Planned Resort Development (PRD) district should incorporate an arcade or continuous architectural canopy along these frontages, unless otherwise approved under a community redevelopment plan. Buildings in other districts should be encouraged to incorporate an arcade or continuous canopy along their A1A northbound frontage providing the feature is consistent with the proposed use, adjacent development and meets all applicable codes. Arcades or continuous canopies should be a minimum of ten feet wide and maintain acceptable minimum clear height. Arcades and canopies should be designed as a fixed non-retractable element integral to the building's architectural mass and compatible with public sector site elements.

Non-continuous canopies, awnings and marquees should also be provided over street level window treatments and building entrances. Such features may be constructed of either rigid or flexible material but should complement the visual and functional quality of the streetscape and be compatible with public sector site elements. No arcade, canopy, awning or marquee should extend into the future public right-of-way nor interfere with street light fixtures or the growth and maintenance of street trees.

Response: N/A. The Project is not within the PRD district.

Trash/Loading Facilities:

All building facilities for loading, trash and service should be incorporated within building volume and screened so as not to be visible from the street and pedestrian circulation areas.

Trash/loading facilities should be discouraged on A1A and People Street frontages. Where buildings are of inadequate volume to accommodate these facilities, trash/loading facilities should be architecturally treated as part of the building mass and screened by solid walls, fences, planting or architectural devices which are compatible with public sector site elements. Trash/loading facilities must be of sufficient size and design to accommodate access by large vehicles.

Response: All trash and loading is interior and shielded from public view within the building.

e) *Other Guidelines:*

Energy Conservation:

Buildings should be oriented to take advantage of southeasterly breezes for summer cooling and interrupt occasionally strong northeasterly winds. Exterior glass surfaces should be shaded to improve energy efficiently. Roof and exterior wall finishes should be light in color to encourage maximum reflection/minimum transmission of heat loadings.

Response: Bahia Cabana's building features large balconies in order to take advantage of southeasterly breezes and water views. Occupied roof areas will have extensive landscaping and shade features, while non-occupied roof areas will be topped with a light-colored surface to help mitigate heat gains. The proposed building's facades also consist of light colors with light accents, and high-performance glazing to improve energy efficiency.

Building Separation:

Buildings should allow adequate space between structural masses for the passage of natural breezes. New building masses should be sited to the extent feasible so they maintain reasonable views to the ocean and Intracoastal Waterway from existing structures.

Response: N/A. The development is a single building mass.

Rooftop Design:

Where possible, rooftops should be designed to accommodate various forms of human activity such as sun decks, tennis courts, outdoor cafes, etc. Roof surfaces not allocated to human activity should be finished with a surface material that does not effect the quality of views from surrounding buildings.

All rooftop mechanical equipment, stair and elevator towers should be designed as an integral part of the building volume and/or adequately screened.

Response: The hotel incorporates roof level recreational areas and amenities to facilitate human activity. All rooftop mechanical equipment is adequately screened, by landscape and/or louvers.

f) *Vehicular Circulation:*

Ingress/Egress:

For the CBA zoning districts, access drives to individual parcels should be limited to those necessary for the adequate function of the use contained therein. Direct vehicular access from A1A northbound should be discouraged unless otherwise approved under the Planned Resort Development (PRD) district community redevelopment plan. Direct vehicular access from A1A southbound should be limited to minimize traffic impacts on the state roadway. Direct vehicular access from side streets should be encouraged. Smaller parcels should be encouraged to share common access with adjacent parcels keeping curb cuts to a minimum.

Response: The buildings has only one drop off and one loading access. Sidewalks and pedestrian entries are adequately separated from vehicular traffic.

Arrival/Drop-off Areas:

Major arrival/drop-off areas should only be encouraged along side streets, especially those designated as People Streets. Arrival/drop-off areas should be encouraged to provide sufficient room for vehicle stacking, loading, unloading, and other main entrance functions. Pedestrian entries for all residential, hotel and commercial structures should be located the maximum possible distance from loading and service areas.

Response: All entries and arrivals are on Harbor Drive, a side street off A1A. Pedestrian entries are as far as possible from loading and service areas.

Other:

Individual parcels should be encouraged to accommodate transit stops for the county bus service, the proposed water taxi and other transit systems. Fire access lanes and other emergency vehicular accessways may be designated by the appropriate public agency. Uses that require service by large vehicles should be designed to allow large vehicle access without blockage of adjoining vehicular or pedestrian circulation.

Response: Drop off areas could handle trolley stops if added and there is dockage on the Intracoastal available if needed for a water taxi stop as well. Service vehicle access will not block pedestrian movement.

g) *Pedestrian Circulation:*

Urban Open Spaces/Plazas:

Open spaces for public congregation and recreation should be encouraged to the extent that these spaces do not substantially interrupt the streetscape edge at the building line. Open spaces should be permitted both within and behind building yards in proportion to the bulk of the adjacent building. The streetscape edge should be maintained by architectural features (arcades) site furnishings (flagpoles, light standards) for landscape elements (palms, etc.) which provide continuity between the building line of adjoining structures.

All urban open spaces should be accessible and visible from the adjoining public sector corridor while providing for the safety and security of patrons. Severe elevation change and walls should be discouraged between the adjoining public corridor and the open space. Entryways and steps to these open spaces should be kept wide and welcoming in character. All urban open space must be kept handicap accessible.

The following amenities should be encouraged within urban open spaces: ornamental fountains, waterfalls, sculpture, trellises, arbors, seating facilities, landscape features, etc. Design features of these open spaces should serve to enhance the visual and functional quality of the adjoining corridor and be compatible with public sector site elements.

Response: Accessible open space located throughout the project is integrated into the surrounding public realm through the project's perimeter, pedestrian plazas and a public sidewalk connection to the nearby beach. As noted herein, the property has been configured in such a manner to maximize this open space supported by active ground level uses.

Currently, the views on the Property are limited given the crown of the A1A road and the height of the boats docked at the marina. This park experience will better afford the public the opportunity to view the site's marina and Intracoastal Waterway. These open spaces will also provide several design features, shaded walkways, benches, landscape art forms, ornamental planting beds and trees with landscape lighting along the internal walkways and plazas.

Pedestrian Corridors:

Private sector pedestrian corridors, which supplement public sector pedestrian facilities and improve access to the beach and/or Intracoastal Waterway should be encouraged. These corridors should be of a width and design which encourages pedestrian use and whenever possible allows for emergency vehicle access. The corridors may pass through open air or enclosed portions of surrounding buildings providing the pedestrian experience is largely uninterrupted.

Response: The neighborhood serviced by Harbor Drive is largely flanked on both sides by on street perpendicular parking which is detrimental to safe pedestrian circulation. By providing wide, shaded sidewalk circulation on either side of Harbor Drive at the gateway to this neighborhood, it adds an important component to the safe and enhanced pedestrian circulation at the critical intersection with A1A, and access to and from the beach on cross

walks and other walkways along A1A. These sidewalks are also a component that will draw in the public to the properties' amenities and provide access to the Intracoastal Waterway.

Parking:

Parking should be provided consistent with the proposed use, adjacent development and as required under [Section 47-20](#). Access drives to parking should be limited to those necessary for the function of the facility and comply with vehicular ingress/egress guidelines outlined herein. Direct backout or "head-in" parking should be expressly prohibited.

Parking facilities should be located in close proximity to the building they serve with direct pedestrian access from parking to building which does not impact public pedestrian facilities. Vehicular circulation within parking areas should remain internal to the parking facility and public roads should not be utilized as part of the parking circulation system. Structure parking should be encouraged subject to the street level building guidelines outlined herein. Covered parking should also be encouraged providing the overhead structures are compatible with adjoining architecture/ site elements and comply with the building rooftop design guidelines outlined herein. Grade level parking must be adequately screened so parked cars are not visible from adjoining public corridors, and landscaped to moderate views from surrounding buildings.

Parking perimeters may incorporate walls, fencing, mounds and/or landscape treatments to meet the screening requirement providing these elements are compatible with adjoining public sector site elements and allow safe and secure use of parking facilities. Trash, storage and mechanical equipment located within parking facilities should also comply with the screening requirements outlined herein.

Response: Parking will be provided consistent with section 47-20. All parking will be housed in an internal parking garage away from the waterfront and full-service valet will facilitate easy arrival and access to the project's amenities and marina.

h) *Perimeter Treatments:*

Screening:

All exterior trash, loading and equipment storage facilities should be screened so as not to be visible from adjoining public corridors and landscaped to moderate views from surrounding buildings. Mechanical equipment including all handling units, exhaust outlets, transformer boxes, electric switching units, etc. should be appropriately screened by planting and/or low walls wherever it cannot be concealed within the building volume.

Grade level parking lots should be appropriately screened from adjacent pedestrian areas with walls, fencing and/or planting. Shrubs surrounding ground level parking lots should be of sufficient height to hide automobile grill work. Landscape material used to meet the

above requirements should provide 100% screening within one growing season, and must be provided with an automatic irrigation system.

Any lot that becomes vacant through removal of a structure should be screened from the abutting public corridor. Vacant lot screening should utilize the elements described above and additional treatments as necessary to protect the visual and functional quality of the adjoining public corridor. Screening design, materials and maintenance should be compatible with public sector site elements.

Response: All trash, loading, equipment storage and mechanical equipment will be located within enclosed structures on both properties, and will thus be adequately screened from view.

Paving:

Public sector paving should be as shown on the approved Beach Revitalization streetscape plans or as specified in other sections of these guidelines. Paving systems used on private plazas and walkways should be compatible in pattern and scale to public sector paving. Private paving systems which immediately abut and are readily visible from adjoining public corridors should reflect the same color, material and texture as the public sector paving and provide a cohesive visual and functional transition without interruption.

While private paving systems should be of outstanding design and character, they should be encouraged to fit within the overall fabric of the streetscape and not dominate the visual experience. Private paving should be sensitive to the needs of the beach-going public and be handicap accessible. Paving design, materials and maintenance should be compatible with public sector site elements.

Response: Applicant will be working with City staff to ensure that the project complies with the streetscape planned for this area.

Landscape:

Private sector landscape planting should be consistent with the proposed use, adjacent development, and as required under [Section 47-12](#) of the ULDR. Existing trees should be preserved or otherwise mitigated as outlined in the ordinance. Plant material should be used in a contemporary urban context, acknowledging the limitations of the beach environment, and creating a lush tropical environment in keeping with the visual quality of the beach and adjoining public corridors. Plant massings should be rich in material, with special attention given to the ground plane treatment. Color should be used in bold, massive statements where appropriate.

Private sector plant material which immediately abuts and is readily visible from adjoining public corridors should reflect the species, size, and spacing of the public sector landscape and provide a cohesive visual and functional transition without interruption. Landscape design and maintenance should be compatible with public sector site elements. Private

sector landscape planting should be consistent with the proposed use, adjacent development, and as required under [Section 47-12](#). Existing trees should be preserved or otherwise mitigated as outlined in [Section 47-12](#). Plant material should be used in a contemporary urban context, acknowledging the limitations of the beach environment, and creating a lush tropical environment in keeping with the visual quality of the beach and adjoining public corridors. Plant massings should be rich in material, with special attention given to the ground plane treatment. Color should be used in bold, massive statements at intersections and where appropriate.

Private sector plant material which immediately abuts and is readily visible from adjoining public corridors should reflect the species, size, and spacing of the public sector landscape and provide a cohesive visual and functional transition without interruption. This street frontage landscaping should not be blocked visually by fences or other architectural treatments. All street frontages should have palms and shade trees. One half of the trees on these frontages should be shade trees. Property abutting the Intracoastal Waterway should have trees and palms planted along this water frontage. Landscape design and maintenance should be compatible with public sector site elements.

Response: All proposed landscaping for the Project will follow the ULDR, as demonstrated on the Landscape Plan, submitted as part of the DRC submittal package. The landscaping has been designed to be consistent with newer developments in the area, featuring a variety of tropical and indigenous landscaping of varying color and size. The selected plant materials were chosen to encourage pedestrian travel throughout the Project to reflect the tropical yet urban environment of the beach area, as well as considerations for salt and wind tolerance, and suitability with the soils on site.

i) *Site Furnishings:*

Private sector site furnishings should be consistent with the proposed use, adjacent development, and as required by applicable codes. Site furnishings should be considered an integral component of the urban streetscape and designed/located accordingly. Emphasis should be given to maximizing passive relaxation opportunities in locations which allow varying degrees of interaction with adjacent pedestrian corridors. Visual clutter and haphazard distribution of site furnishings should be discouraged. Site furnishing design, materials, and maintenance should be compatible with public sector site elements.

Vending machines visible from public rights-of-way should be located and/or designed to be compatible with the adjacent development and public sector site elements. The location of these vending machines shall be compatible with adjacent architectural color and style; uniform in style, material, height and color when located next to other vending machines and must not interfere with public automobile or pedestrian access.

Response: Site furnishings are provided throughout the project. These furnishings will supplement the design language demonstrated by architectural and material components included throughout the project for a cohesive design statement.

j) *Signage:*

Private sector signage should be consistent with the proposed use, adjacent development, and as required under [Section 47-12](#). Signage should be considered an integral component of the urban streetscape and designed/located accordingly. Signage should be restrained in character and no larger than necessary for adequate identification. Wherever possible, signage should be integrated with the building architecture, arcades or canopies. Private signage which improves the pedestrian's orientation to adjoining pedestrian and vehicular circulation systems should be encouraged.

Building signage should be discouraged above the building's second floor elevation except on hotels which may be permitted to display a single discrete sign on both the north and south faces of the main building mass. Roof signs and billboards should be expressly prohibited. Freestanding signs should be located and sized so they do not obstruct views to/from adjoining parcels or impede clear view of pedestrian and vehicular traffic and traffic control devices.

The intensity and type of signage illumination should not be offensive to surrounding parcels or the uses therein. Signage style and character should enhance the visual and functional quality of the adjoining public corridor. Signage design, material and maintenance should be compatible with public sector site elements.

Response: Acknowledged. All proposed signage will satisfy these requirements.

k) *Lighting:*

Private sector site lighting should be consistent with the proposed use, adjacent development, and as required under applicable codes. Site lighting should be considered an integral component of the urban streetscape and designed/located accordingly. Emphasis should be placed on both the nighttime effects of illumination quality and the daytime impact of the standard's appearance.

Site lighting should be consistent with the theme of the immediate context and compatible with the lighting of adjacent parcels. Light distributions should be relatively uniform and appropriate foot-candle levels should be provided for various uses. (Refer to adopted Public Sector Site Lighting Guidelines for average maintained foot-candle recommendations). All exterior private sector spaces should be sufficiently lit to allow police and citizen surveillance, enhance personal security, and discourage undesirable activities. Exterior lighting should be controlled by an automatic timer or photocell to insure regular activation.

Site lighting which immediately abuts and is readily visible from adjoining public corridors should reflect the fixture style, light source and illumination intensity of adjoining public lighting and provide a cohesive visual and functional transition without interruption. Site lighting design, materials and maintenance should be compatible with public sector site elements.

Response: Acknowledged. All landscape and garage lighting will be internal to the site and intended to create a safe pedestrian environment with no spillage over property lines.

l) *Utilities:*

Private sector utilities should be consistent with the proposed use, adjacent development, and as required under applicable codes. Above-grade utilities should be integrated with surrounding uses and carefully located to minimize visual and functional impact on the adjoining streetscape.

New development should be encouraged to provide underground utility lines. Existing or renovated development should be encouraged to relocate overhead utility lines underground.

Any above-grade utility elements should be consistent in placement, orientation, mounting and material. All above-grade utility elements should be painted one unobtrusive color which allows the elements to blend with their surroundings. All above-grade utilities should be screened by planting and/or low walls so they are not visible from the street and pedestrian circulation areas.

Response: All utilities will be located underground. All above grade utility elements will be adequately shielded from view and painted one unobtrusive color, other than the necessary back-flow preventers and standpipes, which are required to be painted red. These elements will, however, be shielded from view by plantings and will not be visible from the street of pedestrian circulation areas.

m) *Site Plan Objectives:*

The following Site Plan Objectives shall be incorporated in all development proposals for the Central Beach Revitalization Area and RMM-25, RMH-25 and RMH-60 zoning districts. This section provides an outline on how and what outdoor spaces need to be provided as part of development proposals. The intent is to ensure that development is more than buildings and structures. The quality of the Central Beach Revitalization area will be enhanced with the addition of planned outdoor spaces.

Response: As noted herein, the Project incorporates several carefully designed outdoor and outwardly oriented spaces, supported by ground level active uses, in order to improve the character of the Property and better integrate into the surrounding

community by encouraging pedestrian traffic, promoting pedestrian interaction and access between the street and the ground floor. These walkways are heavily landscaped and complimented with features such as site walls and other seating opportunities to foster a pedestrian-friendly atmosphere.

n) *Usable Outdoor Spaces:*

Hotel and residential development shall provide usable outdoor recreation spaces designated to accommodate passive areas (sitting, etc.) and active areas (pools, etc.). Commercial development shall provide usable outdoor sitting and gathering spaces designed to furnish a place for pedestrians to view, use or consume the goods and services offered.

There shall be a variety in the sizes of outdoor spaces and the level of detail shall be such as ornamental fountains, waterfalls, sculptures, trellises, arbors, seating facilities and landscape features.

The total size required for the outdoor spaces will be evaluated on the size and use of the proposed development.

Response: The Project incorporates outdoor usable recreational areas.

Pedestrian Accessible Spaces:

Hotel and commercial development shall provide direct access to adjoining public sidewalks in order to stimulate pedestrian activity. These spaces shall supplement public sector walkways and improve access to the beach and the Intracoastal Waterway, or both.

Response: Direct access to public sidewalks is provided.

o) *Defensible Space:*

All projects shall promote a secure environment. This is to be accomplished by designing with CPTED (Crime Prevention Through Environmental Design) principles.

In addition to the above requirements, the following may be required based on the site specifics of each project:

- Provide plant material in the adjacent right-of-way.

- Provide foundation/entry plantings to the development.

- All sites should exhibit lush tropical landscaping.

Provide large trees/shrubs (mature plantings). This may be required in order to mitigate certain objectionable uses or needed to assist in the neighborhood compatibility of the proposed development.

Preserve view corridors. The City recognizes that existing and new views to and from the Intracoastal Waterway, Atlantic Ocean, Bonnet House and public parks are important to maintain.

Response: The Project incorporates lush tropical landscaping throughout the site; however, the landscape is designed with CPTED (Crime Prevention Through Environmental Design) principles to provide adequate visibility and a safe environment for the pedestrian. Adequate lighting will also be provided to increase safety.

Sec. 47-23.8. - Waterway use.

- A. Buildings and land uses on parcels abutting waterways in nonresidential districts and in multifamily districts shall be designed to preserve the character of the city and neighborhood in which they are located, harmonize with other development in the area, and protect and enhance the scenic quality and tranquility of the waterways. Special provisions are needed to realize these objectives, which can be stated only in general terms, and at the same time permit a reasonable use of land and depend on details of design of the buildings, appurtenances, yards and landscaping and their relation to the waterway and other uses on the waterway.

Response: Bahia Cabana is envisioned to be a modern resort community that reflects the surrounding waterfront fabric and lifestyle of Fort Lauderdale. The residential mixed-use nature of the project is in context with the surrounding residential and commercial uses throughout the Central Beach area. Bahia Cabana is fronting the Intracoastal Waterway and a large marina. This world class marina serves to setback the residential mixed-use buildings from adjacent waterways and surrounding properties. The development's light and transparent architectural styling complements the area's maritime heritage and modernizes the property within the unique setting of Fort Lauderdale's waterfront through creative massing, enclosed parking and the thoughtful placement of buildings.

- B. For purposes of this Section 47-23.8, "on a waterway" means a development site which abuts a waterway. This section shall not apply to development within the downtown RAC, except for development within the RAC-RPO district, and shall not apply to the central beach area districts. Any proposed nonresidential or multifamily use on a waterway shall require a site plan level III development permit, as provided

in Section 47-24, Development Permits and Procedures. The application shall include all elevations visible from the waterfront. A use on a waterway shall, in addition to all other requirements of the ULDR, meet the requirements as follows:

1. A twenty-foot landscaped yard is required adjacent to the existing bulkhead line. The required twenty-foot yard shall not be used or developed for any purpose other than landscaping and the minimum number of driveways or walkways reasonably necessary to serve permitted nonresidential or multifamily waterfront uses, unless specifically approved by the planning and zoning board. The twenty-foot yard shall not apply to marinas or yacht clubs.
2. Review of Neighborhood Compatibility, Scale, Bulk and Mass, as provided in Section 47-25.3.A.3.e.i.

Response: N/A. However, there is a 20-foot setback from the docks.

- C. Any property zoned B-2, B-3 or I which abuts a waterway shall be used for a marina, a hotel marina, or a shipyard, where such uses are permitted within the B-2, B-3 or I zoning districts.

Response: N/A. Does not apply

Adequacy Requirements (Sec. 47-25.2)

A. Applicability

RESPONSE: The adequacy requirements of Section 47-25.2 are applicable to this project.

B. Communications Network

RESPONSE: The project will not overburden the existing communications network.

C. Drainage Facilities

RESPONSE: Public drainage facilities are not affected by the proposal. Any construction on the Property will comply with applicable stormwater management regulations.

D. Environmentally Sensitive Lands

RESPONSE: To Applicant's knowledge, there are no environmentally sensitive lands in the vicinity.

E. Fire Protection

RESPONSE: Adequate water supply, fire hydrants, fire apparatus, appropriate access and exits, and a sprinkler system will remain or be provided in accordance with the Florida Building Code and South Florida Fire Code.

F. Parks and Open Space

RESPONSE: Applicant will pay the City parks impact fee as required at time of permit.

G. Police Protection

RESPONSE: The Project will incorporate controlled access, security, and surveillance on the premises.

H. Potable Water and Potable Water Facilities

RESPONSE: Applicant anticipates that the existing water mains and treatment facilities have sufficient capacity and an ability to handle the added demand of this project. A written determination of reservation of available capacity will be requested from the City and provided to Sustainable Development staff upon request.

I. Sanitary Sewer

RESPONSE: Applicant anticipates that existing sewer treatment facilities and systems have sufficient capacity to provide for the needs of the project. A written determination of reservation of available capacity will be requested from the City and provided to Sustainable Development staff upon request.

J. Schools

RESPONSE: Applicant will obtain a SCAD from the School Board prior to final DRC approval.

K. Solid Waste

RESPONSE: The Applicant will provide adequate solid waste collection facilities and service in connection with the project and will provide evidence to the City demonstrating all solid waste will be disposed of in a manner consistent with all governmental requirements upon request.

L. Storm Water

RESPONSE: Please see site plan and civil drawings for information regarding proposed treatment of storm water.

M. Transportation Facilities

RESPONSE: Applicant has provided a traffic impact statement.

N. Wastewater

RESPONSE: Adequate wastewater services will be provided for the needs of the proposed development.

O. Trash Management

RESPONSE: Applicant will provide a trash management plan.

P. Historic and Archaeological Resources

RESPONSE: N/A.

Q. Hurricane Evacuation

RESPONSE: Applicant will provide a letter from the Broward County Emergency Management Division regarding hurricane evacuation analysis.

PROJECT DESIGN TEAM

ARCHITECTS

ADACHE GROUP ARCHITECTS
550 SOUTH FEDERAL HIGHWAY, FT. LAUDERDALE, FL 33301
PH: (954) 525-8133 FAX: (954) 728-8159
EMAIL: info@adache.com www.adache.com

LISCENCE #: AR0007073

CIVIL ENGINEER

GRAEF
9400 SOUTH DADELAND BLVD., SUITE 601, MIAMI, FL 33156
PH: 305 / 378 5555
EMAIL: nelson.ortiz@graef-usa.com graef-usa.com

SURVEYOR

McLAUGHLIN ENGINEERING CO
1700 NW 64TH ST SUITE 400
PH: 954.763.7611 FAX: 954.763.7615
EMAIL: admin@meco400.com www.meco400.com

STRUCTURAL ENGINEER

MASTER CONSULTING ENGINEERING
4101 RAVENSWOOD RD, SUITE 307, FT LAUDERDALE
PH: 954 / 210-7871
EMAIL: ARMANDO@CASTELLON@MCEENGINEERS.COM

MECHANICAL, ELECTRICAL , PLUMBING ENGINEER

JOHNSON, AVEDANO, LOPEZ, RODRIGUEZ & WALEWSKI
ENGINEERING GROUP, INC
2510 NW 97TH AVENUE, SUITE 220, DORAL, FLORIDA 33172-1407
PH: 561/ 597-0660
EMAIL: AHLOPEZ@JALRW.COM WWW.JALRW.COM

LANDSCAPE ARCHITECT

TOPO-GRAPHIC LLC
PUNTA GORDA, FLORIDA
PH: 561 840 0248 (954) 728-8159
EMAIL: INFO@TOPO-GRAPHIC.COM WWW.TOPO-GRAPHIC.COM

TRAFFIC STUDY

SUZANNE DANIELSEN

PH: 954 798 0926
EMAIL: jsdanielsen12@outlook.com

OWNER

3001-18 HARBOR DRIVE LLC
1199 SOUTH FEDERAL HIGHWAY, SUITE 384, BOCA RATON, FL, 33432
PH:
EMAIL:

PARKING REQ'D HOTEL_GUESTROOM						
UNIT TYPE		COUNT	PARKING FACTOR	PARKING REQ'D CALC		
HOTEL - GUESTROOMS		124	0.67	84		
PARKING REQ'D HOTEL_RESIDENTIAL						
UNIT TYPE	BEDS	Dens	Dens	COUNT	PARKING FACTOR	PARKING REQ'D
PH	3	1	1	4	2.1	9
PH	4	1	1	2	2.2	5
TOTAL # OF UNITS & PARKING REQ'D				6		14

PARKING REQUIRED _APARTMENT BUILDING			
UNIT TYPE DESCRIPTION	UNIT COUNT	PARKING FACTOR	PARKING REQ'D CALC
1 BED/ 1 DEN	2	2	4
1 BEDROOM	35	1.75	61
TOTAL #OF UNITS & PARKING REQ'D	37		65
VISITOR (10% REQ)			17
TOTAL PARKING REQUIRED (84+14+65+17)			180

PARKING PROVIDED	
8' - 8" x 18' ADA PARKING	7
8' - 8" x 18' LIFT 10' - 6" CLR	26
8' - 8" x 18' LIFT 12' - 2" CLR	20
8' - 8" x 18' LIFT 14' - 0" CLR	12
8' - 8" x 18' TYPICAL PARKING	157
12' - 0" x 18' ADA VAN ACCESSIBLE	2
	224

ADA PARKING REQUIREMENTS:
FBC ACCESSIBILITY 508.2 FOR BUILDINGS WITH 201-300 SPACES, 7 PARKING SPACES ARE REQUIRED TO BE ADA
FBC ACCESSIBILITY 502 OF THE ADA SPACES, 1 IN 6 MUST BE ADA VAN ACCESSIBLE

INDEX OF DRAWINGS

A.0.0 GENERAL

• A-0.00	COVER SHEET
• A-0.01	SITE SURVEY
• A-0.02	SITE PHOTOS
• A-0.03	LAND USE 700'
• A-0.04	PLAT MAP
• A-0.05	RENDERING
• A-0.06	RENDERING
• A-0.07	RENDERING SUNSET
• A-0.08	RENDERING- SUNSET
• A-0.09	RENDERING NIGHT VIEW
• A-0.10	RENDERING NIGHT VIEW
• A-0.11	RENDERINGS NIGHT VIEW
• A-0.12	RENDERINGS NIGHT VIEW
• A-0.13	RENDERING CONTEXT
• A-0.14	RENDERING- CONTEXT
• A-0.15	VEHICULAR MOVEMENT DIAGRAM
• A-0.16	VEHICULAR MOVEMENT DIAGRAM

A.1.1 SITE

• A-1.00	SITE PLAN
• A-1.01	COLORSD SITE PLAN
• A-1.02	SETBACK STUDY-HOTEL
• A-1.03	STREET SECTIONS
• A-1.04	PERVIOUS AREAS

A.1.3 FLOOR PLANS

• A-1.3.01H	FLOOR PLAN HOTEL _LEVEL 1
• A-1.3.02H	FLOOR PLAN HOTEL _LEVEL 2
• A-1.3.03H	FLOOR PLAN HOTEL _LEVEL 3
• A-1.3.04H	FLOOR PLAN HOTEL _LEVEL 4
• A-1.3.05H	FLOOR PLAN HOTEL _LEVEL 5
• A-1.3.06H	FLOOR PLAN HOTEL _LEVEL 6
• A-1.3.07H	FLOOR PLAN HOTEL _LEVEL 7
• A-1.3.08H	FLOOR PLAN HOTEL _LEVEL 8
• A-1.3.09H	FLOOR PLAN HOTEL _LEVEL 9
• A-1.3.10H	FLOOR PLAN HOTEL _LEVEL 10
• A-1.3.11H	FLOOR PLAN HOTEL _ROOF PLAN

A.1.4 ENLARGED FLOOR PLANS

• A-1.4.01	ENLARGED FLOOR PLANS
• A-1.4.02	ENLARGED FLOOR PLANS
• A-1.4.03	ENLARGED FLOOR PLANS

A.2 ELEVATIONS

• A-1.5.01H	ELEVATIONS-HOTEL
• A-1.5.02H	ELEVATIONS-HOTEL

A.3 SECTIONS

• A-3.01H	SECTIONS HOTEL
• A-3.02H	SECTIONS HOTEL

A.4 3D VIEWS

• A-4.01	3D AERIAL VIEWS
----------	-----------------

A.5 SITE STUDIES

• A-5.00	SUN STUDIES
• A-5.01	LOGISTICS STUDY

A.6 DETAILS

• A-6.01	DETAILS
• A-6.02	DETAILS

A.7 LIFE SAFETY

• A-7.1.01H	LIFE SAFETY
• A-7.1.02H	LIFE SAFETY
• A-7.1.03H	LIFE SAFETY



True and accurate data and preliminary concepts can only result from a thorough design process involving collaboration with engineers and governmental authorities having jurisdiction over this site. All conceptual plans, elevations, and data are based upon assumptions and unconfirmed preliminary information and are subject to change.

SITE 1- PROJECT DATA

DEVELOPMENT/ PROJECT NAME	BAHIA CABANA HOTEL	SITE PLAN LEVEL IV
DEVELOPMENT/ PROJECT ADDRESS	3001 HARBOR DRIVE, FT LAUDERDALE, FL; 3007 HARBOR DRIVE, 33316	
LEGAL DESCRIPTION	OCEAN HARBOR 26-39 B LOT 12, OCEAN HARBOR 26-39 B LOT 13	
TAX ID FOLIO NUMBERS (FOR ALL PARCELS IN DEVELOPMENT)	FOLIO NUMBER: 504212240100, FOLIO NUMBER: 504212240090	
REQUEST/ DESCRIPTION OF PROJECT	MIXED USE: LEVEL 1: HOTEL LOBBY/ RESTAURANT; LEVEL 2: MEETING ROOMS, SPA , GYM; LEVEL 3-7: 1-STORY HOTEL ROOMS; LEVEL 8-9: RESIDENCES BUILDING DESIGN MAINTAINS WATERWAY VIEWS BY MEANS OF TRANSPARENT ELEMENTS IN THE FACADE, OPEN PLAZA AND HEIGHT OF THE FIRST LEVEL. THE DESIGN ENCOURAGES THE PRESERVATION OF THE WATERWAYS MAINTAINING THE EXISTING DESIGNATED AREA FOR BOAT DOCKS AND ITS PEDESTRIAN ACCESS.	
BUILDING TYPE	• Mixed-use/Hotel R-1, Residences R-2 Occupancy • Construction Type: I-B, Noncombustible, sprinklered • Allowable Height: 180 ft - FBC Table 504.3 • Allowable Stories: 12 - FBC Table 504.4 • Allowable Area: Unlimited - FBC Table 506.2 • FBC Table 601 • Structural frame: 2 hrs • Bearing Walls: 2 hrs, or 1 hr where supporting a roof only • Floor construction: 2 hrs • Roof: 1 hr	
APPLICABLE CODES	2017 FLORIDA BUILDING CODE (6TH EDITION), Residential units to comply with with Fair Housing Provisions per FBC Accessibility Code	

	EXISTING	PROPOSED
LAND USE DESIGNATION	CENTRAL BEACH REGIONAL ACTIVITY CENTER	CENTRAL BEACH REGIONAL ACTIVITY CENTER
ZONING DESIGNATION	SBMHA	SBMHA
USE OF PROPERTY/ OCCUPANCY	10 - Commercial - Vacant Commercial	COMMERCIAL- RESIDENTIAL (R-1)
RESIDENTIAL SF (AND TYPE)	RESIDENCES= 16,636 SF TERRACES AND BALCONIES= 3,450 SF	
NUMBER OF RESIDENTIAL UNITS	6 DWELLING UNITS	
NON-RESIDENTIAL SF (AND TYPE)	HOTEL ROOMS (INCLUDING TERRACES & BALCONIES)= 61,698 SF KEYS=124 SERVICE & COMMON AREAS= 114,449 SF	
TOTAL BLDG. SF. (INCLUDE STRUCTURED PARKING)	130,889 SF (NOT INCLUDING LANDSCAPE OR OUTDOOR AREAS)	
SITE ADJACENT TO WATERWAY	Yes	

DIMENSIONAL REQUIREMENTS	REQUIRED/ PERMITTED	PROPOSED
LOT SIZE (SF/ ACREAGE)		42,626SF/ 43,560=0.978 ACRE
LOT DENSITY RESIDENTIAL	48 UNITS /ACRE X 0.987 ACRE= 46.94 DWELLING UNITS	6 DWELLING UNITS
LOT DENSITY HOTEL	N/A	124 HOTEL UNITS
LOT WIDTH	211'-0" AVERAGE	211'-0" AVERAGE
BUILDING HEIGHT (FEET/ LEVELS)	120'-0"	117'-0" (MEASURED FROM GRADE)
STRUCTURE LENGTH	200'-0"	180'-0"
STRUCTURE WIDTH	200'-0"	194'-6"
FLOOR AREA RATIO	5 MAX	130,889 SF/ 42,626 SF= 3.02 F.A.R.
LOT COVERAGE	N/A	20,678 SF 49%
OPEN SPACE	N/A	21,729 SF 51%
LANDSCAPE AREA	25%	AREA LOT SF LOT % 10,952.5 SF 42,626 SF 25.69%
PARKING SPACES	98 No parking on this site-Required Parking provided on site 2	98 PROVIDED ON SITE 2- SEE CHART
PARKING SPACES CALCULATION (Restaurant, Dining areas and Roof top Bars are for guest use only. No additional parking provided.) Bicycle Parking see Sheet A-6.01		GUESTROOMS N/A 124 0.67 84 RESIDENCES 3 4 2.1 9 4 2 2.2 5 98
LOADING ZONE	LOADING ZONE TYPE II	LOADING ZONE TYPE II- 12' x 45'

* SEE SETBACK STUDY SHEETA-1.02	REQUIRED/ PERMITTED	PROPOSED
FRONT [S]	20'-0"	20'-0" SETBACK 27'-10" STEPBACK ROOF TOP
SIDE [W]	15'-0" OR 1/2 BLDG HEIGHT	15' SETBACK/ 20' STEPBACK MAX HEIGHT 40' 30' STEPBACK MAX HEIGHT 60'/ 40' STEPBACK MAX HEIGHT 80' / 51'-2" STEPBACK MAX HEIGHT 102'-4" 57'-6" STEPBACK ROOF HEIGHT 115'-0"
REAR [N]	20'-0" OR 1/2 BLDG HEIGHT	20' SETBACK/ 30' STEPBACK MAX HEIGHT 60' 40' STEPBACK MAX HEIGHT 80' 51'-2" STEPBACK MAX HEIGHT 102'-4" 57'-6" STEPBACK ROOF HEIGHT 115'-0"
SIDE [E]	20'-0" OR 1/2 BLDG HEIGHT	30' SETBACK/ 40' STEPBACK MAX HEIGHT 80' 51'-2" STEPBACK MAX HEIGHT 102'-4" 57'-6" STEPBACK ROOF HEIGHT 115'-0"

HOTEL BUILDING AREAS	HOTEL BUILDING AREAS	HOTEL BUILDING AREAS	HOTEL BUILDING AREAS	HOTEL ROOM KEY COUNT
LANDSCAPE 150 SF 150 SF	BUSINESS 115 SF BC 362 SF BOARD ROOM 2,017 SF COVERED TERRACE 4,453 SF MEETING ROOM 407 SF OFFICE 1,024 SF PREFUNCTION 1,909 SF TERRACE 10,289 SF	1,329 SF LANDSCAPE 6,047 SF PLANTER 781 SF 6,828 SF LANDSCAPE 142 SF 142 SF MECHANICAL 701 SF CONTROL ROOM 626 SF COOLING TOWER 136 SF DOMESTIC WATER 1,011 SF ELECT LV 577 SF ELECT 241 SF FIRE COMMAND 178 SF FIRE PUMP 421 SF FPL 392 SF GEN 207 SF RETAIL 500 SF GREASE 45 SF LV 131 SF MECH 1,773 SF OUTSIDE AIR UNIT 121 SF POOL EQUIPMENT 69 SF 6,222 SF	RESIDENTIAL BALCONY 3,450 SF TERRACE 3,450 SF RESTAURANT - DINING 3,356 SF BAR 1,049 SF CAFE/BAR 1,023 SF TERRACE 3,378 SF HOTEL AMENITY 3,406 SF RESTAURANT/ BAR 12,212 SF RESTROOMS 892 SF MEN 1,078 SF WOMEN 2,010 SF RETAIL 293 SF GRAB & GO 207 SF RETAIL 500 SF STORAGE 1,469 SF ST 876 SF STORAGE/ PANTRY 175 SF VEND 175 SF 2,696 SF	Level 3 SD 6 SK 22 28 Level 4 SD 5 SK 22 27 Level 5 SD 6 SK 19 25 Level 6 SD 5 SK 17 22 Level 7 SD 5 SK 17 22 124
AMENITIES 1,554 SF GYM 2,286 SF SPA 3,840 SF	AMENITIES-OUTDOORS 4,583 SF OUTDOOR PRIVATE PLAZA 3,560 SF TERRACE 8,143 SF	ASSEMBLY 3,012 SF LOBBY 251 SF LOBBY ENTRANCE 3,263 SF	ASSEMBLY - POOL 4,034 SF LANDSCAPE/ POOL DECK LAWN LOUNGE 1,990 SF POOL 3,094 SF POOL DECK 9,118 SF	FEMA Flood Elevation: Property is located in Zone AE 6 on the December 2019 Preliminary FIRM, Panel 576J (6.00 NAVD)
BOH 177 SF LAUNDRY 247 SF LOADING DOCK 1,220 SF LUGGAGE 68 SF OFFICE 255 SF RESTROOM 49 SF TR 42 SF TRASH ROOM 214 SF 2,272 SF	LAUNDRY 177 SF LOADING DOCK 1,220 SF LUGGAGE 68 SF OFFICE 255 SF RESTROOM 49 SF TR 42 SF TRASH ROOM 214 SF 2,272 SF	PEDESTRIAN WALKWAY 1,185 SF STAIRS 425 SF CORES & VERTICAL CIRCULATION 20,028 SF ELEV. 2,150 SF ELEV. VEST. 409 SF S.E. 72 SF STAIRS 8,410 SF 11,041 SF	HOTEL - GUESTROOMS 7,929 SF GUEST ROOM 48,987 SF TERRACE 4,782 SF 61,698 SF KITCHEN COMMERCIAL 336 SF BAR 993 SF	Grade Elevation: Property is located landward of the CCCL (Coastal Construction Central Line) therefore the grade elevation should be BFE + 1 (6.00 NAVD + 1.00 = 7.00 NAVD)

FINAL DRC
74.06A_HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERALE, FL 33316

COVER SHEET

A-0.00

adache
group architects



FINAL DRC
74.06A_HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERDALE, FL 33316

SITE PHOTOS

A-0.02

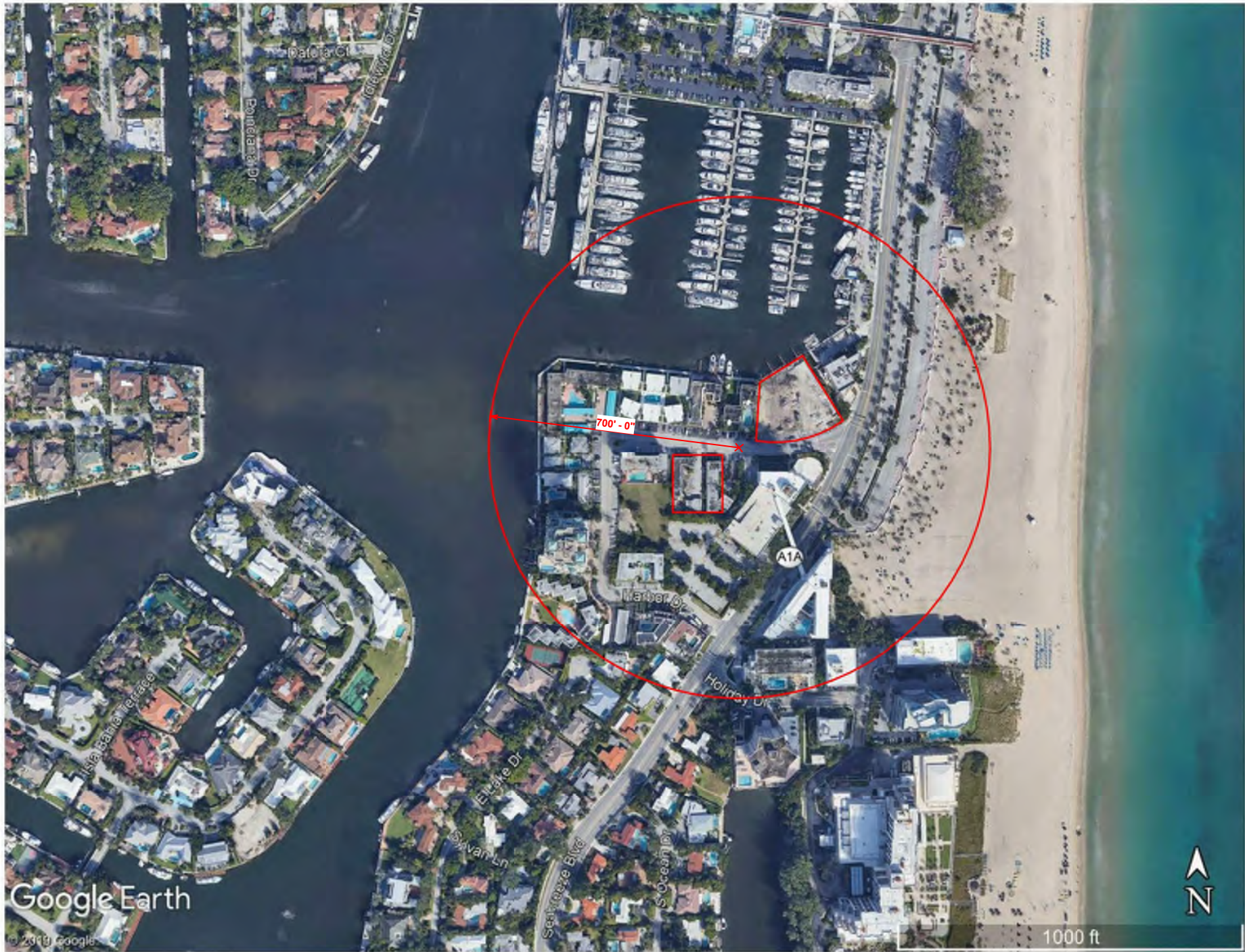
adache
group architects



GIS-LAND USE
SCALE: 1" = 400'-0"



GIS-ZONING
SCALE: 1" = 400'-0"



GIS ZONING MAP
SCALE: 1" = 160'-0"

FINAL DRC
74.06A HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERDALE, FL 33316

LAND USE 700'

A-0.03

adache
group architects

OCEAN HARBOR

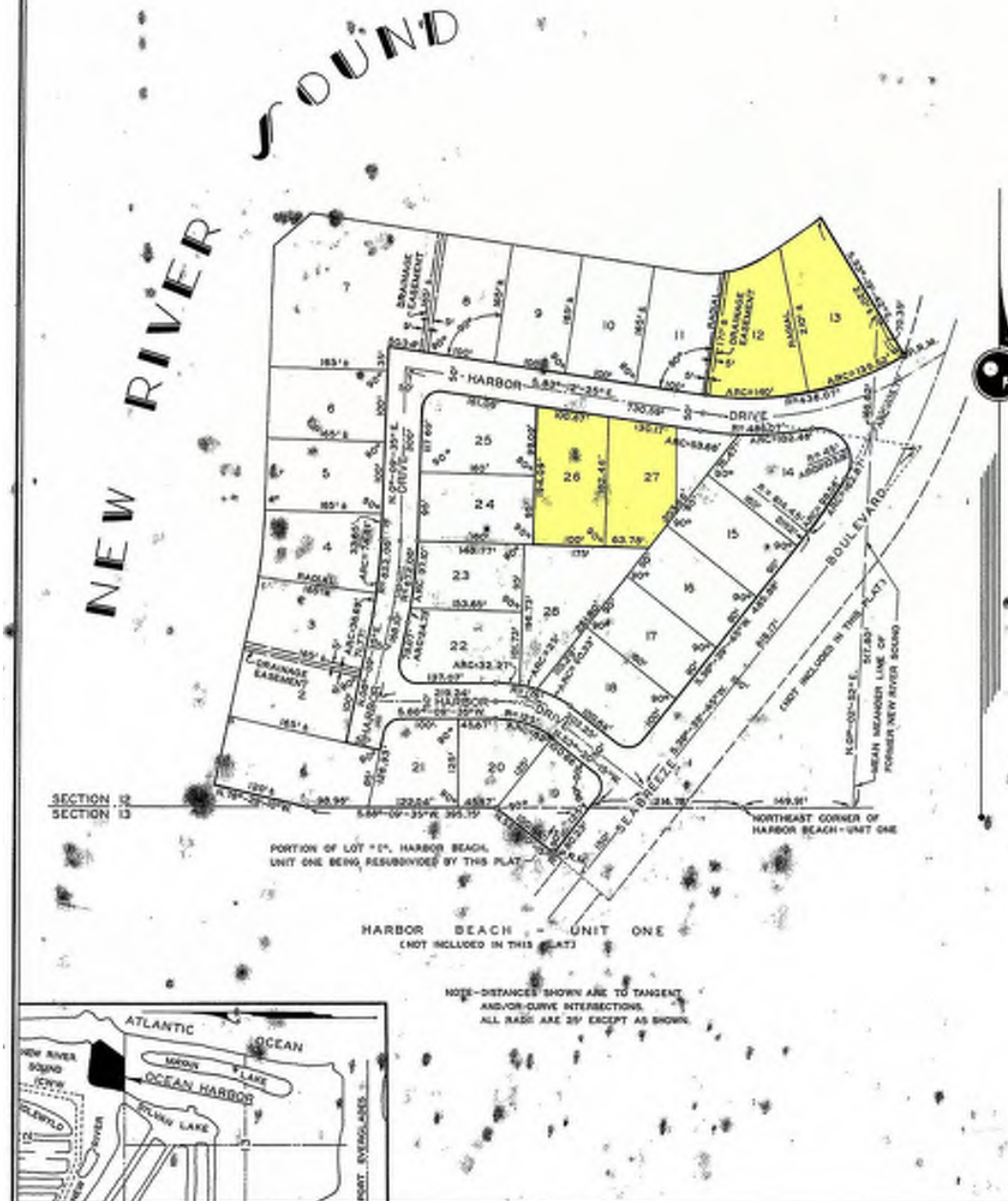
A SUBDIVISION IN SECTIONS 12 AND 13, TOWNSHIP 50 S., RANGE 42 E.,
CITY OF FORT LAUDERDALE, BROWARD COUNTY, FLORIDA

J. H. PHILPOTT
CIVIL ENGINEER
FORT LAUDERDALE

J. W. McLAUGHLIN
CIVIL ENGINEER
FORT LAUDERDALE

SCALE: 1"=100'

MAY, 1950



DESCRIPTION
A PORTION OF SECTION 12, TOWNSHIP 50 SOUTH, RANGE 42 EAST, BROWARD COUNTY, FLORIDA, DESCRIBED AS FOLLOWS:
COMMENCE AT THE INTERSECTION OF THE SOUTH BOUNDARY OF SAID SECTION 12 AND THE MEAN MEANDER LINE OF NEW RIVER SOUND; THENCE N. 01°-02'-32" E., ALONG SAID MEAN MEANDER LINE, A DISTANCE OF 517.60 FEET TO THE POINT OF BEGINNING; THENCE CONTINUOUS N. 01°-02'-32" E., A DISTANCE OF 166.82 FEET; THENCE S. 33°-09'-42" E., A DISTANCE OF 70.35 FEET; THENCE SOUTHWEST-ERLY ON A CURVE TO THE RIGHT HAVING A RADIUS OF 84.45 FEET, AN ARC DISTANCE OF 15.71 FEET TO THE POINT OF BEGINNING; AND
THAT PORTION OF SECTION 12, TOWNSHIP 50 SOUTH, RANGE 42 EAST, BROWARD COUNTY, FLORIDA, DESCRIBED AS FOLLOWS: BEGINNING AT A POINT ON THE SOUTH BOUNDARY OF SAID SECTION 12, SAID POINT BEING 254.78 FEET WEST OF THE NORTHEAST CORNER OF HARBOR BEACH UNIT ONE, ACCORD-ING TO THE PLAT THEREOF RECORDED IN PLAT BOOK 18, PAGE 6, OF THE PUBLIC RECORDS OF BROW-ARD COUNTY, FLORIDA; THENCE S. 68°-09'-39" W., ALONG SAID SOUTH BOUNDARY, A DISTANCE OF 395.75 FEET; THENCE N. 70°-29'-10" W., A DISTANCE OF 123 FEET, MORE OR LESS TO THE SHORE LINE OF NEW RIVER SOUND; THENCE NORTHERLY AND EASTERLY ALONG SAID SHORE LINE TO AN INTER-SECTION WITH THE MEAN MEANDER LINE OF FORMER NEW RIVER SOUND; THENCE S. 33°-09'-42" E., A DISTANCE OF 150 FEET, MORE OR LESS TO AN ANGLE POINT IN SAID MEAN MEANDER LINE; THENCE CONTINUING ALONG SAID MEAN MEANDER LINE S. 01°-02'-32" W., A DISTANCE OF 166.82 FEET; THENCE SOUTHWESTERLY ON A CURVE TO THE RIGHT HAVING A RADIUS OF 84.45 FEET, AN ARC DISTANCE OF 15.71 FEET; THENCE S. 33°-09'-42" E., A DISTANCE OF 485.89 FEET TO THE POINT OF BEGINNING, AND
THAT PORTION OF LOT "1", HARBOR BEACH, UNIT ONE, ACCORDING TO THE PLAT THEREOF RECOR-DED IN PLAT BOOK 18, PAGE 6, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA, DESCRIBED AS FOLLOWS:
BEGINNING AT A POINT ON THE NORTH BOUNDARY OF SAID LOT "1", SAID POINT BEING 314.78 FEET WEST OF THE NORTHEAST CORNER OF SAID HARBOR BEACH, UNIT ONE; THENCE S. 33°-09'-42" W., A DISTANCE OF 80.23 FEET; THENCE N. 53°-20'-15" W., A DISTANCE OF 100.64 FEET TO THE SAID NORTH BOUNDARY OF LOT "1"; THENCE N. 68°-09'-39" E., A DISTANCE OF 128.88 FEET TO THE POINT OF BEGINNING.

ENGINEERS CERTIFICATE
STATE OF FLORIDA, ss
COUNTY OF BROWARD, ss
I, J. W. McLAUGHLIN, REGISTERED ENGINEER AND REGISTERED LAND SURVEYOR, DO HEREBY CERTIFY THAT THIS PLAT OF OCEAN HARBOR IS A TRUE REPRESENTATION OF THE LANDS PLATTED AND SHOWN THEREON, THAT PERMANENT REFERENCE MONUMENTS HAVE BEEN SET AS INDICATED BY "R.M.", THAT THESE MEASUREMENTS HAVE BEEN MADE ON THE GROUND UNDER MY DIRECTION AND ARE TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF.
DATED AT FORT LAUDERDALE, FLORIDA, THIS 10th DAY OF MAY, A. D. 1950.
J. W. McLAUGHLIN
REGISTERED ENGINEER NO. 1113
REGISTERED LAND SURVEYOR NO. 187
STATE OF FLORIDA

PLANNING BOARD CERTIFICATE
THIS IS TO CERTIFY THAT THE PLANNING BOARD OF FORT LAUDERDALE, FLORIDA APPROVED AND ACCEPTED THIS PLAT OF OCEAN HARBOR BY RESOLUTION ADOPTED THIS 10th DAY OF MAY, A. D. 1950.
APPROVED FOR RECORD THIS 10th DAY OF MAY, A. D. 1950.
CITY ENGINEER
J. H. Philpott
CITY ENGINEER
REGISTERED ENGINEER NO. 2136
STATE OF FLORIDA
APPROVED FOR RECORD THIS 10th DAY OF MAY, A. D. 1950.
COUNTY ENGINEER
J. W. McLaughlin
COUNTY ENGINEER
REGISTERED ENGINEER NO. 46
STATE OF FLORIDA

RECORD
STATE OF FLORIDA, ss
COUNTY OF BROWARD, ss
THIS INSTRUMENT FILED FOR RECORD THIS 16th DAY OF JUNE, A. D. 1950 AND RECORDED IN BOOK 26 OF PLATS ON PAGE 39. RECORD VERIFIED.
J. D. Cabot
CLERK OF CIRCUIT COURT
BROWARD COUNTY, FLORIDA
BY Dorothy Adams

DEDICATION
STATE OF FLORIDA
COUNTY OF BROWARD
KNOW ALL MEN BY THESE PRESENTS, THAT OCEAN HARBOR, INC., A CORPORATION, THE OWNER OF ALL THE LANDS SHOWN AND REPRESENTED ON THIS PLAT HAS CAUSED SAID LANDS TO BE SUBDIVIDED AND PLATTED IN THE MANNER HEREON SHOWN, SAID SUBDIVISION TO BE KNOWN AS "OCEAN HARBOR". ALL PREVIOUS PLATS OF SAID LANDS ARE HEREBY VOIDED, CANCELLED, ANNULLED AND SUPERSEDED BY THIS PLAT. HARBOR DRIVE IS HEREBY DEDICATED TO THE PERPETUAL USE OF THE PUBLIC, RESERVING, HOWEVER, UNTO OCEAN HARBOR, INC., A CORPORATION, ITS SUCCESSORS AND ASSIGNS, THE REVERSION OR REVERSIONS THEREOF WHATEVER ANY OF THE SAME MAY BE DISCONTINUED BY LAW.
IN WITNESS WHEREOF, OCEAN HARBOR, INC., A CORPORATION, HAS CAUSED THIS INSTRUMENT TO BE SIGNED BY ITS PRESIDENT, AND ITS CORPORATE SEAL TO BE AFFIXED AND ATTESTED BY ITS SECRETARY THIS 10th DAY OF MAY, A. D. 1950.
SIGNED, SEALED AND DELIVERED
IN THE PRESENCE OF
J. H. Philpott
J. W. McLaughlin
OCEAN HARBOR, INC.
J. H. Philpott
PRESIDENT
J. W. McLaughlin
SECRETARY

NOTARY PUBLIC
STATE OF FLORIDA, ss
COUNTY OF BROWARD, ss
I HEREBY CERTIFY THAT ON THIS DAY PERSONALLY APPEARED BEFORE ME, AN OFFICER DULY AUTHO-RIZED TO ADMINISTER OATHS AND TAKE ACKNOWLEDGMENTS, L. COLEMAN JORDAN AND EDWARD L. VALIER AS PRESIDENT AND SECRETARY, RESPECTIVELY, OF OCEAN HARBOR, INC., A CORPORATION, AND THEY ACKNOWLEDGED BEFORE ME THAT THEY EXECUTED THE FOREGOING DEDICATION OF THE PLAT OF OCEAN HARBOR AS SUCH OFFICERS OF SAID CORPORATION, IN THE NAME OF SAID CORPOR-ATION, BEING DULY AUTHORIZED BY SAID CORPORATION SO TO EXECUTE SAID DEDICATION, AND THAT SAID DEDICATION IS THE ACT AND DEED OF SAID CORPORATION FOR THE PURPOSE THEREIN EXPRESSED.
WITNESS MY HAND AND OFFICIAL SEAL AT FORT LAUDERDALE, COUNTY OF BROWARD AND STATE OF FLORIDA, THIS 10th DAY OF MAY, A. D. 1950.
MY COMMISSION EXPIRES July 24, 1951
L. Coleman Jordan
NOTARY PUBLIC

NOTARY PUBLIC
STATE OF FLORIDA, ss
COUNTY OF BROWARD, ss
KNOW ALL MEN BY THESE PRESENTS, THAT HARBOR BEACH COMPANY, A CORPORATION, THE OWNER AND HOLDER OF MORTGAGE DATED MAY 22, 1950 AND NOW ON RECORD IN MORTGAGE BOOK 352, PAGE 102, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA UPON THE LANDS SHOWN ON THIS PLAT DOES FOR ITSELF, ITS SUCCESSORS AND ASSIGNS HEREBY JOINTLY CONSENT TO AND APPROVE THE AFORESAID PLAT OF SAID LANDS EXECUTED BY OCEAN HARBOR, INC., A CORPORATION, IN WITNESS WHEREOF, HARBOR BEACH COMPANY, A CORPORATION, HAS CAUSED THIS INSTRUMENT TO BE SIGNED BY ITS PRESIDENT, AND ITS CORPORATE SEAL TO BE AFFIXED AND ATTESTED BY ITS SECRETARY THIS 10th DAY OF MAY, A. D. 1950.
SIGNED, SEALED AND DELIVERED
IN THE PRESENCE OF
J. H. Philpott
J. W. McLaughlin
HARBOR BEACH COMPANY
J. H. Philpott
PRESIDENT
J. W. McLaughlin
SECRETARY

NOTARY PUBLIC
STATE OF FLORIDA, ss
COUNTY OF BROWARD, ss
I HEREBY CERTIFY THAT ON THIS DAY PERSONALLY APPEARED BEFORE ME, AN OFFICER DULY AUTHO-RIZED TO ADMINISTER OATHS AND TAKE ACKNOWLEDGMENTS, ARTHUR W. DOLE AND J. H. PHILPOTT AS PRESIDENT AND SECRETARY RESPECTIVELY OF HARBOR BEACH COMPANY, A CORPORATION, AND THEY ACKNOWLEDGED BEFORE ME THAT THEY EXECUTED THE FOREGOING DEDICATION OF THE PLAT OF OCEAN HARBOR AS SUCH OFFICERS OF SAID CORPORATION, IN THE NAME OF SAID CORPORATION, BEING DULY AUTHORIZED BY SAID CORPORATION SO TO EXECUTE SAID DEDICATION, AND THAT SAID DEDICATION IS THE ACT AND DEED OF SAID CORPORATION FOR THE PURPOSE THEREIN EXPRESSED.
WITNESS MY HAND AND OFFICIAL SEAL AT FORT LAUDERDALE, COUNTY OF BROWARD AND STATE OF FLORIDA, THIS 10th DAY OF MAY, A. D. 1950.
MY COMMISSION EXPIRES September 26, 1951
Arthur W. Dole
NOTARY PUBLIC

CITY CLERK CERTIFICATE
STATE OF FLORIDA, ss
COUNTY OF BROWARD, ss
I, S. H. MARSHALL, CITY AUDITOR AND CLERK OF THE CITY OF FORT LAUDERDALE, IN BROWARD COUNTY, FLORIDA, DO HEREBY CERTIFY THAT THIS PLAT OF OCEAN HARBOR IS APPROVED AND ACCEPTED FOR RECORD BY THE CITY COMMISSION OF SAID CITY OF FORT LAUDERDALE IN AND BY ORDINANCE NO. 6171 ADOPTED BY THE SAID CITY COMMISSION ON THE 23rd DAY OF MAY, A. D. 1950.
IN WITNESS WHEREOF, I HAVE HEREUNTO SET MY HAND AND AFFIXED THE CORPORATE SEAL OF SAID CITY IN SAID CITY THIS 23rd DAY OF MAY, A. D. 1950.
S. H. Marshall
CITY AUDITOR AND CLERK AS AFORESAID

COUNTY CLERK CERTIFICATE
STATE OF FLORIDA, ss
COUNTY OF BROWARD, ss
J. TED CABOT, CLERK OF THE CIRCUIT COURT OF SAID COUNTY, AND EX-OFFICIO CLERK OF THE BOARD OF COUNTY COMMISSIONERS OF SAID COUNTY, DO HEREBY CERTIFY THAT THIS PLAT OF OCEAN HARBOR WAS APPROVED AND ACCEPTED FOR RECORD BY THE SAID BOARD OF COUNTY COMMISSIONERS IN AND BY RESOLUTION ADOPTED BY SAID BOARD ON THE 16th DAY OF JUNE, A. D. 1950, ALSO THAT THIS PLAT COMPLIES WITH THE PROVISIONS OF "AN ACT TO REGULATE THE MAKING OF SURVEYS AND FILING FOR RECORD OF MAPS OR PLATS IN THE STATE OF FLORIDA", APPROVED BY THE GOVERNOR JUNE 15, 1925.
IN WITNESS WHEREOF, I HAVE SET MY HAND AND AFFIXED THE SEAL OF SAID COURT AT FORT LAUDERDALE, FLORIDA THIS 16th DAY OF JUNE, A. D. 1950.
J. D. Cabot
CLERK AS AFORESAID
BY Dorothy Adams



IMAGE IS NOT AN ACCURATE REPRESENTATION OF FINAL DESIGN-REFER TO LATEST DOCUMENTS

FINAL DRC : DESIGN
74.06A_HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERDALE, FL 33316

RENDERING

A-0.05

adache
group architects



IMAGE IS NOT AN ACCURATE REPRESENTATION OF FINAL DESIGN-REFER TO LATEST DOCUMENTS

FINAL DRC : DESIGN
74.06A_HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERDALE, FL 33316

RENDERING

A-0.06

adache
group architects



IMAGE IS NOT AN ACCURATE REPRESENTATION OF FINAL DESIGN-REFER TO LATEST DOCUMENTS

FINAL DRC : DESIGN
74.06A_HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERALE, FL 33316

RENDERING- SUNSET

A-0.08

adache
group architects



IMAGE IS NOT AN ACCURATE REPRESENTATION OF FINAL DESIGN-REFER TO LATEST DOCUMENTS

FINAL DRC : DESIGN
74.06A_HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERDALE, FL 33316

RENDERING NIGHT VIEW

A-0.09

adache
group architects



IMAGE IS NOT AN ACCURATE REPRESENTATION OF FINAL DESIGN-REFER TO LATEST DOCUMENTS

FINAL DRC : DESIGN
74.06A_HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERALE, FL 33316

RENDERING NIGHT VIEW

A-0.10

adache
group architects



IMAGE IS NOT AN ACCURATE REPRESENTATION OF FINAL DESIGN-REFER TO LATEST DOCUMENTS

FINAL DRC : DESIGN
74.06A_HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERDALE, FL 33316

RENDERINGS NIGHT VIEW

A-0.11

adache
group architects



FINAL DRC
74.06A_HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERALE, FL 33316

RENDERINGS NIGHT VIEW

A-0.12

adache
group architects



AERIAL DAY TIME CONTEXT
VIEW- SOUTH EAST

IMAGE IS NOT AN ACCURATE REPRESENTATION OF FINAL DESIGN-REFER TO LATEST DOCUMENTS

FINAL DRC
74.06A HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERDALE, FL 33316

RENDERING CONTEXT

A-0.13

adache
group architects



HOTEL SITE
3001- 3007 HARBOR DRIVE

AERIAL DAY TIME CONTEXT
VIEW- NORTH EAST

IMAGE IS NOT AN ACCURATE REPRESENTATION OF FINAL DESIGN-REFER TO LATEST DOCUMENTS

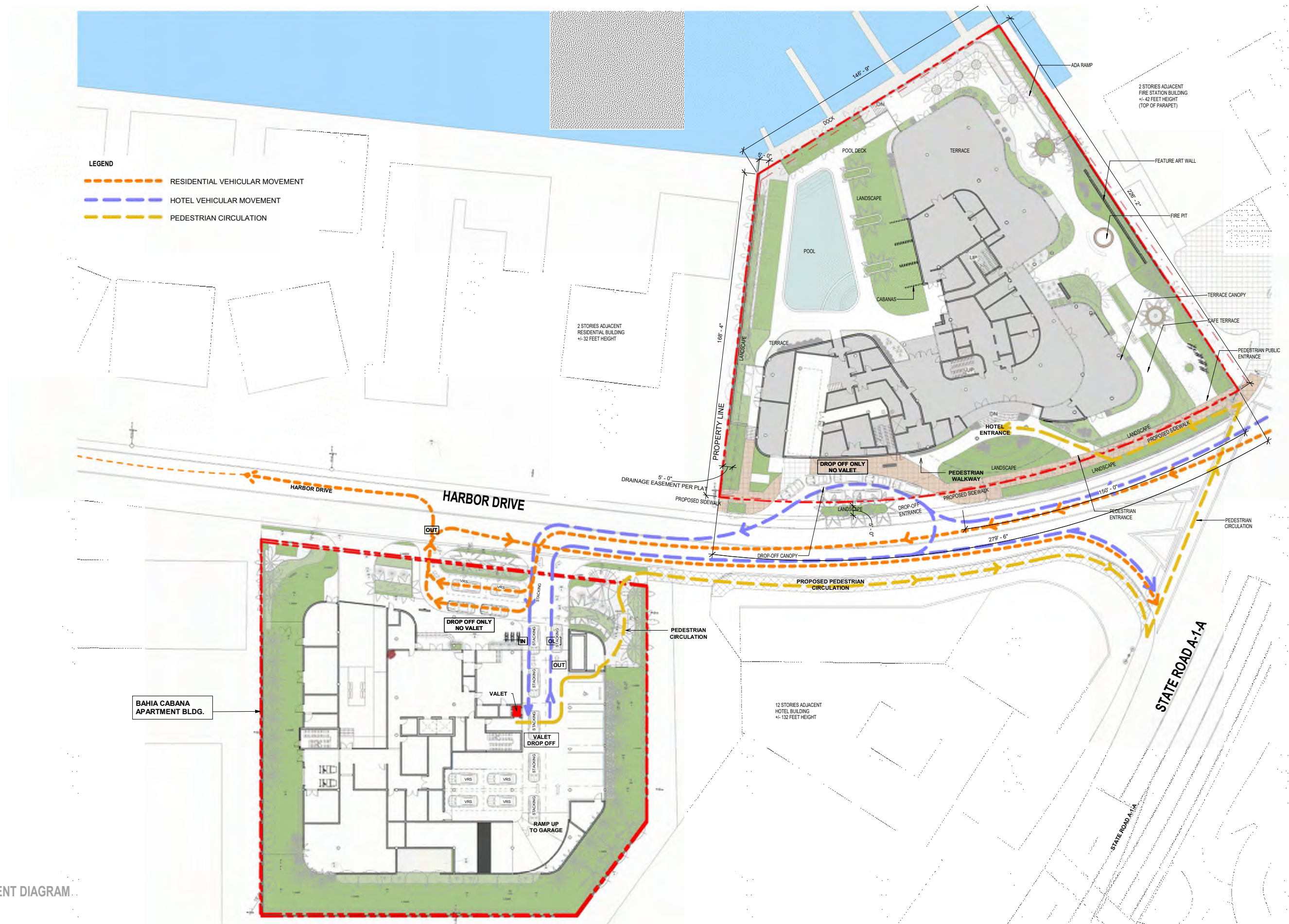
FINAL DRC : DESIGN
74.06A HOTEL
10/23/2020

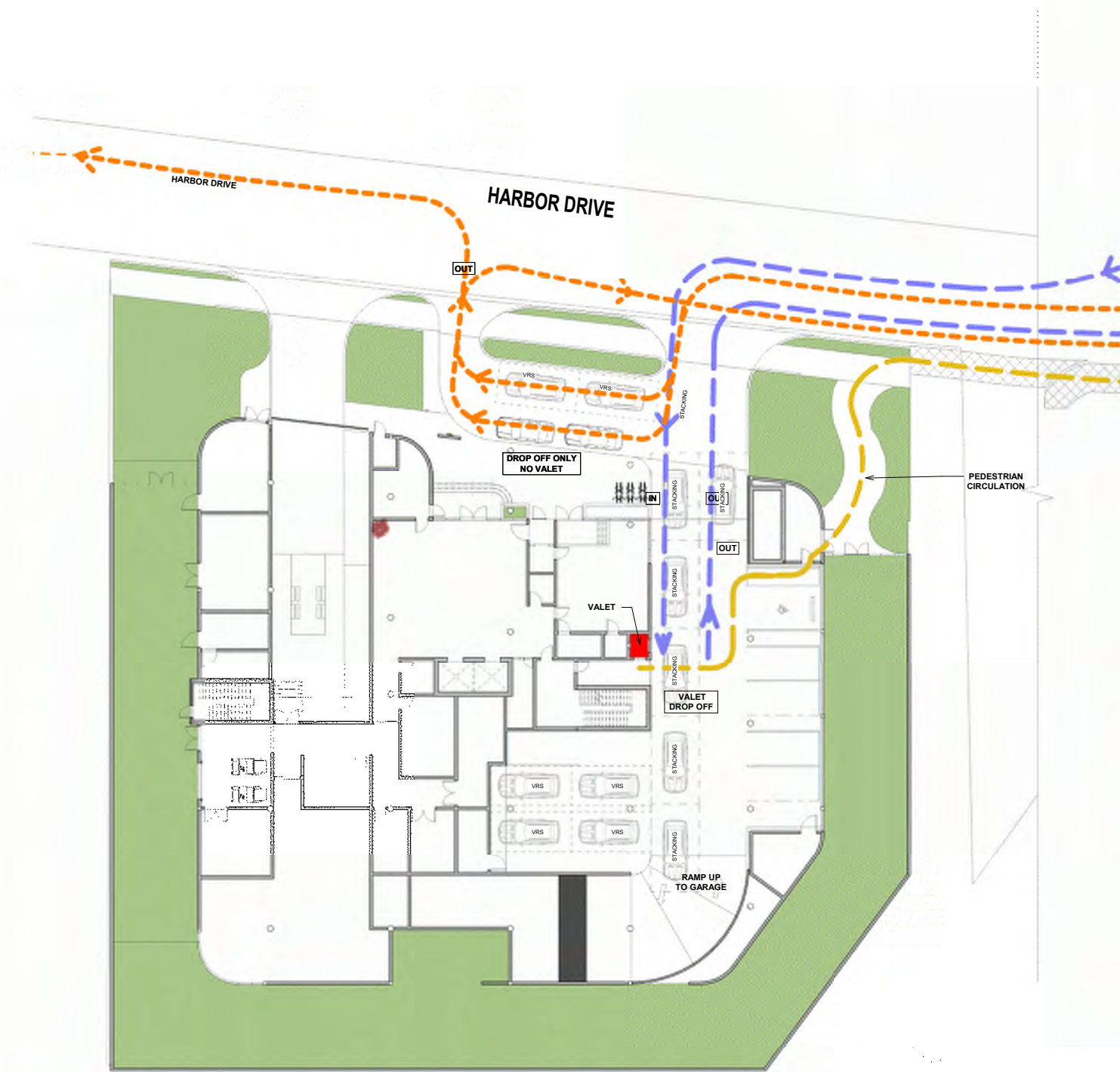
BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERDALE, FL 33316

RENDERING- CONTEXT

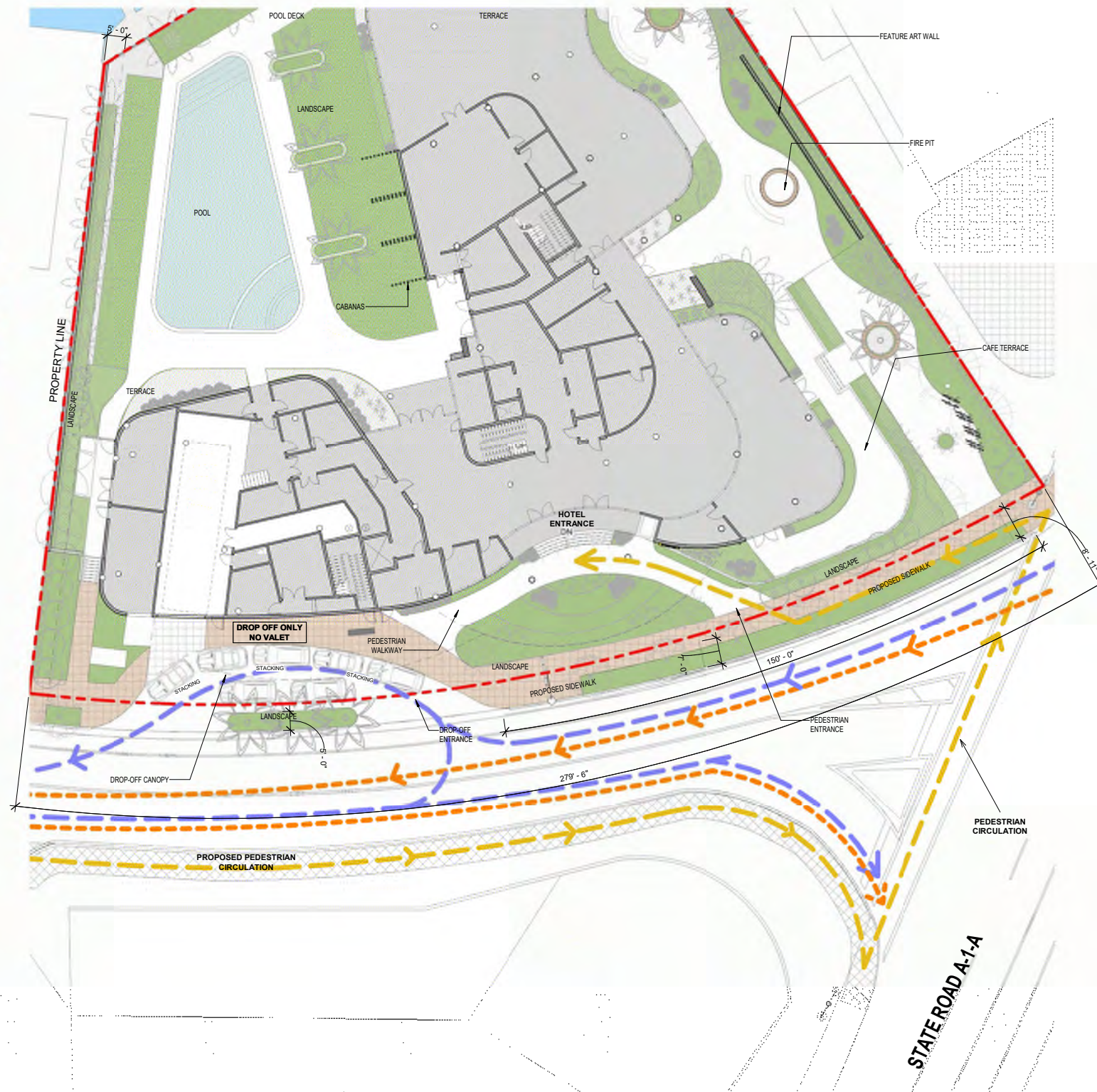
A-0.14

adache
group architects

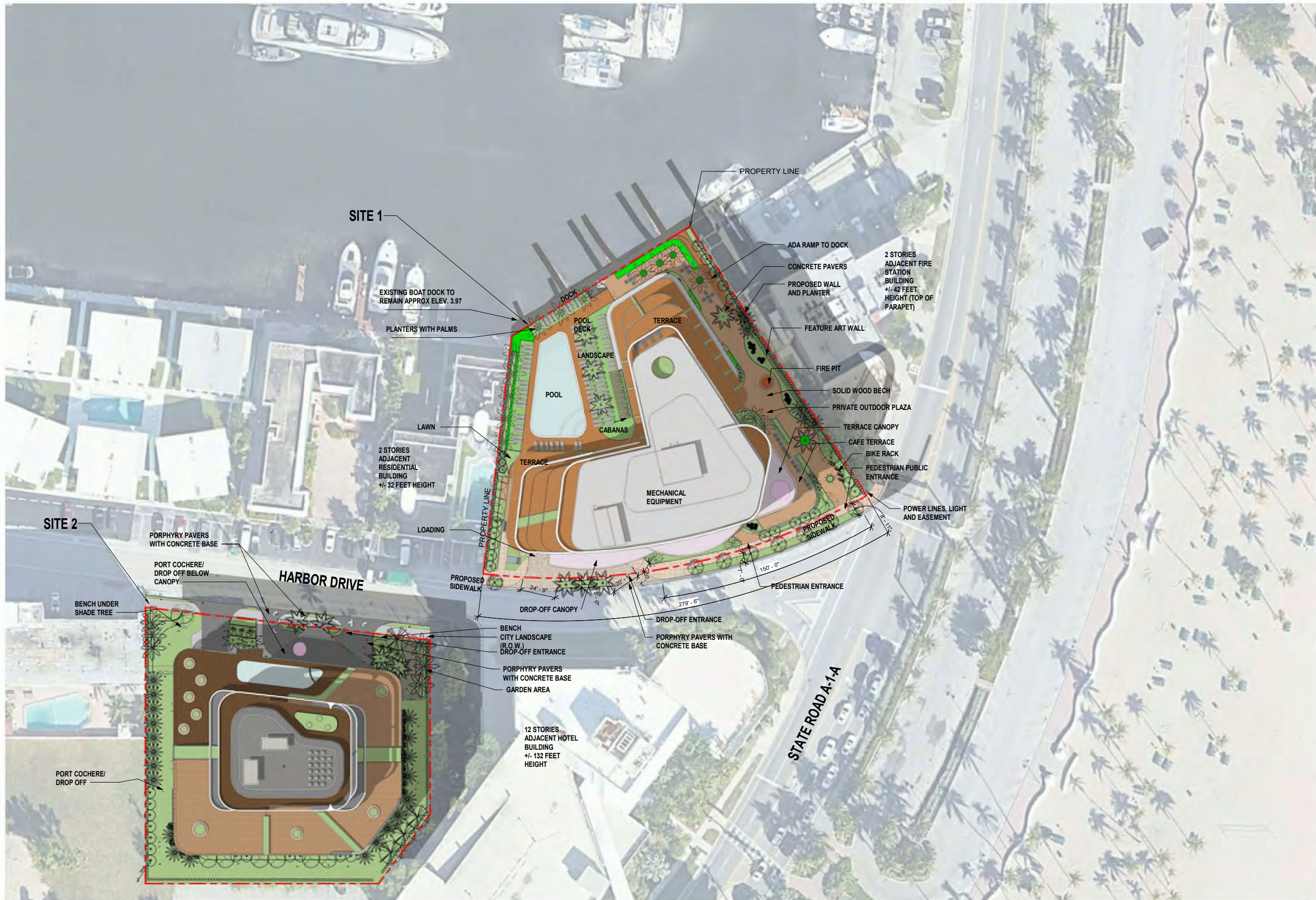




VEHICULAR MOVEMENT DIAGRAM_APARTMENT BUILDING
SCALE: 1/16" = 1'-0"



VEHICULAR MOVEMENT DIAGRAM_HOTEL
SCALE: 1/16" = 1'-0"



 **SITE PLAN-COLORED**
SCALE: 1" = 30'-0"

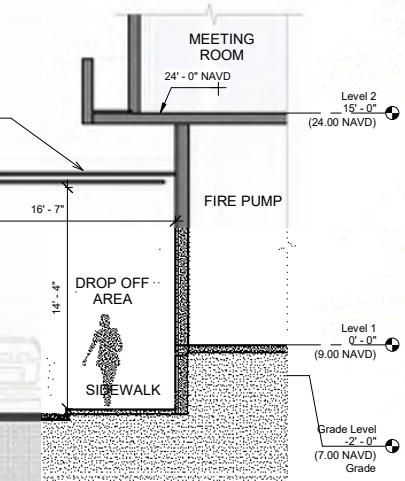
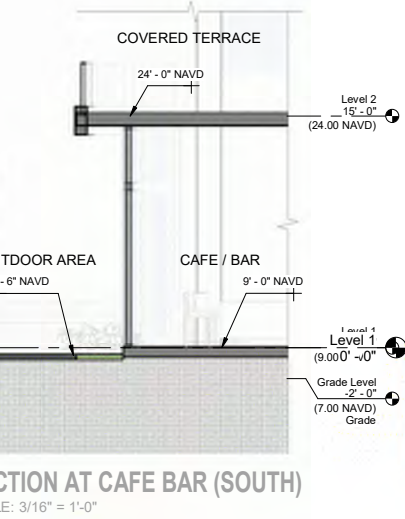
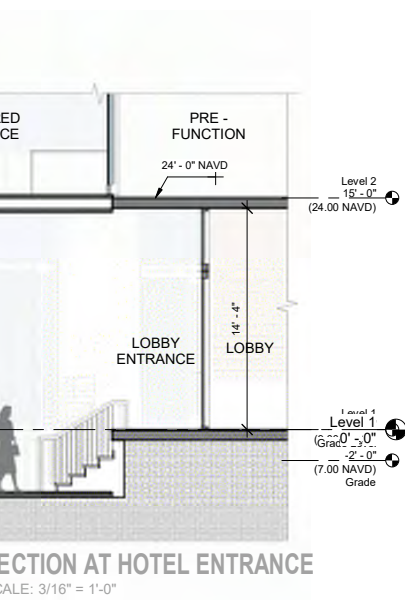
FINAL DRC
74.06A HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERALE, FL 33316

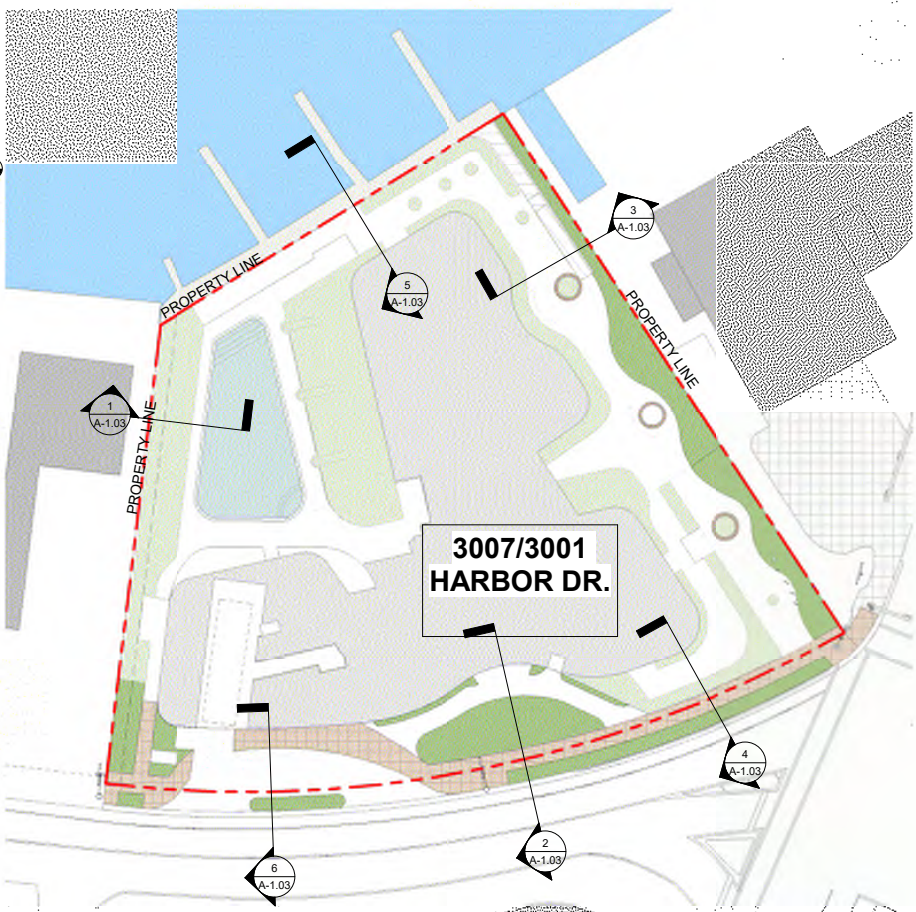
COLORED SITE PLAN

A-1.01

adache
group architects



6 SECTION AT PORTE COCHERE
SCALE: 3/16" = 1'-0"



KEY PLAN
SCALE: 1" = 30'-0"

IONS

A-1.03

adache
group architects



SEE PERVIOUS AND COVERED AREAS STUDY ON SHEET A.1.04

1. THE SITE TO BE EQUIPPED WITH A COMPREHENSIVE CCTV SYSTEM THAT IS CAPABLE OF RETRIEVING AN IDENTIFIABLE IMAGE OF AN INDIVIDUAL ON SITE. THE SYSTEM TO COVER ALL ENTRY-EXIT POINTS, COMMON AREAS, MAINTENANCE STORAGE AREAS AND ANY SENSITIVE AREA OF THE SITE.
2. OFFICE DOORS AND COMMON AREA DOORS TO BE LOCKABLE FROM THE INSIDE TO PROVIDE SAFE SHELTER IN THE CASE OF AN ACTIVE THREAT OR AN ACTIVE KILLER EVENT.
3. GROUND-LEVEL STAIR DOORS TO BE ACCESS ONLY OR ACCESS CONTROLLED. STAIR DOORS INTO FLOORS TO BE ACCESS CONTROLLED.
4. GUEST ENTRY DOORS TO HAVE A SECONDARY DEADBOLT LOCKING SYSTEM.
5. A SYSTEM TO TRACK ANY INDIVIDUAL ACCESS INTO EACH HOTEL ROOM (GUEST, HOUSEKEEPING, OR SERVICE CALLS).
6. ALL RESTRICTED AREAS AND GUEST ONLY AREAS TO BE ACCESS CONTROLLED AND LABEL AS SUCH.
7. ALL ELEVATOR LOBBIES AND/OR ELEVATORS TO BE ACCESS CONTROLLED.
8. ALL GLAZING TO BE IMPACT RESISTANT.
9. EASILY IDENTIFIABLE EMERGENCY COMMUNICATION DEVICES TO BE AVAILABLE AT THE POOL AREAS, COMMON AREAS, LOBBY, AND RESTAURANT.
10. LIGHTING AND LANDSCAPING TO FOLLOW CITY OF FORT LAUDERDALE C.P.T.E.D. GUIDELINES.

SECURITY SYSTEMS

POOL LOUNGE CHAIRS QTY	
Court	QTY
1	51



KEY PLAN HOTEL
SCALE: 1" = 100'-0"

FINAL DRC
74.06A_HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERDALE, FL 33316

FLOOR PLAN HOTEL_LEVEL 1

A-1.3.01H

adache
group architects



FINAL DRC
74.06A_HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERALE, FL 33316

FLOOR PLAN HOTEL_LEVEL 2

A-1.3.02H

adache
group architects



- AREA COLOR LEGEND**
- BALCONY
 - CORRIDOR
 - ELEC/ LV
 - ELEV.
 - ELEV. VEST.
 - GUEST ROOM
 - L.C.
 - MECH.
 - PLANTER
 - ST
 - STAIRS
 - STORAGE
 - TERRACE
 - TR
 - VEND

HOTEL KEYS_LEVEL 3	
UNIT TYPE	#OF KEYS
SD	6
SK	22
# OF KEYS	28

FLOOR PLATE AREA_LEVEL 3	
	15,563 SF

- BK = BRONZE KING
- SD = SILVER DOUBLE
- SK = SILVER KING
- GK = GOLD KING
- GHS = GUEST HOUSE SUITE
- PKL = PLPATINUM KING LOFT
- SS = SILVER SUITE
- GS = GOLD SUITE
- PS = PLATINUM SUITE

ROOM TYPES



KEY PLAN_HOTEL
SCALE: 1" = 160'-0"



AREA COLOR LEGEND

- BALCONY
- CORRIDOR
- ELEC/ LV
- ELEV.
- ELEV. VEST.
- GUEST ROOM
- L.C.
- MECH.
- PLANTER
- ST
- STAIRS
- STORAGE
- TERRACE
- TR
- VEND



HOTEL KEYS_LEVEL 4	
UNIT TYPE	#OF KEYS
SD	5
SK	22
# OF KEYS	27

FLOOR PLATE AREA_LEVEL 4	
	13,778 SF

BK	= BRONZE KING
SD	= SILVER DOUBLE
SK	= SILVER KING
OK	= GOLD KING
GHS	= GUEST HOUSE SUITE
PKL	= PLPATINUM KING LOFT
SS	= SILVER SUITE
GS	= GOLD SUITE
PS	= PLATINUM SUITE

ROOM TYPES



KEY PLAN_HOTEL
SCALE: 1" = 160'-0"

HOTEL_Level 4
SCALE: 1/16" = 1'-0"



FINAL DRC
74.06A_HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERALE, FL 33316

FLOOR PLAN HOTEL_LEVEL 4

A-1.3.04H

adache
group architects



AREA COLOR LEGEND

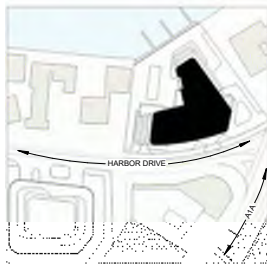
- BALCONY
- CORRIDOR
- ELEC/ LV
- ELEV.
- ELEV. VEST.
- GUEST ROOM
- L.C.
- MECH.
- PLANTER
- ST
- STAIRS
- STORAGE
- TERRACE
- TR
- VEND

HOTEL KEYS_LEVEL 5	
UNIT TYPE	#OF KEYS
SD	6
SK	19
# OF KEYS	25

FLOOR PLATE AREA_LEVEL 5	
	13,222 SF

BK	= BRONZE KING
SD	= SILVER DOUBLE
SK	= SILVER KING
OK	= GOLD KING
GHS	= GUEST HOUSE SUITE
PKL	= PLATINUM KING LOFT
SS	= SILVER SUITE
GS	= GOLD SUITE
PS	= PLATINUM SUITE

ROOM TYPES
SCALE: 12" = 1'-0"



KEY PLAN_HOTEL
SCALE: 1" = 160'-0"

HOTEL_Level 5
SCALE: 1/16" = 1'-0"



FINAL DRC
74.06A_HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERALE, FL 33316

FLOOR PLAN HOTEL_LEVEL 5

A-1.3.05H

adache
group architects

AREA COLOR LEGEND

- BALCONY
- CORRIDOR
- ELEC/ LV
- ELEV.
- ELEV. VEST.
- GUEST ROOM
- L.C.
- MECH.
- PLANTER
- ST
- STAIRS
- STORAGE
- TERRACE
- TR
- VEND



HOTEL KEYS_LEVEL 6	
UNIT TYPE	#OF KEYS
SD	5
SK	17
# OF KEYS	22

FLOOR PLATE AREA_LEVEL 6	
	12,867 SF

BK	= BRONZE KING
SD	= SILVER DOUBLE
SK	= SILVER KING
OK	= GOLD KING
GHS	= GUEST HOUSE SUITE
PKL	= PLATINUM KING LOFT
SS	= SILVER SUITE
GS	= GOLD SUITE
PS	= PLATINUM SUITE

ROOM TYPES
SCALE: 12" = 1'-0"



KEY PLAN HOTEL
SCALE: 1" = 160'-0"

HOTEL_Level 6
SCALE: 1/16" = 1'-0"



FINAL DRC
74.06A_HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERALE, FL 33316

FLOOR PLAN HOTEL_LEVEL 6

A-1.3.06H

adache
group architects



AREA COLOR LEGEND

- BALCONY
- CORRIDOR
- ELEC/LV
- ELEV.
- ELEV. VEST.
- GUEST ROOM
- L.C.
- MECH.
- PLANTER
- ST
- STAIRS
- STORAGE
- TERRACE
- TR
- VEND

HOTEL KEYS_LEVEL 7	
UNIT TYPE	#OF KEYS
SD	5
SK	17
# OF KEYS	22

FLOOR PLATE AREA_LEVEL 7	
FLOOR PLATE AREA CALCULATION NOT INCLUDING BALCONIES OR TERRACES	12,789 SF

- BK = BRONZE KING
- SD = SILVER DOUBLE
- SK = SILVER KING
- OK = GOLD KING
- GHS = GUEST HOUSE SUITE
- PKL = PLATINUM KING LOFT
- SS = SILVER SUITE
- GS = GOLD SUITE
- PS = PLATINUM SUITE

ROOM TYPES



HOTEL_Level 7
SCALE: 1/16" = 1'-0"

1/16" = 1' - 0"
0 16 32 48 64 FEET

FINAL DRC
74.06A_HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERALE, FL 33316

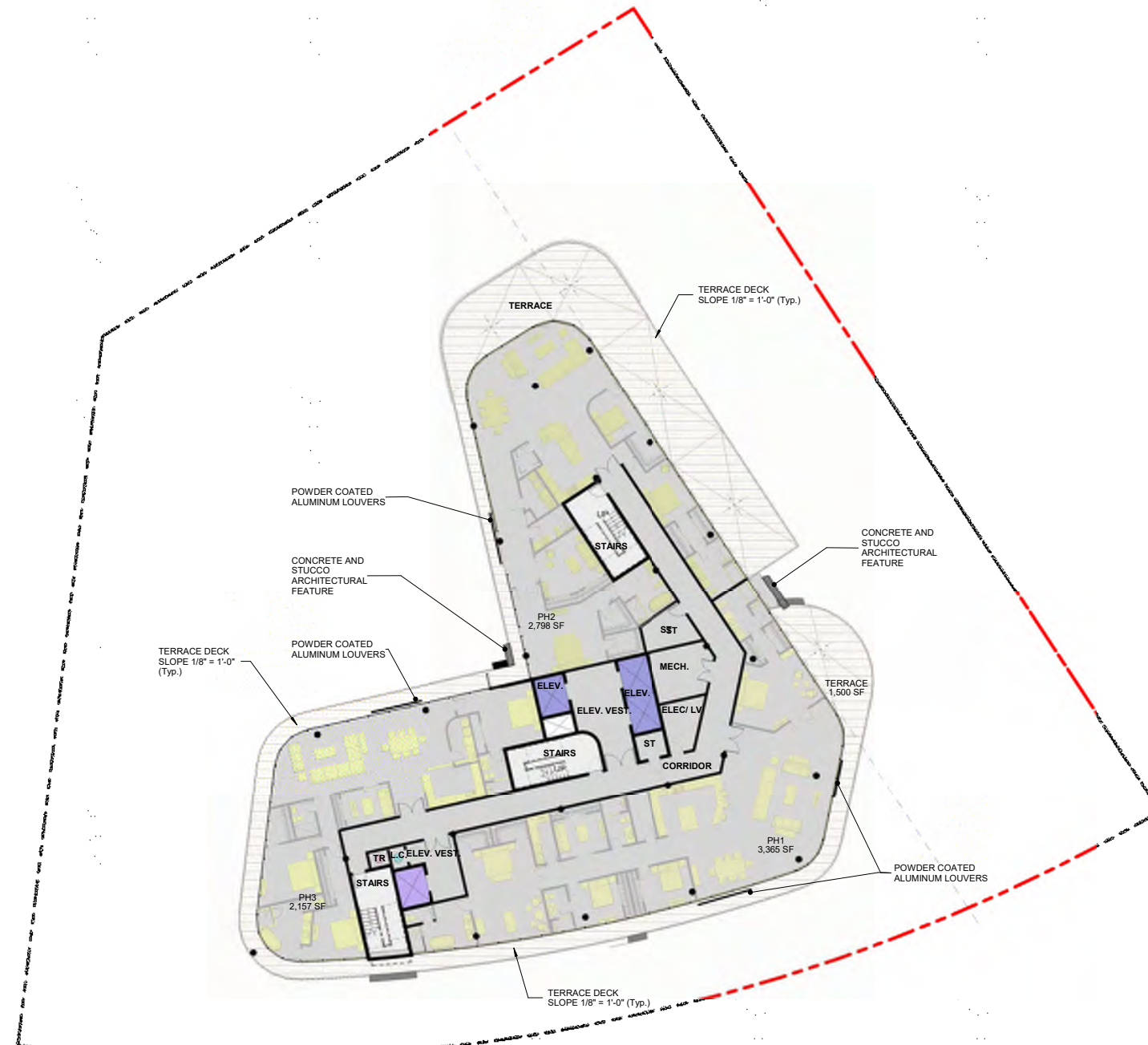
FLOOR PLAN HOTEL_LEVEL 7

A-1.3.07H

adache
group architects

AREA COLOR LEGEND

- CORRIDOR
- ELEC/ LV
- ELEV.
- ELEV. VEST.
- L.C.
- MECH.
- PH1
- PH2
- PH3
- S.E.
- ST
- STAIRS
- TERRACE
- TR



Residential units to comply with Fair Housing Provisions per FBC Accessibility Code

HOTEL TOTAL_RESIDENTIAL_UNIT COUNT
6

FLOOR PLATE AREA_LEVEL 8
FLOOR PLATE AREA CALCULATION NOT INCLUDING BALCONIES OR TERRACES 11,257 SF

BK = BRONZE KING
SD = SILVER DOUBLE
SK = SILVER KING
OK = GOLD KING
GHS = GUEST HOUSE SUITE
PKL = PLATINUM KING LOFT
SS = SILVER SUITE
GS = GOLD SUITE
PS = PLATINUM SUITE

ROOM TYPES



KEY PLAN_HOTEL
SCALE: 1" = 160'-0"

1/16" = 1' - 0"
0 16 32 48 64 FEET

HOTEL_Level 8
SCALE: 1/16" = 1'-0"

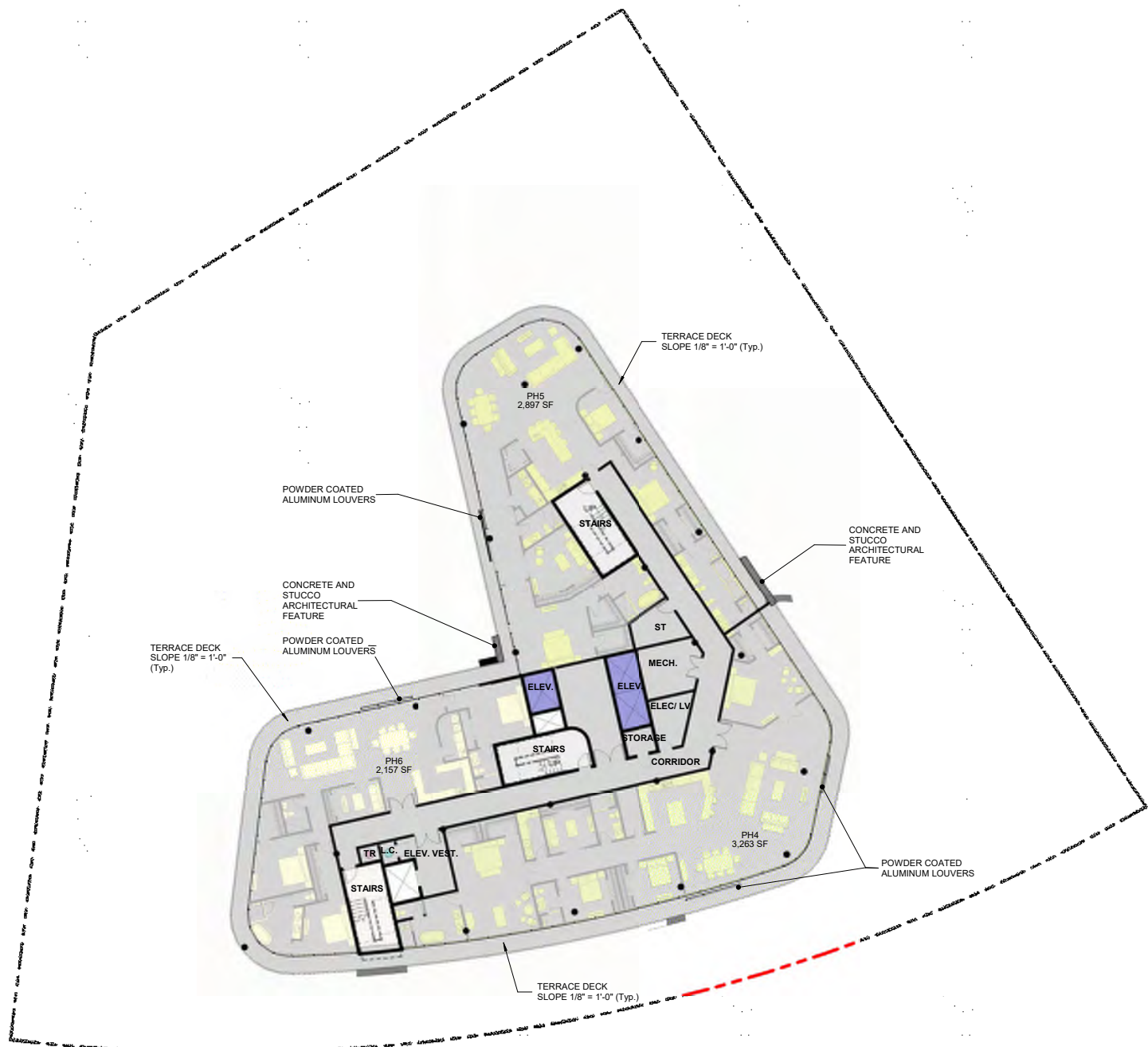
FINAL DRC
74.06A_HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERALE, FL 33316

FLOOR PLAN HOTEL_LEVEL 8

A-1.3.08H

adache
group architects



AREA COLOR LEGEND

- BALCONY
- CORRIDOR
- ELEC/ LV
- ELEV.
- ELEV. VEST.
- L.C.
- MECH.
- PH4
- PH5
- PH6
- ST
- STAIRS
- STORAGE
- TR

Residential units to comply with Fair Housing Provisions per FBC Accessibility Code

HOTEL TOTAL RESIDENTIAL UNIT COUNT
6

FLOOR PLATE AREA LEVEL 9
FLOOR PLATE AREA CALCULATION NOT INCLUDING BALCONIES OR TERRACES
12,703 SF

- BK = BRONZE KING
- SD = SILVER DOUBLE
- SK = SILVER KING
- GK = GOLD KING
- GHS = GUEST HOUSE SUITE
- PKL = PLPATINUM KING LOFT
- SS = SILVER SUITE
- GS = GOLD SUITE
- PS = PLATINUM SUITE

ROOM TYPES



KEY PLAN HOTEL
SCALE: 1" = 160'-0"

HOTEL_Level 9
SCALE: 1/16" = 1'-0"



FINAL DRC
74.06A_HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERALE, FL 33316

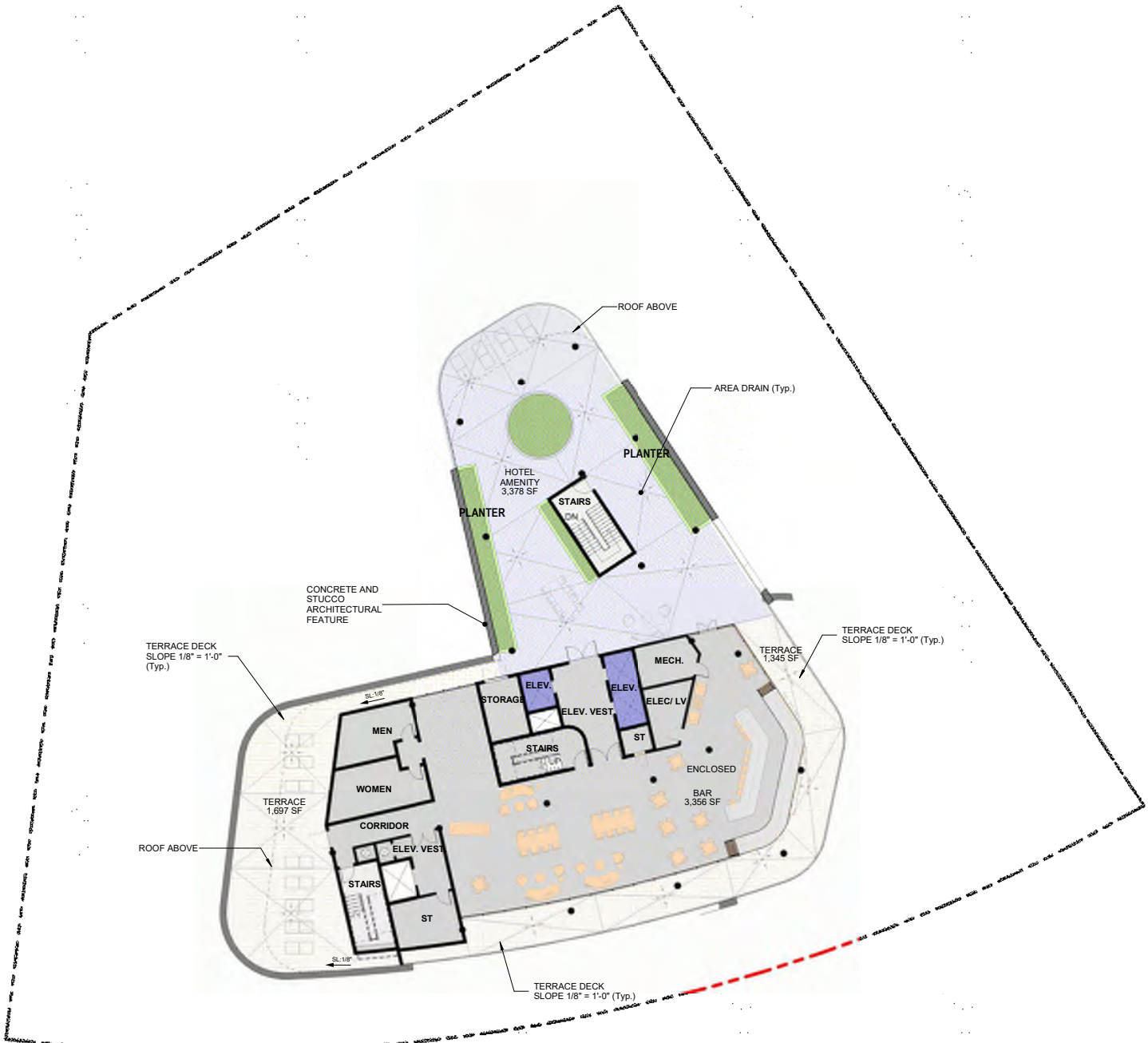
FLOOR PLAN HOTEL_LEVEL 9

A-1.3.09H

adache
group architects

AREA COLOR LEGEND

- BAR
- CORRIDOR
- ELEC/ LV
- ELEV.
- ELEV. VEST.
- HOTEL AMENITY
- MECH.
- MEN
- PLANTER
- ST
- STAIRS
- STORAGE
- TERRACE
- WOMEN



FLOOR PLATE AREA_LEVEL 10	
FLOOR PLATE AREA CALCULATION NOT INCLUDING BALCONIES OR TERRACES	6,506 SF



KEY PLAN HOTEL
SCALE: 1" = 160'-0"

HOTEL_Level 10
SCALE: 1/16" = 1'-0"



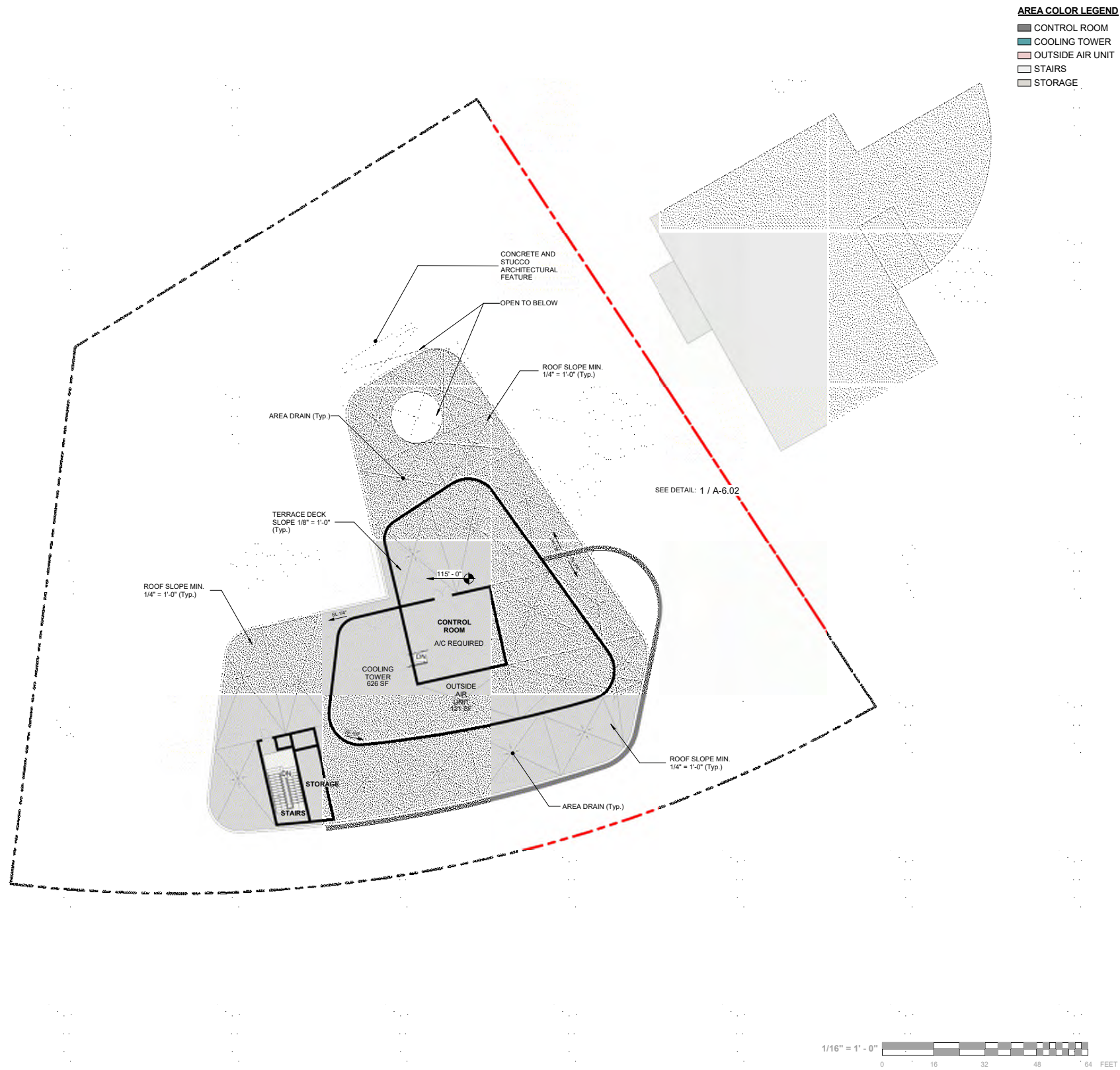
FINAL DRC
74.06A_HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERALE, FL 33316

FLOOR PLAN HOTEL_LEVEL 10

A-1.3.10H

adache
group architects



FLOOR PLATE AREA_LEVEL 11	
FLOOR PLATE AREA CALCULATION NOT INCLUDING BALCONIES OR TERRACES)	1,064 SF



ROOF PLAN
SCALE: 1/16" = 1'-0"

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERALE, FL 33316

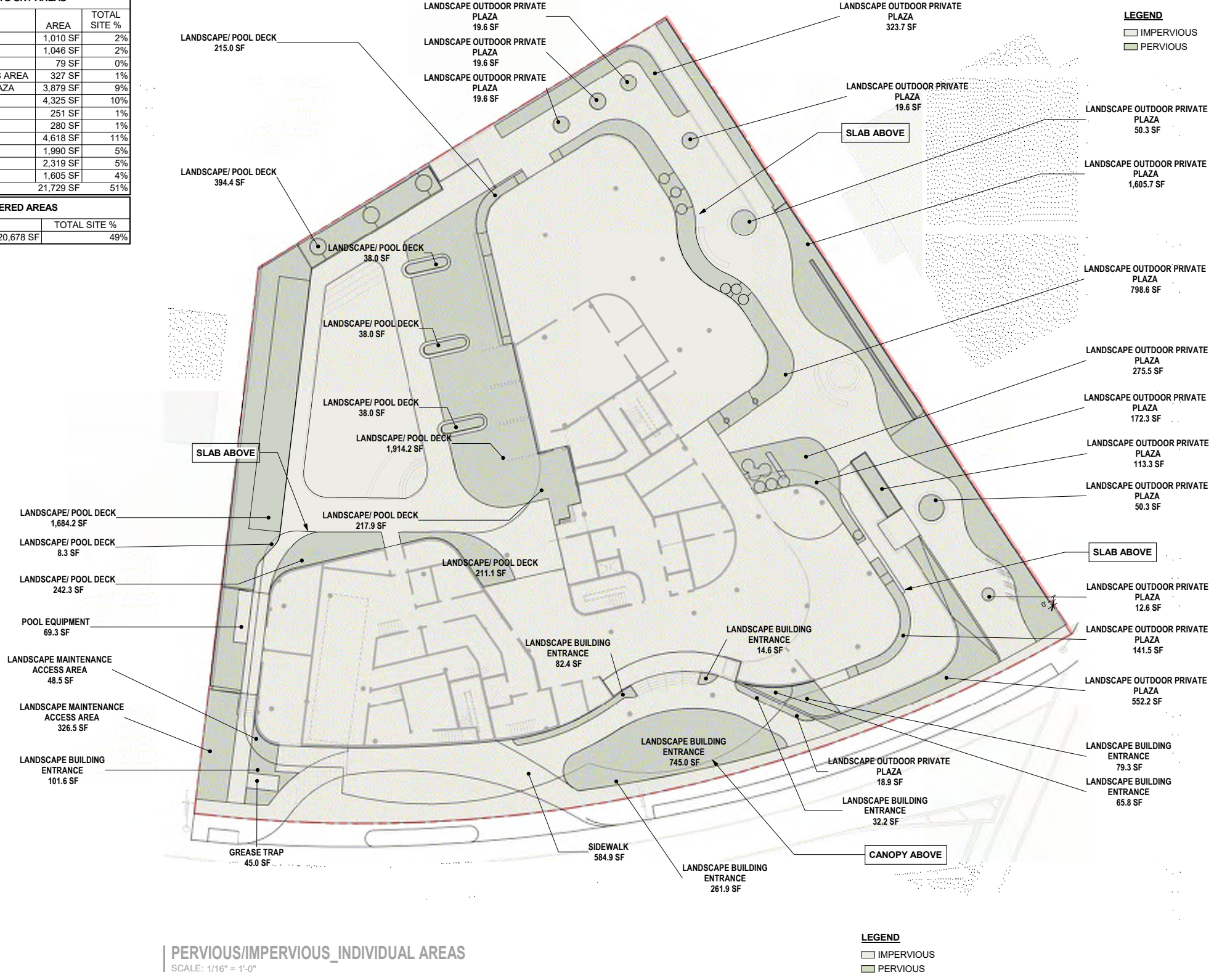
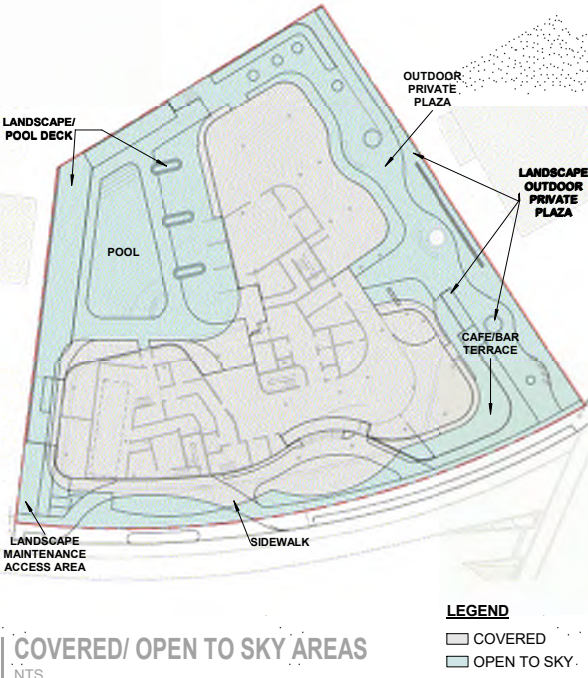
FLOOR PLAN HOTEL _ROOF PLAN

A-1.3.11H

adache
group architects

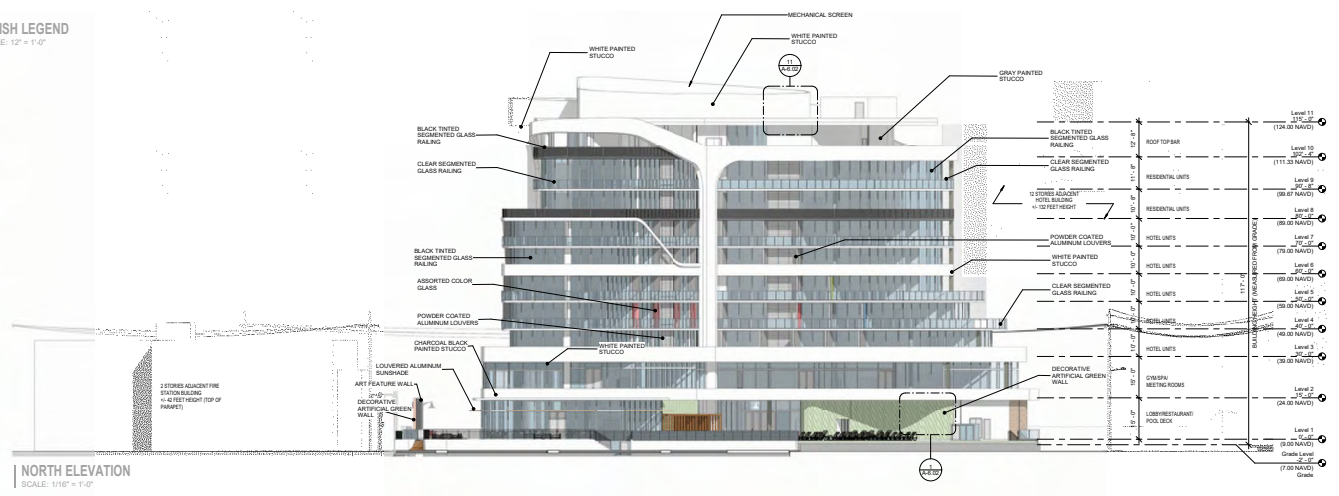
SITE STUDIES - PERVIOUS INDIVIDUAL AREAS			
	NAME	AREA	% OF LOT AREA
COVERED	LANDSCAPE BUILDING ENTRANCE	65.8 SF	0.15%
	LANDSCAPE BUILDING ENTRANCE	745.0 SF	1.75%
	LANDSCAPE BUILDING ENTRANCE	14.6 SF	0.03%
	LANDSCAPE BUILDING ENTRANCE	82.4 SF	0.19%
	LANDSCAPE BUILDING ENTRANCE	32.2 SF	0.08%
	LANDSCAPE MAINTENANCE ACCESS AREA	48.5 SF	0.11%
	LANDSCAPE OUTDOOR PRIVATE PLAZA	141.5 SF	0.33%
	LANDSCAPE OUTDOOR PRIVATE PLAZA	172.3 SF	0.40%
	LANDSCAPE/ POOL DECK	217.9 SF	0.51%
	LANDSCAPE/ POOL DECK	242.3 SF	0.57%
	LANDSCAPE/ POOL DECK	211.1 SF	0.50%
	LANDSCAPE/ POOL DECK	215.0 SF	0.50%
	LANDSCAPE/ POOL DECK	8.3 SF	0.02%
	OPEN TO SKY	LANDSCAPE BUILDING ENTRANCE	79.3 SF
LANDSCAPE BUILDING ENTRANCE		261.9 SF	0.61%
LANDSCAPE BUILDING ENTRANCE		101.6 SF	0.24%
LANDSCAPE MAINTENANCE ACCESS AREA		326.5 SF	0.77%
LANDSCAPE OUTDOOR PRIVATE PLAZA		552.2 SF	1.30%
LANDSCAPE OUTDOOR PRIVATE PLAZA		275.5 SF	0.65%
LANDSCAPE OUTDOOR PRIVATE PLAZA		1,605.7 SF	3.77%
LANDSCAPE OUTDOOR PRIVATE PLAZA		50.3 SF	0.12%
LANDSCAPE OUTDOOR PRIVATE PLAZA		19.6 SF	0.05%
LANDSCAPE OUTDOOR PRIVATE PLAZA		19.6 SF	0.05%
LANDSCAPE OUTDOOR PRIVATE PLAZA		50.3 SF	0.12%
LANDSCAPE OUTDOOR PRIVATE PLAZA		113.3 SF	0.27%
LANDSCAPE OUTDOOR PRIVATE PLAZA		12.6 SF	0.03%
LANDSCAPE OUTDOOR PRIVATE PLAZA		323.7 SF	0.76%
LANDSCAPE OUTDOOR PRIVATE PLAZA		19.6 SF	0.05%
LANDSCAPE OUTDOOR PRIVATE PLAZA		19.6 SF	0.05%
LANDSCAPE OUTDOOR PRIVATE PLAZA		798.6 SF	1.87%
LANDSCAPE OUTDOOR PRIVATE PLAZA		18.9 SF	0.04%
LANDSCAPE/ POOL DECK		38.0 SF	0.09%
LANDSCAPE/ POOL DECK		1,684.2 SF	3.95%
LANDSCAPE/ POOL DECK		394.4 SF	0.93%
LANDSCAPE/ POOL DECK		38.0 SF	0.09%
LANDSCAPE/ POOL DECK	38.0 SF	0.09%	
LANDSCAPE/ POOL DECK	1,914.2 SF	4.49%	
	10,952.5 SF	25.69%	

SITE STUDIES - OPEN TO SKY AREAS		
NAME	AREA	TOTAL SITE %
CAFE/BAR TERRACE	1,010 SF	2%
DRIVEWAY	1,046 SF	2%
LANDSCAPE BUILDING ENTRANCE	79 SF	0%
LANDSCAPE MAINTENANCE ACCESS AREA	327 SF	1%
LANDSCAPE OUTDOOR PRIVATE PLAZA	3,879 SF	9%
LANDSCAPE/ POOL DECK	4,325 SF	10%
LOBBY ENTRANCE	251 SF	1%
OUTDOOR AREA	280 SF	1%
OUTDOOR PRIVATE PLAZA	4,618 SF	11%
POOL	1,990 SF	5%
POOL DECK	2,319 SF	5%
SIDEWALK	1,605 SF	4%
	21,729 SF	51%
SITE STUDIES - COVERED AREAS		
AREA	TOTAL SITE %	
	20,678 SF	49%

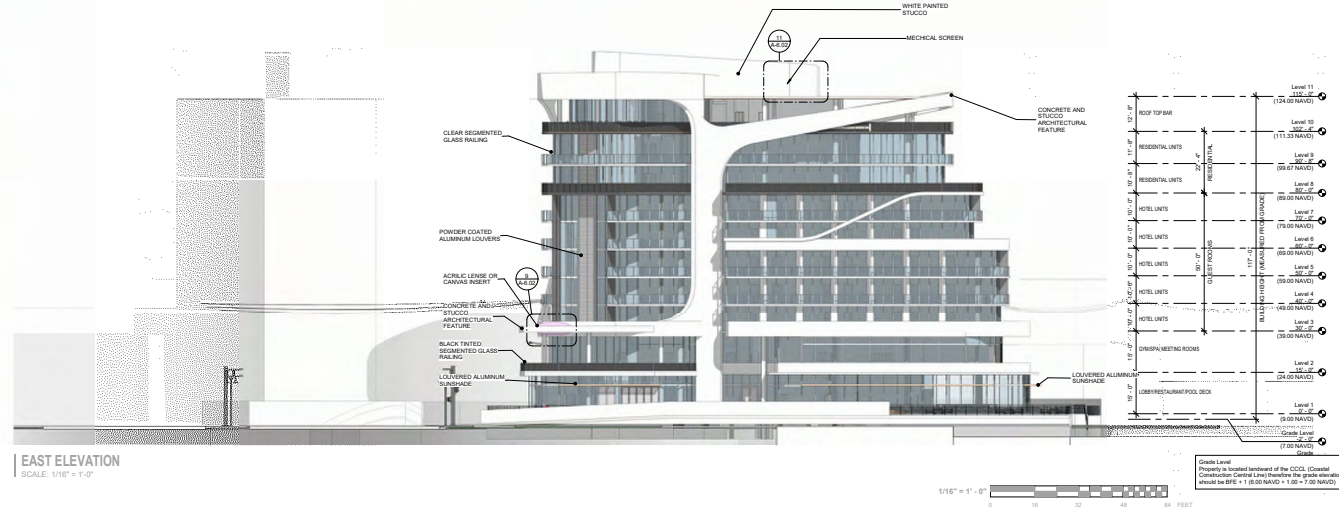




FINISH LEGEND
SCALE: 12" = 1'-0"



NORTH ELEVATION
SCALE: 1/8" = 1'-0"



EAST ELEVATION
SCALE: 1/8" = 1'-0"

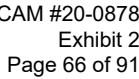
FINAL DRC
7/25/2023
10/23/2023

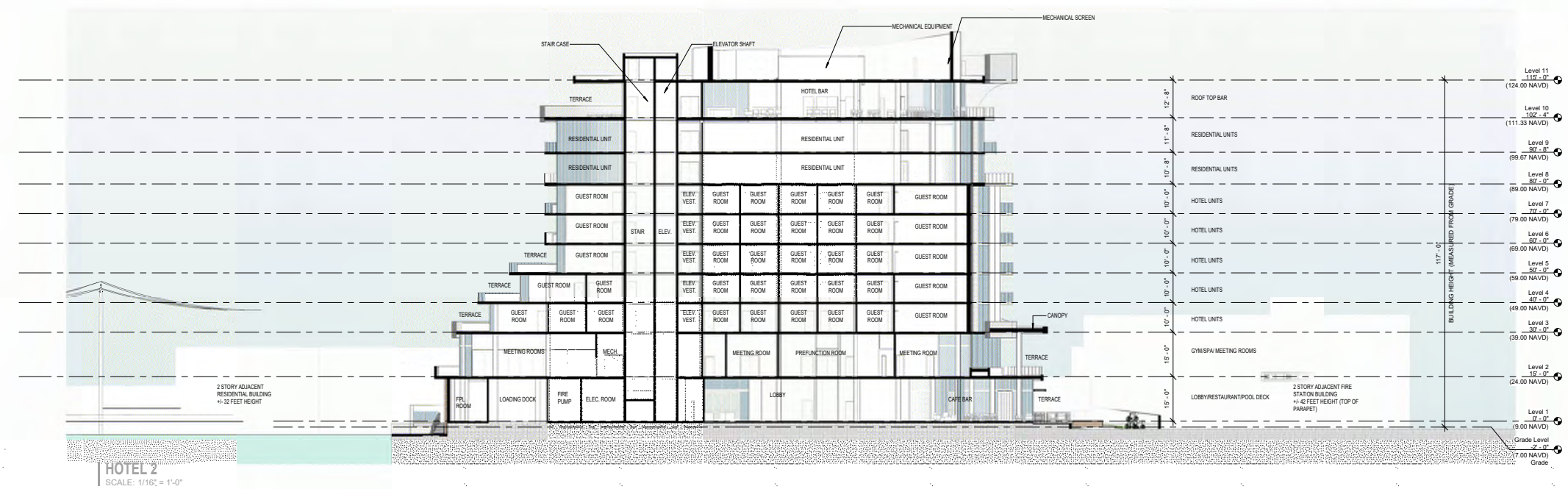
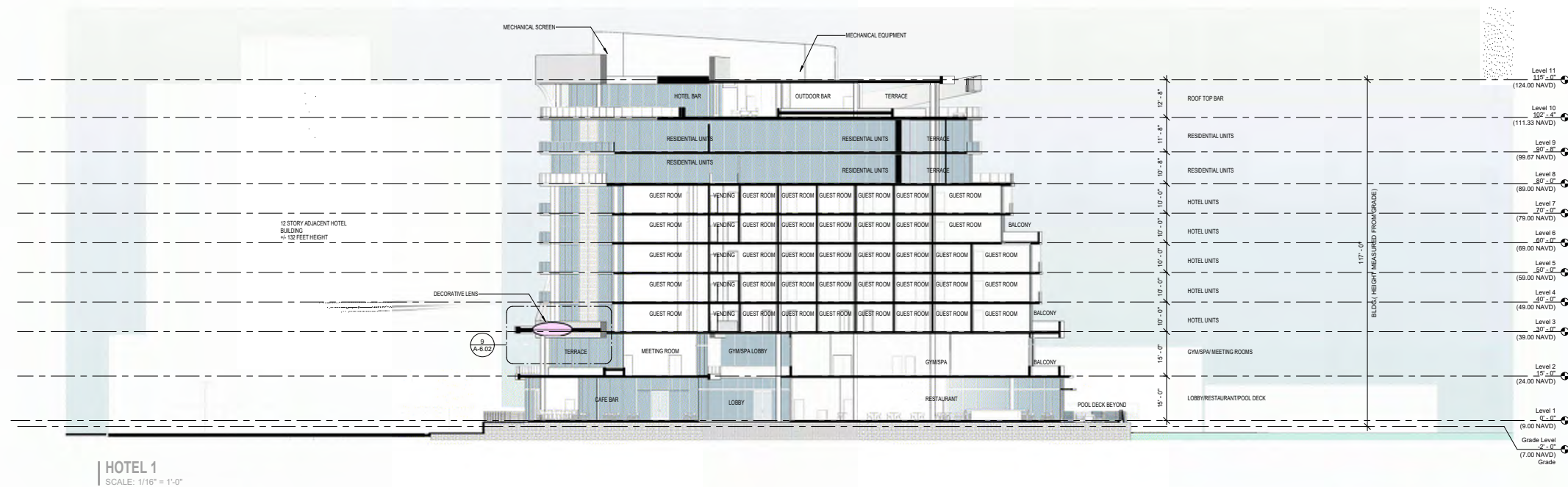
BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERDALE, FL 33316

ELEVATIONS-HOTEL

A-1.5.01H

adache
group architects







AERIAL VIEW_NORTH EAST



AERIAL VIEW_NORTH WEST



AERIAL VIEW_SOUTH EAST



AERIAL VIEW_SOUTH WEST

FINAL DRC
74.06A HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERDALE, FL 33316

3D AERIAL VIEWS

A-4.01

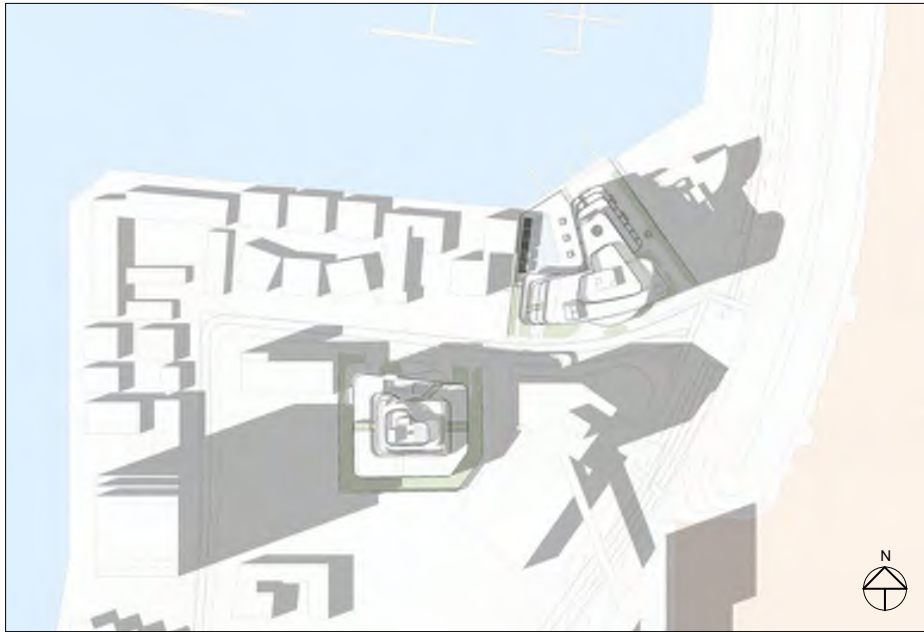
adache
group architects



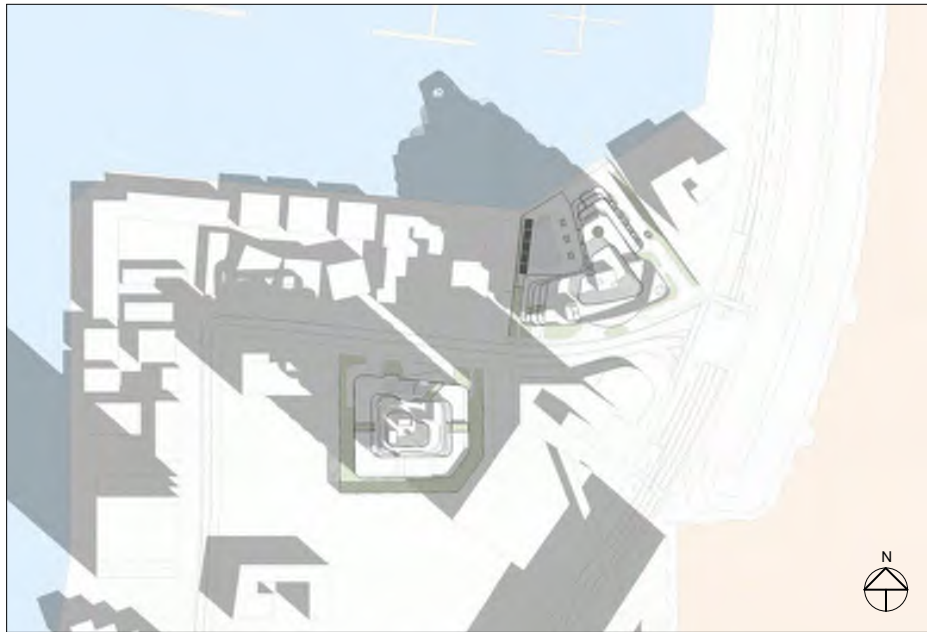
SPRING EQUINOX (MARCH 20) 9:00 AM



SPRING EQUINOX (MARCH 20) 12:00 PM



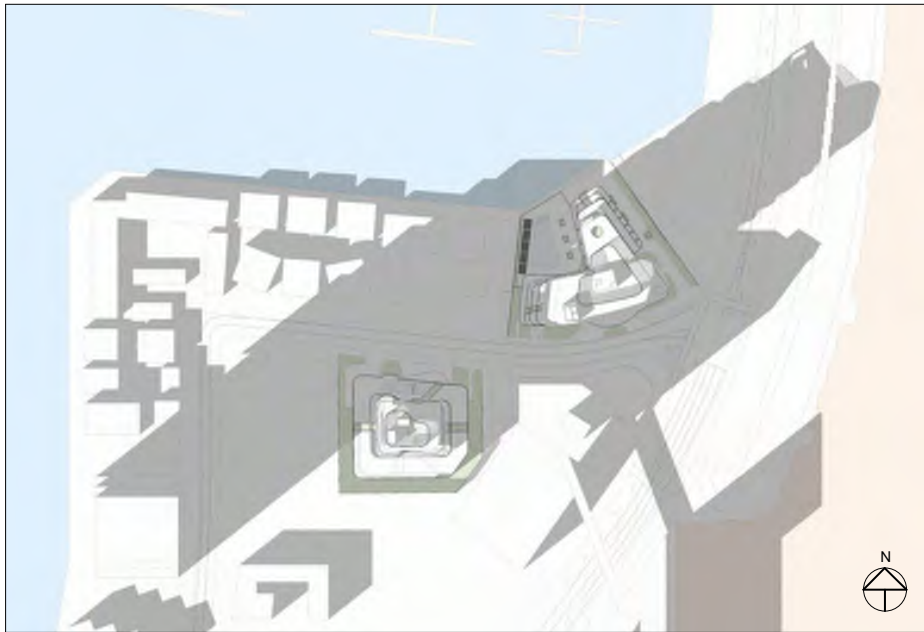
SPRING EQUINOX (MARCH 20) 4:00 PM



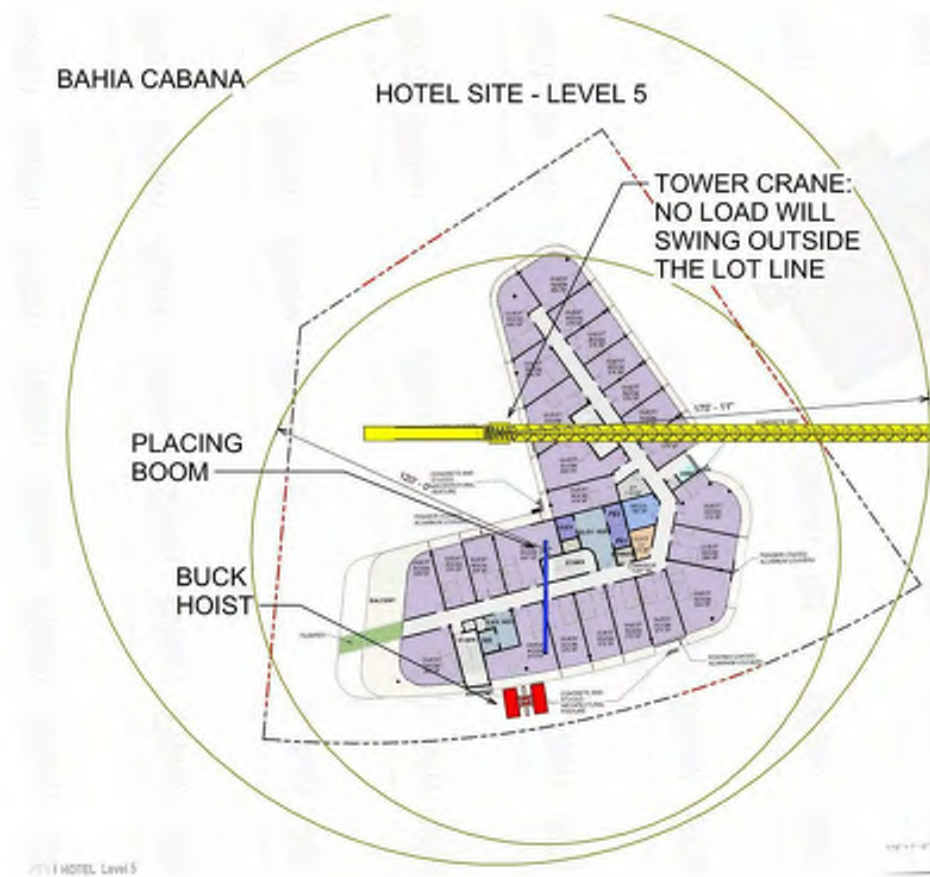
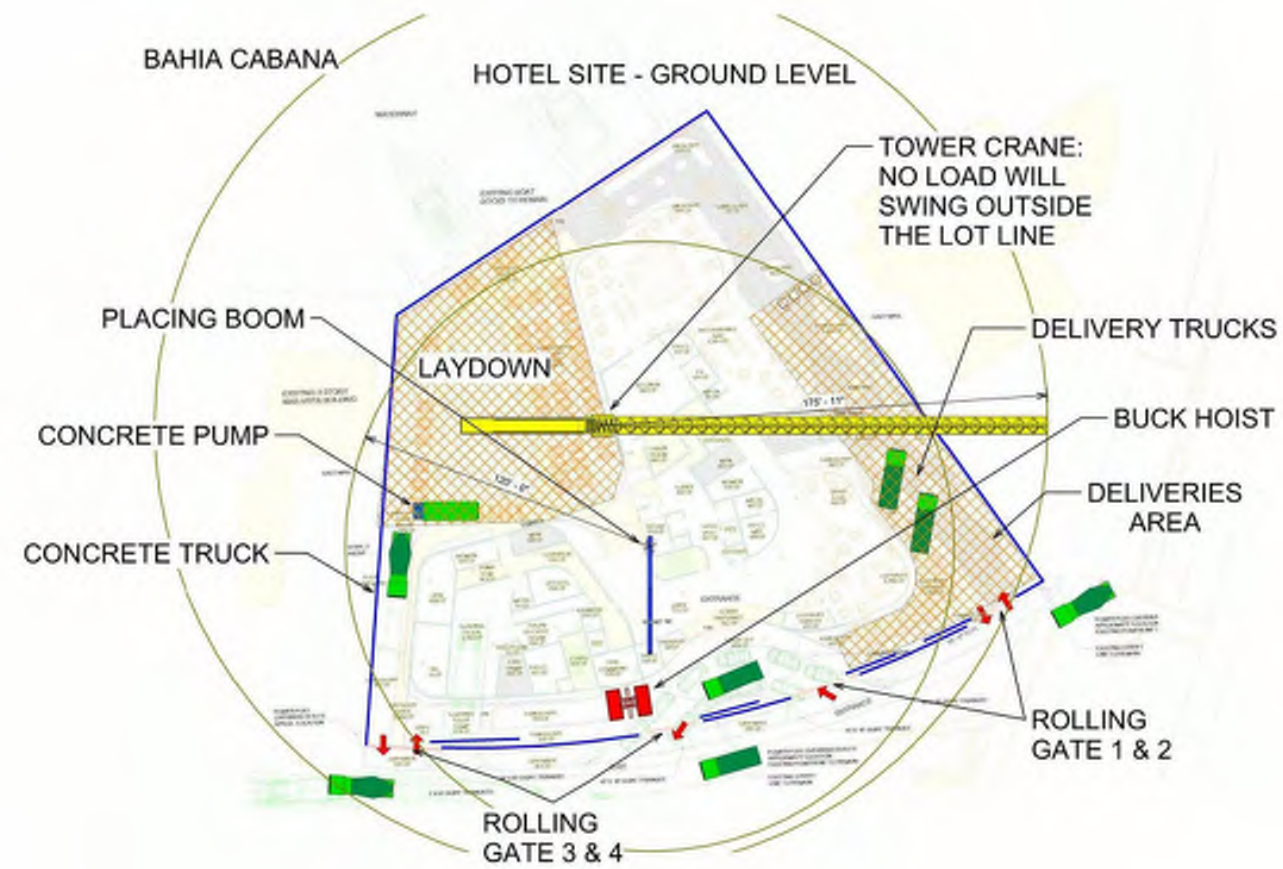
WINTER SOLSTICE (DECEMBER 21) 9:00 AM



WINTER SOLSTICE (DECEMBER 21) 12:00 PM

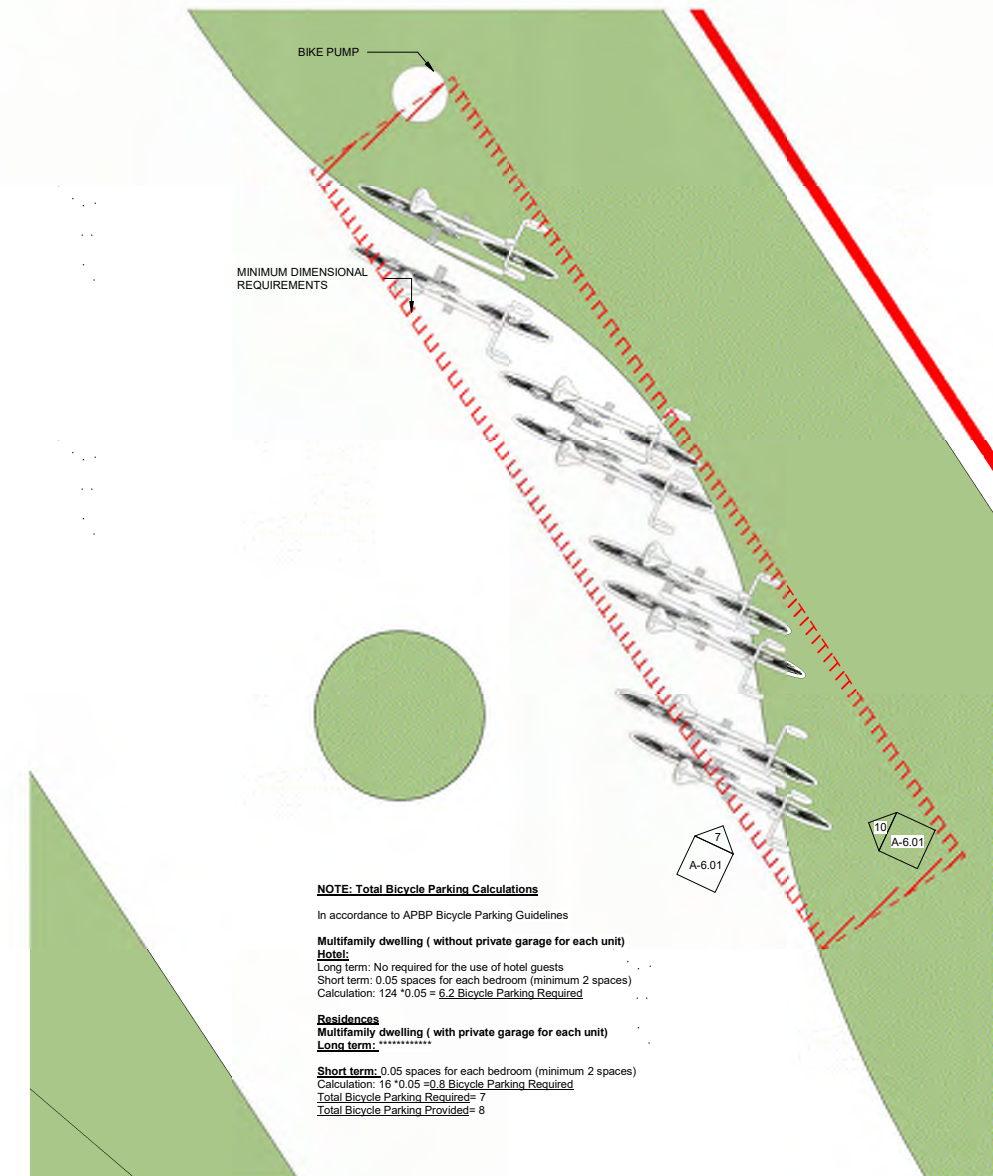


WINTER SOLSTICE (DECEMBER 21) 4:00 PM



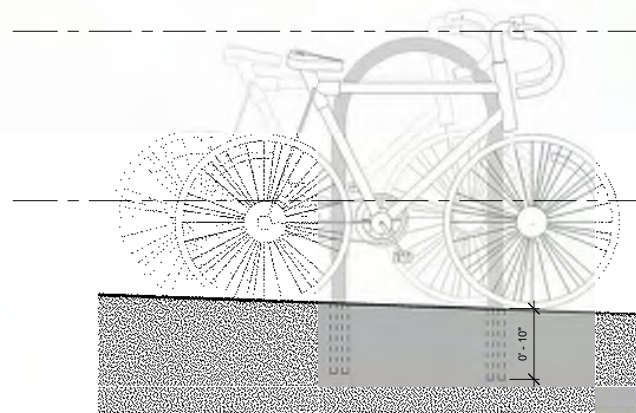


CONCEPT IMAGES

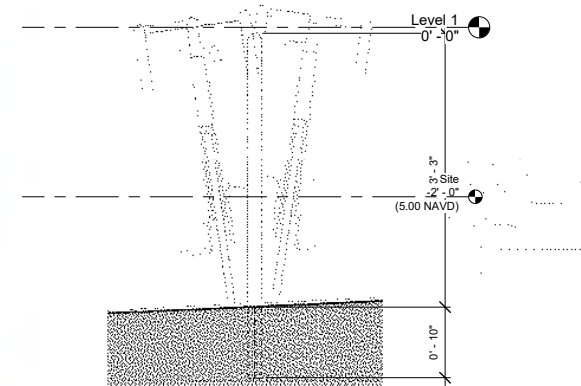


9 | BIKE RACK - PLAN
SCALE: 1/2" = 1'-0"

BIKE RACKS

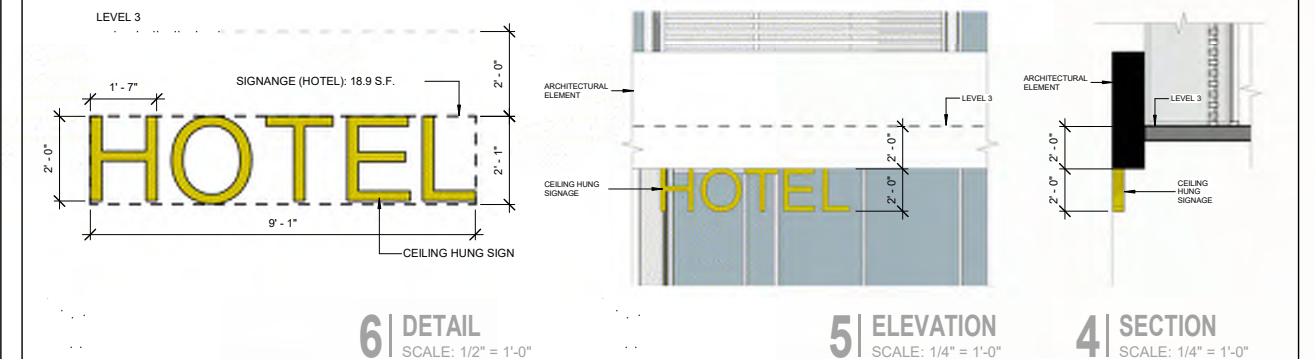
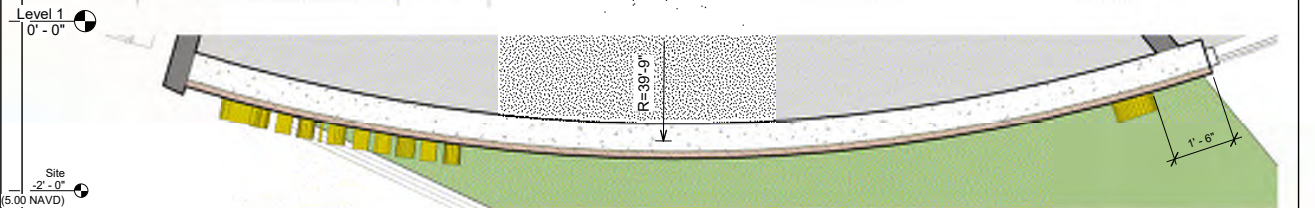
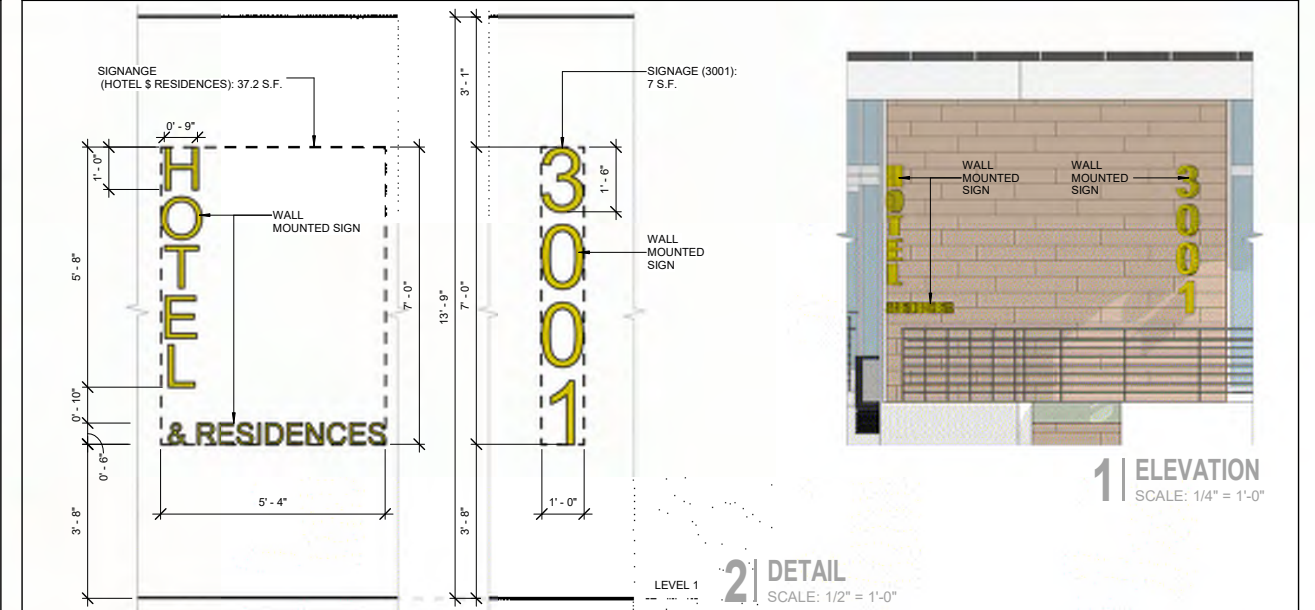


7 | ELEVATION
SCALE: 1" = 1'-0"

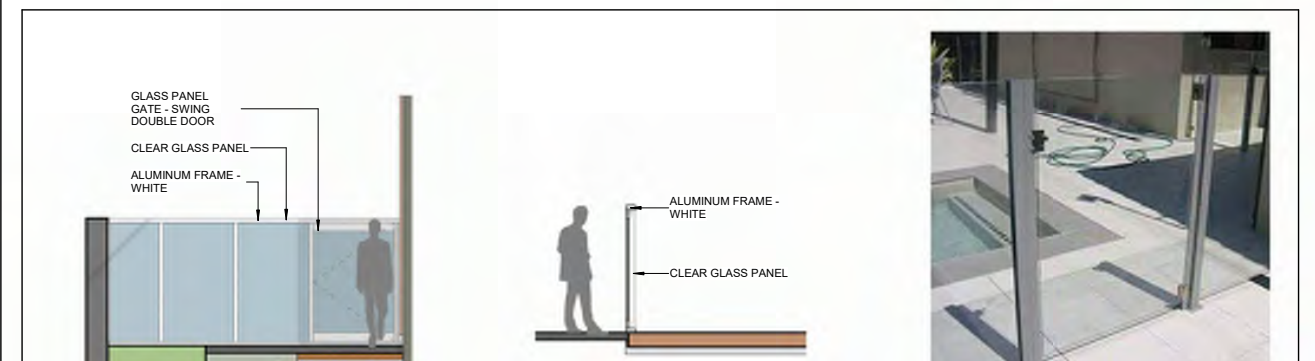


10 | ELEVATION
SCALE: 1" = 1'-0"

DETAILS



SIGNAGE



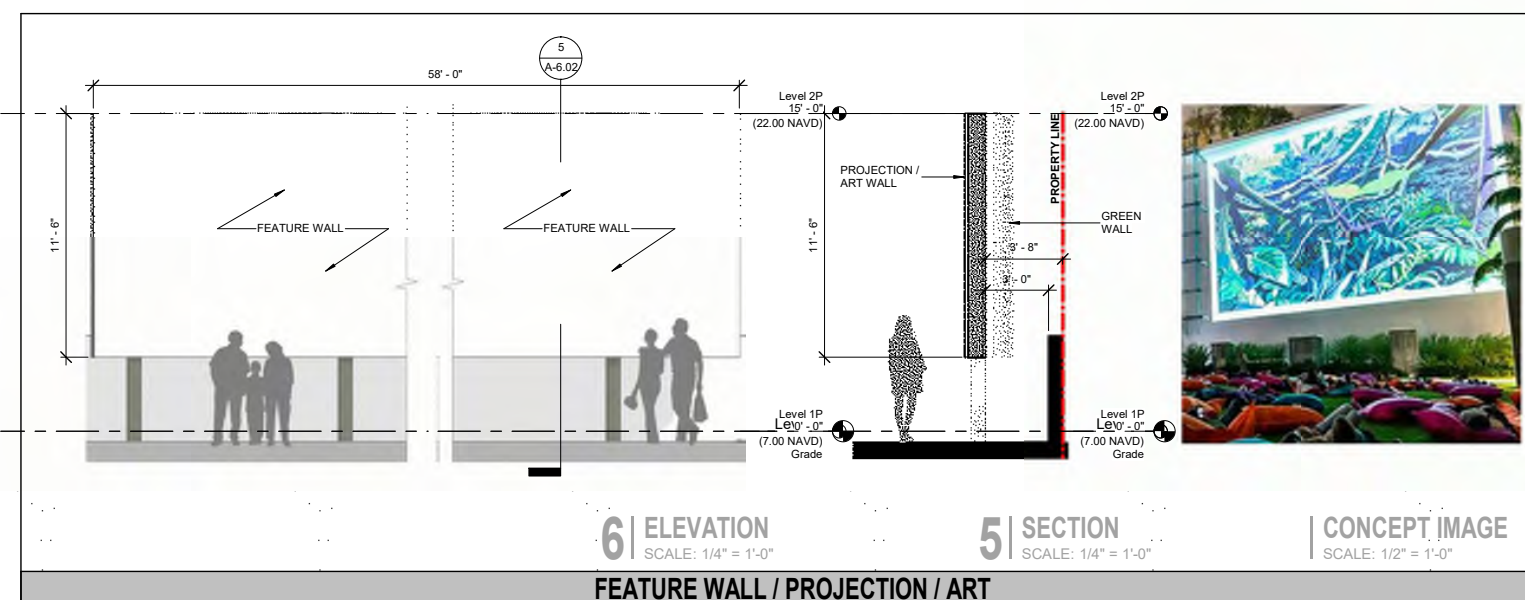
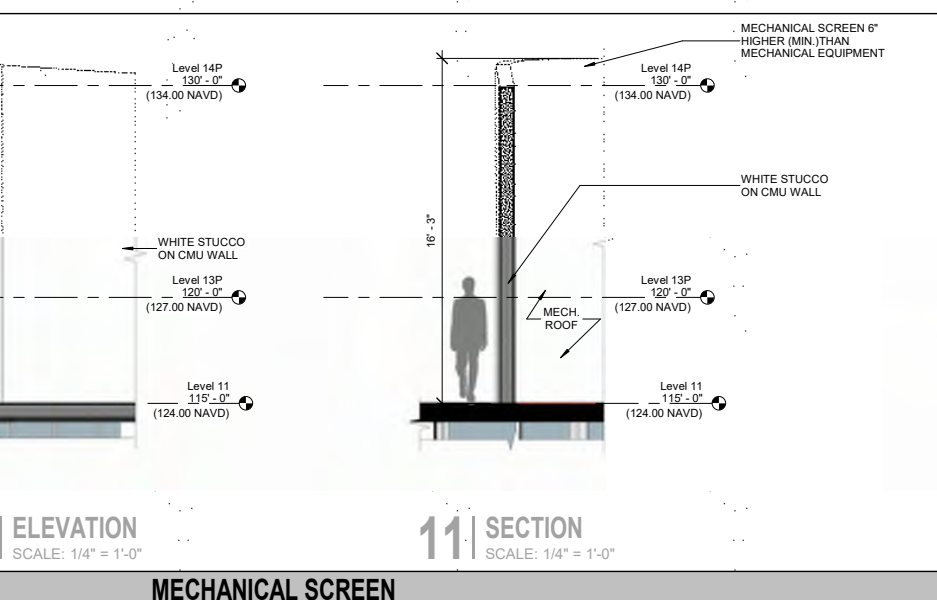
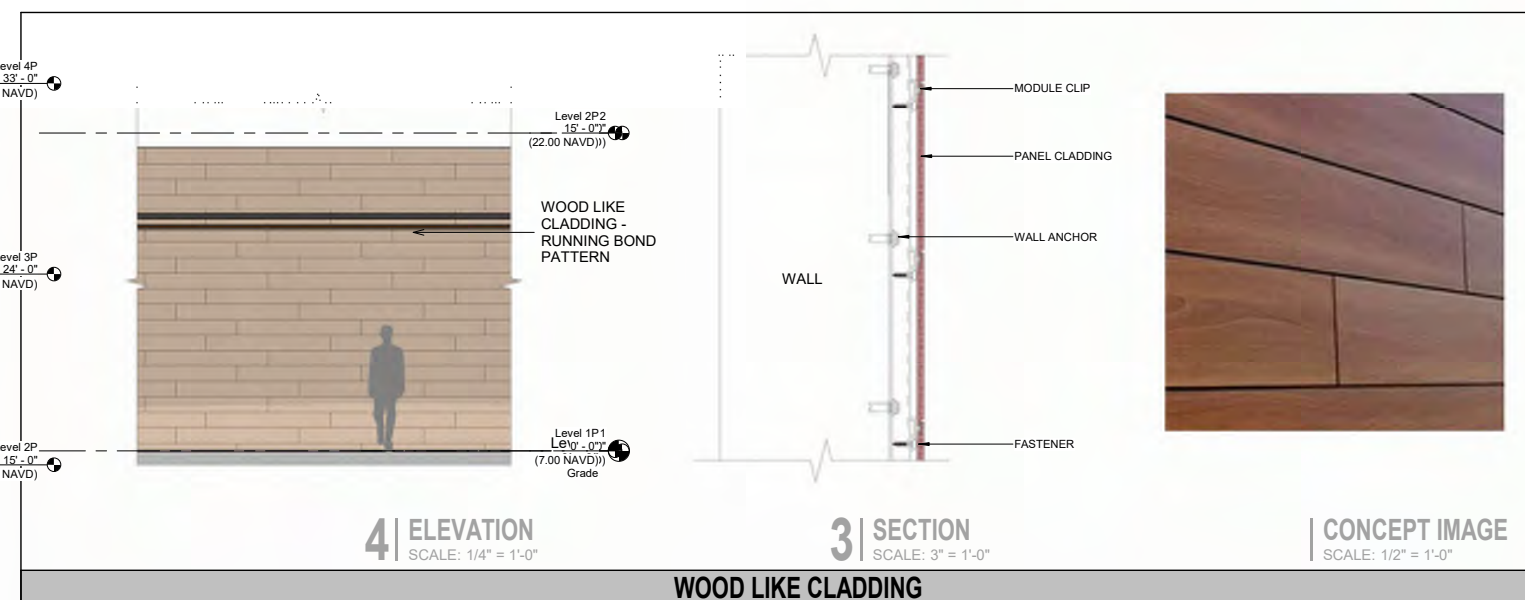
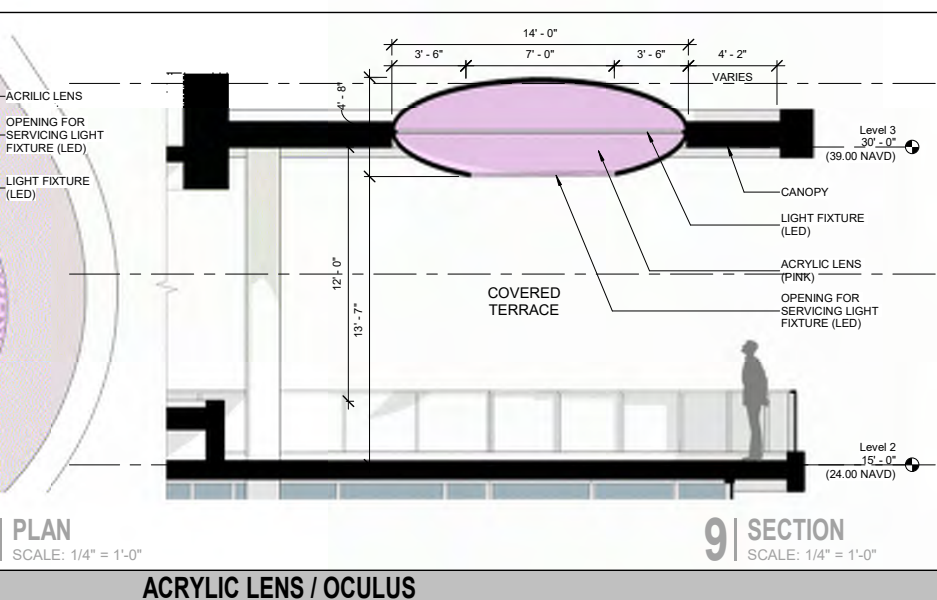
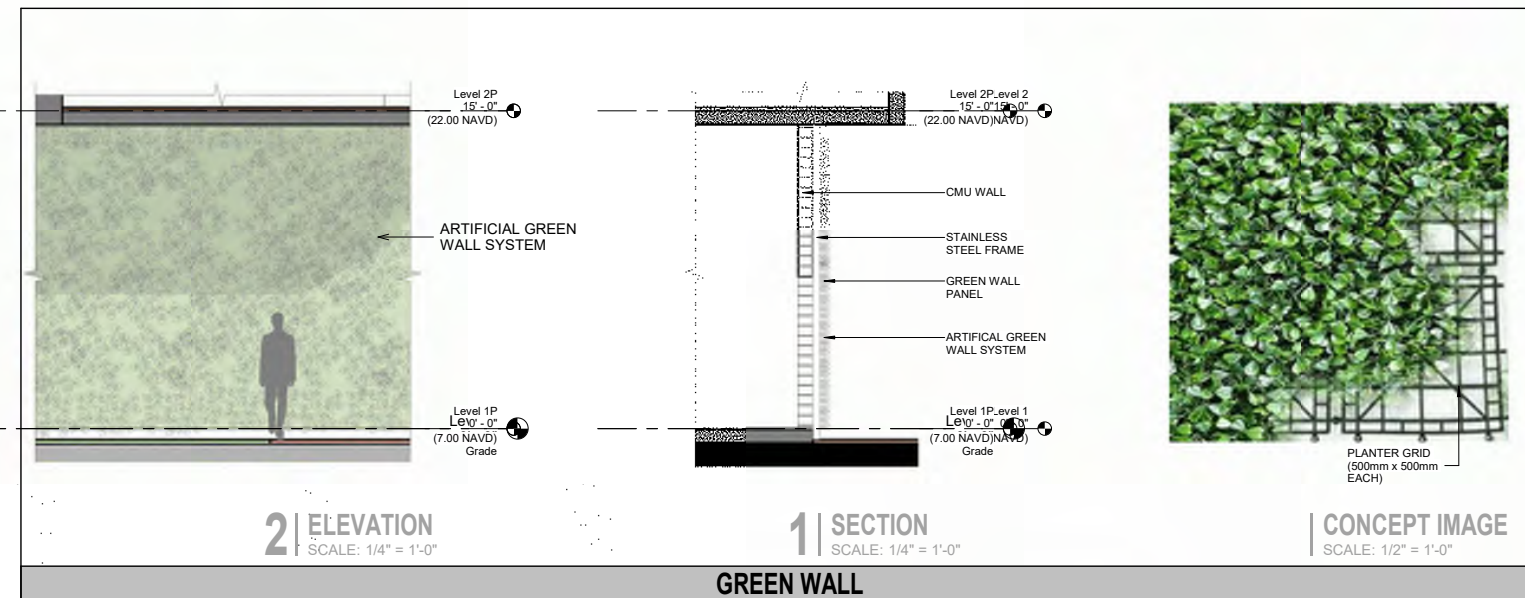
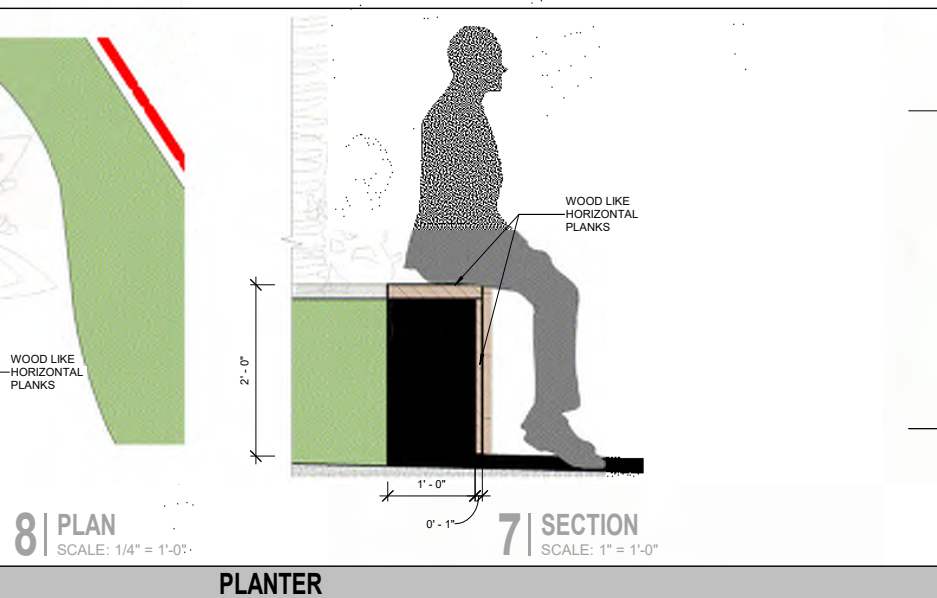
GLASS FENCE

FINAL DRC
74.06A HOTEL
10/23/2020

BAHIA CABANA HOTEL
3001-3007 HARBOR DRIVE, FORT LAUDERALE, FL 33316

A-6.01

adache
group architects





OCCUPANCY LOADS - LEVEL 1						
AREA NAME	SF	OCCUPANCY FUNCTION SPACE	NFPA OCC FACTOR	NFPA OCC LOAD	FBC OCC FACTOR	FBC OCC LOAD
ASSEMBLY						
CAFE/BAR	1,060 SF	ASSEMBLY	14	76	15	71
GRAB AND GO	293 SF	ASSEMBLY	14	21	15	20
POOL DECK	6,401 SF	ASSEMBLY	15	426	15	426
RESTAURANT/BAR	3,406 SF	ASSEMBLY	15	228	15	228
BUSINESS						
LAUNDRY	232 SF	BUSINESS	100	3	100	3
OFFICES	235 SF	BUSINESS	100	3	100	3
RESIDENTIAL LOBBY	2,778 SF	BUSINESS	100	28	100	28
KITCHEN						
BAR	336 SF	KITCHEN	7	48	7	48
KITCHEN	993 SF	KITCHEN	100	10	100	10
LOADING DOCK						
LOADING DOCK	985 SF	LOADING DOCK	300	4	300	4
MECHANICAL						
ELECTRICAL	376 SF	MECHANICAL	300	2	300	2
ELECTRICAL / MDF	188 SF	MECHANICAL	300	1	300	1
FIRE COMMAND	185 SF	MECHANICAL	300	1	300	1
FIRE PUMP	160 SF	MECHANICAL	300	1	300	1
FPL	368 SF	MECHANICAL	300	2	300	2
GENERATOR	409 SF	MECHANICAL	300	2	300	2
LV	131 SF	MECHANICAL	300	1	300	1
MECHANICAL	248 SF	MECHANICAL	300	3	300	3
PUMP DOM.	141 SF	MECHANICAL	300	1	300	1

OCCUPANCY LOADS - LEVEL 1						
AREA NAME	SF	OCCUPANCY FUNCTION SPACE	NFPA OCC FACTOR	NFPA OCC LOAD	FBC OCC FACTOR	FBC OCC LOAD
RETAIL						
RETAIL	207 SF	RETAIL	30	7	30	7
STORAGE						
LUGGAGE	86 SF	STORAGE	300	1	300	1
STORAGE	103 SF	STORAGE	300	2	300	2
STORAGE/ PANTRY	165 SF	STORAGE	300	1	300	1
TRASH ROOM	214 SF	STORAGE	300	1	300	1
TRASH/ RECYCLE ROOM	259 SF	STORAGE	300	1	300	1
VALET	92 SF	STORAGE	300	1	300	1
		20,072 SF		877		871

EXIT CAPACITY - LEVEL 1						
DOOR NUMBER	ROUGH WIDTH	DOOR WIDTH	NFPA DOOR CAPACITY FACTOR	NFPA DOOR CAPACITY	FBC DOOR CAPACITY FACTOR	FBC DOOR CAPACITY
P1-03		5'-9 1/2"	0.200	348	0.150	448
P1-04		5'-9 1/2"	0.200	348	0.150	448
P1-10		5'-11"	0.200	355	0.150	458
S2-1	3'-6"	3'-0"	0.200	180	0.150	224
S3-1	3'-4"	3'-0"	0.200	180	0.150	224
S-01		3'-9 1/2"	0.200	228	0.150	288
P1-01		6'-6 1/4"	0.200	391	0.150	508
P1-02	6'-3"	6'-0"	0.200	360	0.150	464
P1-07	3'-4"	3'-0"	0.200	180	0.150	224
P1-08	3'-4"	3'-0"	0.200	180	0.150	224
S1-1	3'-4"	3'-0"	0.200	180	0.150	224
				3,276		



OCCUPANCY LOADS - LEVEL 2						
AREA NAME	SF	OCCUPANCY FUNCTION SPACE	NFPA OCC FACTOR	NFPA OCC LOAD	FBC OCC FACTOR	FBC OCC LOAD
ASSEMBLY						
BOARD ROOM	362 SF	ASSEMBLY	15	25	15	25
COVERED TERRACE	4,077 SF	ASSEMBLY	15	274	15	274
MEETING ROOM	4,458 SF	ASSEMBLY	15	298	15	298
BUSINESS						
OFFICE	526 SF	BUSINESS	100	7	100	7
PREFUNCTION	1,031 SF	BUSINESS	100	11	100	11
GYM						
GYM	1,541 SF	GYM	50	31	50	31
SPA	2,298 SF	GYM	100	23	100	23
MECHANICAL						
MECHANICAL	455 SF	MECHANICAL	300	4	300	4
RESTROOM						
MEN	433 SF	RESTROOM	100	5	100	5
WOMEN	451 SF	RESTROOM	100	5	100	5
STORAGE						
STORAGE	496 SF	STORAGE	300	5	300	5
		16,129 SF		688		688

TOTAL STAIR CAPACITY (LIFE SAFETY)										
STAIR NAME	STAIR WIDTH	STAIR BASE	NFPA STAIR EGRESS CAPACITY				FBC STAIR EGRESS CAPACITY			
			STAIR SURPLUS		STAIR BASE		NFPA EGRESS CAPACITY	FBC EGRESS CAPACITY	FBC EGRESS CAPACITY	
			WIDTH SURPLUS	NFPA SURPLUS FACTOR	NFPA FACTORED OCCUPANTS	NFPA BASE FACTOR				NFPA BASE OCCUPANTS
STAIR #1	4' - 0"	3' - 8"	0' - 4"	0.218	18	0.3	147	165	0.2	210
STAIR #2	4' - 0"	3' - 8"	0' - 4"	0.218	18	0.3	147	165	0.2	210
STAIR #3	4' - 0"	3' - 8"	0' - 4"	0.218	18	0.3	147	165	0.2	210
TOTAL:								495		720

EXIT CAPACITY - LEVEL 2						
DOOR NUMBER	ROUGH WIDTH	DOOR WIDTH	NFPA DOOR CAPACITY FACTOR	NFPA DOOR CAPACITY	FBC DOOR CAPACITY FACTOR	FBC DOOR CAPACITY
403-1053	3'-4"	3'-0"	0.200	180	0.150	224
S1-2	4'-4"	4'-0"	0.200	240	0.150	304
S2-2	4'-4"	4'-0"	0.200	240	0.150	304
S3-15	4'-4"	4'-0"	0.200	240	0.150	304
S2-2	3'-4"	3'-0"	0.200	180	0.150	224
						1,080

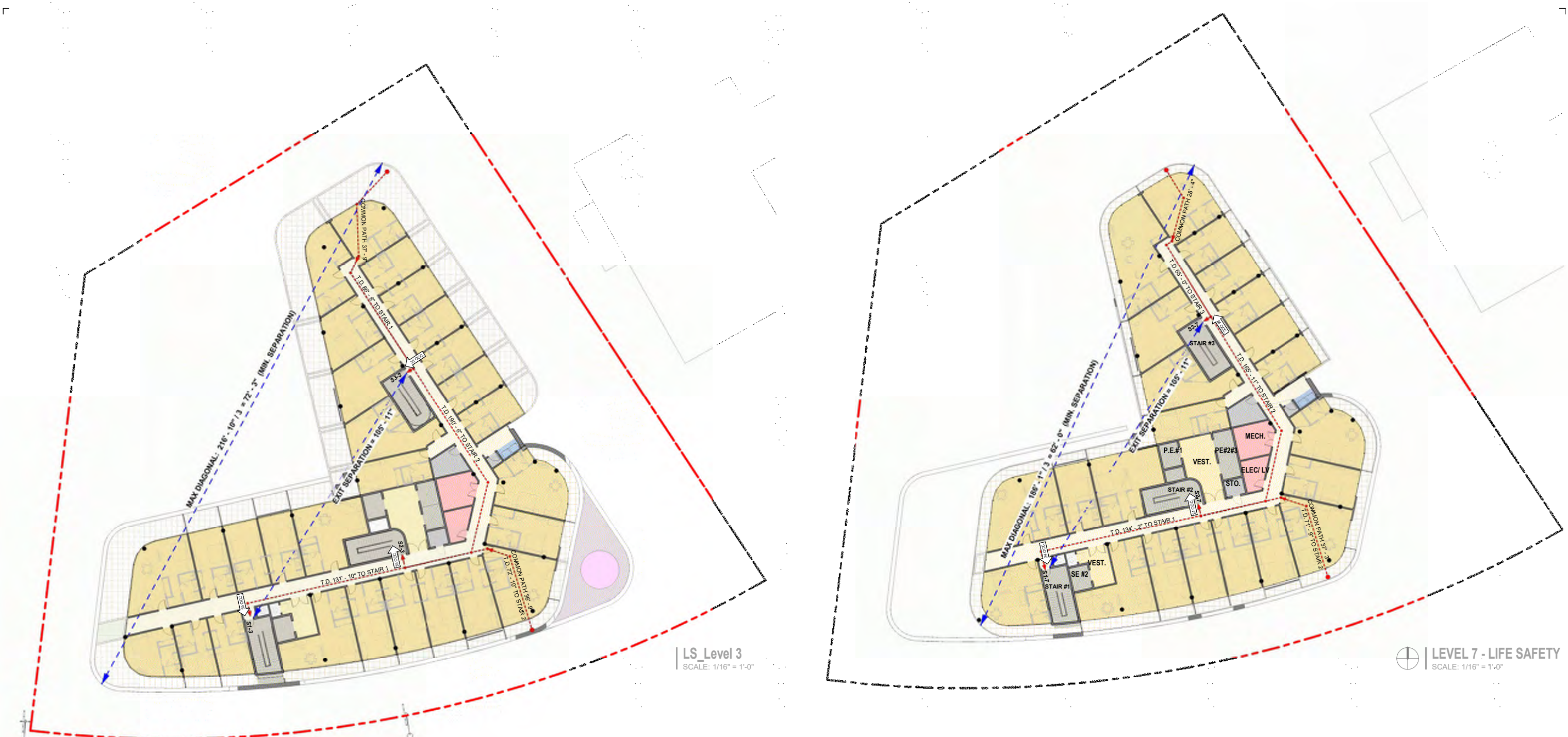
MAX. COMMON PATH OF EGRESS TRAVEL DISTANCE			
STORY	OCCUPANCY	MAXIMUM NUMBER OF DWELLING UNITS	MAX. COMMON PATH OF EGRESS TRAVEL DISTANCE
Basement, first, second or third story above grade plane	R-2a, b, 4	dwelling units	125 feet
Fourth story above grade plane and higher	NP	NA	NA

MAX. COMMON PATH OF EGRESS TRAVEL DISTANCE			
STORY	OCCUPANCY	MAXIMUM OCCUPANT LOAD PER STORY	MAX COMMON PATH OF EGRESS TRAVEL DISTANCE
First story above or below grade plane	A, B, E, F, M, U	49	75 feet
	H-2, H-3	3	25 feet
	H-4, H-5, I, R-1, R-2a, R-4	10	75 feet
	Sa	29	75 feet
Second story above grade plane	B, F, M, Sa	29	75 feet
Third story above grade plane and higher	NP	NA	NA

FINAL DRC
 74.06A HOTEL
 10/23/2020

BAHIA CABANA HOTEL
 3001-3007 HARBOR DRIVE, FORT LAUDERALE, FL 33316

A-7.1.01H adache group architects



OCCUPANCY LOADS - LEVEL 3						
AREA NAME	SF	OCCUPANCY FUNCTION SPACE	NFPA		FBC	
			OCC FACTOR	NFPA OCC LOAD	OCC FACTOR	FBC OCC LOAD
BUSINESS						
VENDING	35 SF	BUSINESS	100	1	100	1
MECHANICAL						
ELEC/ LV	105 SF	MECHANICAL	300	1	300	1
MECHANICAL	173 SF	MECHANICAL	300	1	300	1
RESIDENTIAL-HOTEL						
COVERED TERRACE	4,365 SF	RESIDENTIAL-HOTEL	200	36	200	36
GUEST ROOM	10,557 SF	RESIDENTIAL-HOTEL	200	63	200	63
STORAGE						
STORAGE	184 SF	STORAGE	300	3	300	3
15,419 SF				105		105

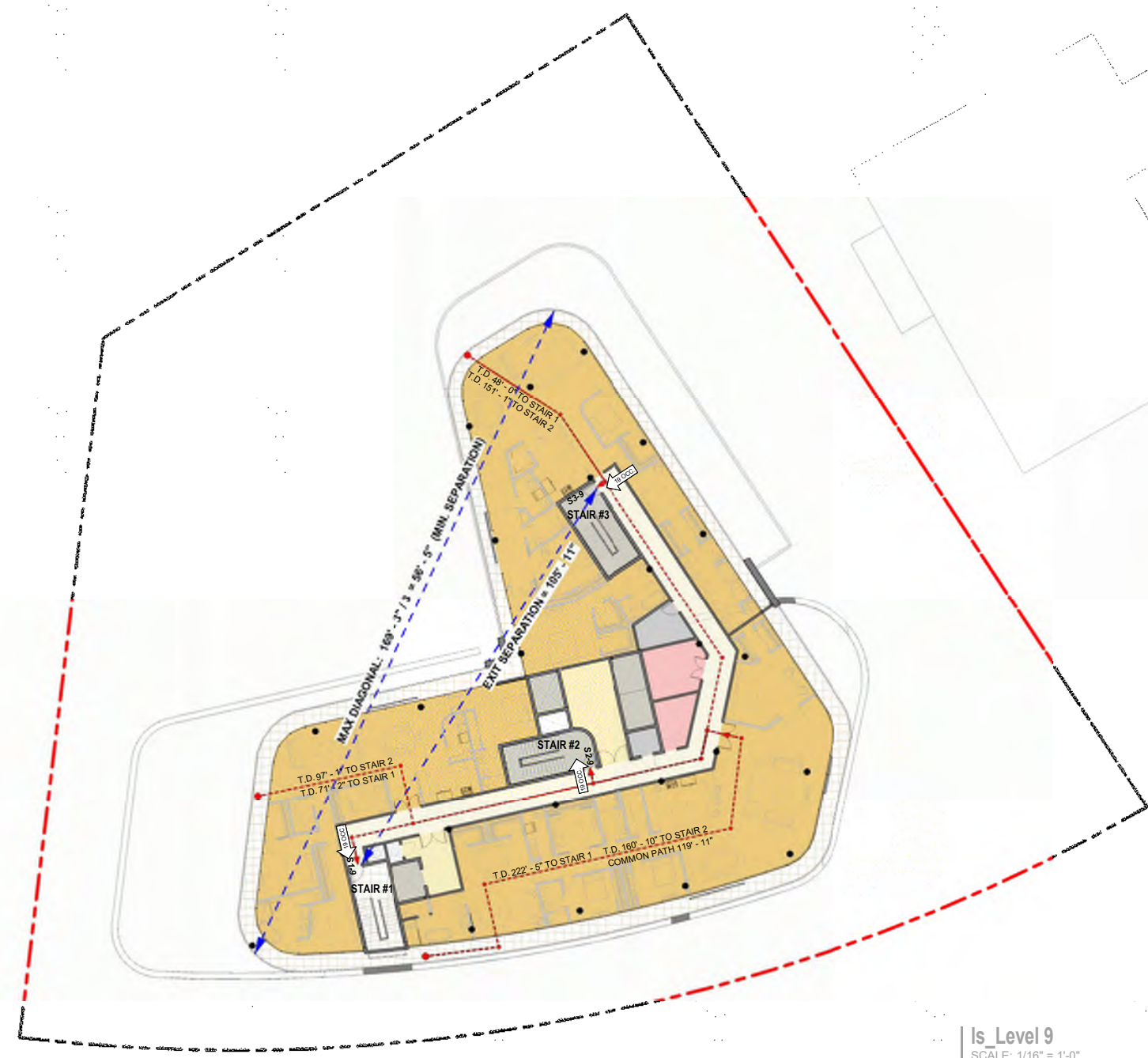
EXIT CAPACITY - LEVEL 3							
DOOR NUMBER	ROUGH WIDTH	DOOR WIDTH	NFPA DOOR CAPACITY		FBC DOOR CAPACITY		LIMITING CAPACITY CALC.
			FACTOR	CAPACITY	FACTOR	CAPACITY	
S1-3	3'-4"	3'-0"	0.200	180	0.150	224	180
S2-3	3'-4"	3'-0"	0.200	180	0.150	224	180
S3-3	3'-4"	3'-0"	0.200	180	0.150	224	180
S2-3	3'-4"	3'-0"	0.200	180	0.150	224	180
							720

MAX. EXIT ACCESS TRAVEL DISTANCES	
FBC TABLE 1017.2 (BLDG. W/ SPRINKLER SYSTEM)	
BUSINESS	300'
POOL DECK	250'
PUBLIC SPACES ASSEMBLY	250'
MECHANICAL ROOMS	100'
WITHIN GUESTROOMS	125'
RESIDENTIAL UNIT DOOR TO STAIR	250'

OCCUPANCY LOADS - LEVEL 7						
AREA NAME	SF	OCCUPANCY FUNCTION SPACE	NFPA		FBC	
BUSINESS			OCC FACTOR	NFPA OCC LOAD	OCC FACTOR	FBC OCC LOAD
VENDING	43 SF	BUSINESS	100	1	100	1
MECHANICAL						
ELEC/ LV	107 SF	MECHANICAL	300	1	300	1
MECHANICAL	175 SF	MECHANICAL	300	1	300	1
RESIDENTIAL-HOTEL						
COVERED TERRACE	1,949 SF	RESIDENTIAL-HOTEL	200	10	200	10
GUEST ROOM	9,393 SF	RESIDENTIAL-HOTEL	200	56	200	56
STORAGE						
STORAGE	182 SF	STORAGE	300	3	300	3
11,849 SF				72		72

EXIT CAPACITY - LEVEL 7							
DOOR NUMBER	ROUGH WIDTH	DOOR WIDTH	NFPA DOOR CAPACITY		FBC DOOR CAPACITY		LIMITING CAPACITY CALC.
			FACTOR	CAPACITY	FACTOR	CAPACITY	
S1-7	3'-4"	3'-0"	0.200	180	0.150	224	180
S2-7	3'-6"	3'-0"	0.200	180	0.150	224	180
S3-7	3'-4"	3'-0"	0.200	180	0.150	224	180
S1-7	3'-4"	3'-0"	0.200	180	0.150	224	180
S1-7.1	3'-4"	3'-0"	0.200	180	0.150	224	180
S2-7	3'-4"	3'-0"	0.200	180	0.150	224	180
							1,080

- CORRIDOR
- LOBBY
- CORES & VERTICAL CIRCULATION
- CIRCULATION
- RESIDENTIAL
- RESIDENTIAL-HOTEL
- RESIDENTIAL-CONDO
- ASSEMBLY
- KITCHEN COMMERCIAL
- ASSEMBLY (FIXED)
- BUSINESS
- GYM
- MECHANICAL
- RETAIL
- KITCHEN
- RESTROOM
- TERRACE
- STORAGE
- LOADING DOCK

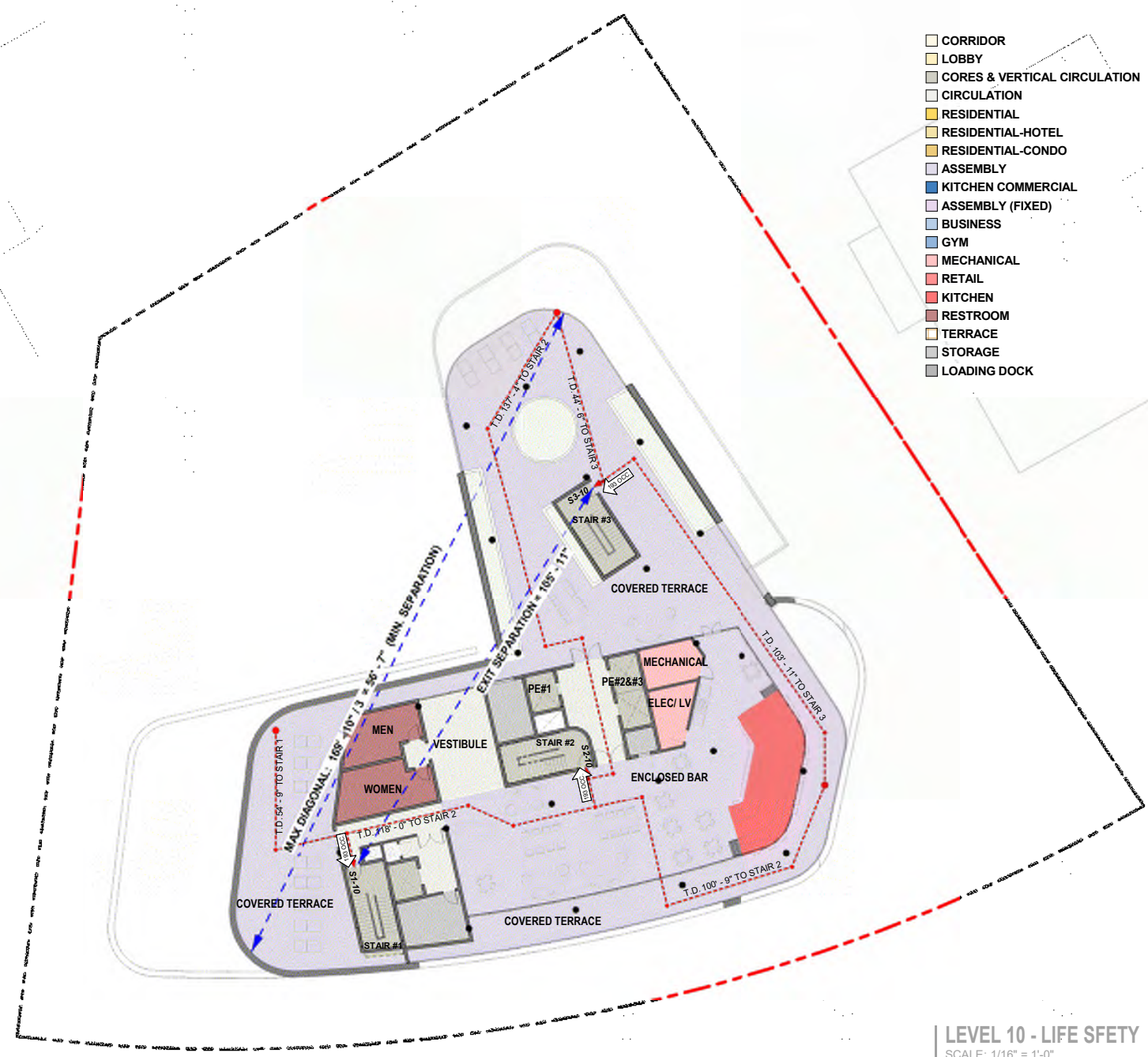


Is Level 9
SCALE: 1/16" = 1'-0"

OCCUPANCY LOADS - LEVEL 9						
AREA NAME	SF	OCCUPANCY FUNCTION SPACE	NFPA		FBC	
			OCC FACTOR	NFPA OCC LOAD	OCC FACTOR	FBC OCC LOAD
MECHANICAL						
ELEC/ LV	107 SF	MECHANICAL	300	1	300	1
MECHANICAL	175 SF	MECHANICAL	300	1	300	1
RESIDENTIAL-CONDO						
COVERED TERRACE	0 SF	RESIDENTIAL-CONDO	200	0	200	0
PH	8,317 SF	RESIDENTIAL-CONDO	200	43	200	43
STORAGE						
STORAGE	155 SF	STORAGE	300	2	300	2
	8,754 SF			47		47

EXIT CAPACITY - LEVEL 9									
DOOR NUMBER	ROUGH WIDTH	DOOR WIDTH	NFPA DOOR CAPACITY		FBC DOOR CAPACITY		LIMITING CAPACITY CALC.		
			FACTOR	CAPACITY	FACTOR	CAPACITY			
S1-9	3'-4"	3'-0"	0.200	180	0.150	224			180
S2-9	3'-4"	3'-0"	0.200	180	0.150	224			180
S3-9	3'-4"	3'-0"	0.200	180	0.150	224			180
S2-9	3'-4"	3'-0"	0.200	180	0.150	224			180
S3-9	3'-4"	3'-0"	0.200	180	0.150	224			180

MAX. EXIT ACCESS TRAVEL DISTANCES	
FBC TABLE 1017.2 (BLDG. W/ SPRINKLER SYSTEM)	
BUSINESS	300'
POOL DECK	250'
PUBLIC SPACES ASSEMBLY	250'
MECHANICAL ROOMS	100'
WITHIN GUESTROOMS	125'
RESIDENTIAL UNIT DOOR TO STAIR	250'



LEVEL 10 - LIFE SFETY
SCALE: 1/16" = 1'-0"

OCCUPANCY LOADS - LEVEL 10						
AREA NAME	SF	OCCUPANCY FUNCTION SPACE	NFPA		FBC	
			OCC FACTOR	NFPA OCC LOAD	OCC FACTOR	FBC OCC LOAD
ASSEMBLY						
COVERED TERRACE	6,088 SF	ASSEMBLY	15	406	15	406
ENCLOSED BAR	2,420 SF	ASSEMBLY	15	162	15	162
KITCHEN						
BAR	678 SF	KITCHEN	300	4	200	5
MECHANICAL						
ELEC/ LV	136 SF	MECHANICAL	300	1	300	1
MECHANICAL	146 SF	MECHANICAL	300	1	300	1
STORAGE						
STORAGE	394 SF	STORAGE	300	3	300	3
9,862 SF				577		578

EXIT CAPACITY - LEVEL 10									
DOOR NUMBER	ROUGH WIDTH	DOOR WIDTH	NFPA DOOR CAPACITY		FBC DOOR CAPACITY		LIMITING CAPACITY CALC.		
			FACTOR	CAPACITY	FACTOR	CAPACITY			
S1-10	3'-8"	3'-4"	0.200	200	0.150	251			200
S2-10	3'-8"	3'-4"	0.200	200	0.150	251			200
S3-10	3'-8"	3'-4"	0.200	200	0.150	251			200
S2-10	3'-4"	3'-0"	0.200	180	0.150	224			180
S3-10	3'-4"	3'-0"	0.200	180	0.150	224			180

TOTAL STAIR CAPACITY (LIFE SAFETY)									
STAIR NAME	STAIR WIDTH	STAIR BASE	NFPA STAIR EGRESS CAPACITY				FBC STAIR EGRESS CAPACITY		
			STAIR SURPLUS		STAIR BASE		NFPA EGRESS CAPACITY	FBC EGRESS CAPACITY	FBC EGRESS CAPACITY 2
			WIDTH SURPLUS	NFPA SURPLUS FACTOR	NFPA FACTORED OCCUPANTS	NFPA BASE FACTOR			
STAIR #1	4'-0"	3'-8"	0'-4"	0.218	18	0.3	147	165	240
STAIR #2	4'-0"	3'-8"	0'-4"	0.218	18	0.3	147	165	240
STAIR #3	4'-0"	3'-8"	0'-4"	0.218	18	0.3	147	165	240
TOTAL							495		720

\\CAM020\950878\0878\0878\Design\Drawings\HOTEL COVER (b44).dwg
2/2/2020 3:54 PM

GENERAL NOTES

- ELEVATIONS SHOWN HERE ON ARE REFERRED TO NORTH AMERICAN VERTICAL DATUM (N.A.V.D. 88) NO. SE 72; ELEVATION = 5.124.
- HORIZONTAL AND VERTICAL CONTROL SHALL BE PROVIDED BY THE CONTRACTOR'S SURVEYOR. LAYOUT IS THE RESPONSIBILITY OF THE CONTRACTOR.
- IT IS THE INTENT OF THESE DRAWINGS TO BE IN ACCORDANCE WITH APPLICABLE CODES AND AUTHORITIES HAVING JURISDICTION. ANY DISCREPANCIES BETWEEN THESE DRAWINGS AND APPLICABLE CODES SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE ENGINEER.
- EXISTING UTILITIES SHOWN ARE BASED ON INFORMATION SUPPLIED BY OTHERS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO MEET WITH ALL APPLICABLE UTILITY COMPANIES TO VERIFY ALL UNDERGROUND FACILITIES PRIOR TO THE START OF CONSTRUCTION. ALL TRENCH EXCAVATION SHALL PROCEED WITH EXTREME CAUTION AT ALL TIMES. IN THE EVENT THAT EXISTING UTILITIES ARE DAMAGED, IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO REPAIR OR REPLACE ALL SUCH DAMAGE.
- EXISTING GRADES WERE TAKEN FROM THE BEST AVAILABLE DATA AND MAY NOT ACCURATELY REFLECT PRESENT CONDITIONS. CONTRACTOR SHALL BE RESPONSIBLE FOR FAMILIARIZING HIMSELF WITH CURRENT SITE CONDITIONS, AND SHALL REPORT ANY DISCREPANCIES TO THE ENGINEER PRIOR TO STARTING WORK.
- REFERENCE SHALL BE MADE TO THE MECHANICAL, PLUMBING AND ELECTRICAL DRAWINGS FOR LOCATIONS, SIZE AND DETAILS OF ALL UTILITY SERVICES TO THE BUILDINGS.
- CONTRACTOR SHALL BE RESPONSIBLE FOR RESETTING ALL DISTURBED EXISTING MANHOLE COVERS, VALVE BOXES, BLOW-OFF RISERS, ETC. TO NEW ELEVATIONS, AS REQUIRED, WHETHER SPECIFICALLY SHOWN ON THESE DRAWINGS OR NOT.
- BOUNDARY AND TOPOGRAPHIC DATA BASED UPON SURVEY PREPARED BY MALAUSLIN ENGINEERING COMPANY, FORT LAUDERDALE, FLORIDA DATED JANUARY 7, 2020.
- THE PROPERTY SHOWN HEREON LIES WITHIN FLOOD ZONE X. ELEVATION = N/A & FLOOD ZONE AF. ELEVATION = 5.00 NAVD, AS SHOWN IN FLOOD INSURANCE RATE MAP NUMBER 12011C 578H, FLORIDA MAP REVISED DATE: AUGUST 18, 2014.

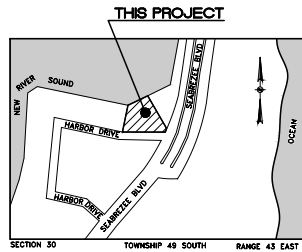
PAVING-GRADING-DRAINAGE NOTES

- HORIZONTAL AND VERTICAL CONTROL POINTS SHALL BE PROVIDED BY THE OWNER. ALL CONSTRUCTION LAYOUT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. IN THE EVENT THAT CONTROL POINTS ARE DISTURBED BY CONTRACTOR, CONTRACTOR SHALL PAY FOR ALL RESETTING OF CONTROL POINTS.
- GRADING SHALL CONSIST OF ALL EXCAVATION, FILLING, SHAPING AND SLOPING NECESSARY FOR THE CONSTRUCTION, PREPARATION AND COMPLETION OF ALL SUBBASES, SHOULDER, SLOPE, INTERSECTIONS, PAVEMENTS AND OTHER AREAS, ALL IN ACCORDANCE WITH THE ALIGNMENT AND GRADES SHOWN IN THESE DRAWINGS.
- DRAINAGE STRUCTURES SHALL BE AS DETAILED IN THESE DRAWINGS. DRAINAGE PIPE TO BE CORRUGATED POLYETHYLENE.
- ASPHALTIC PAVEMENTS SHALL CONSIST OF COMPACTED SUBGRADE, COMPACTED LIMEROCK SUBBASE AND TYPE 5-3 ASPHALTIC CONCRETE, AS SHOWN IN THESE DRAWINGS.
- WHERE APPLICABLE, SAWCUT ALL PAVEMENT EDGES WHERE EXISTING PAVEMENT IS TO REMAIN.
- CONCRETE CURB, SIDEWALK AND SLABS SHALL CONSIST OF 3,000 PSI PORTLAND CEMENT CONCRETE, AS SHOWN IN THESE DRAWINGS.
- ALL REINFORCING, ALL SIZES, SHALL CONFORM TO ASTM A615, GRADE 60.
- ALL EXPOSED CONCRETE SURFACES SHALL RECEIVE A LIGHT BROOM FINISH, UNLESS OTHERWISE DIRECTED. SEE ARCHITECTURAL DRAWINGS FOR WALKWAY FINISHES.
- FINAL GRADING OF LANDSCAPING AREAS SHALL BE COORDINATED IN THE FIELD WITH ENGINEER/LANDSCAPE ARCHITECT. COMPACTED FILL IN LANDSCAPED AREAS SHALL BE PLACED SUCH THAT 12 INCHES OF TOP SOIL MAY BE PLACED BETWEEN THE COMPACTED FILL AND FINISHED GRADE.
- TESTING LABORATORY SHALL BE RETAINED BY THE CONTRACTOR TO VERIFY SPECIFIED COMPACTION DENSITY AND CONCRETE STRENGTH. FAILURE OF ANY TESTING TO MEET SPECIFIED REQUIREMENTS SHALL BE DEEMED NON-COMPLYING.

ADA COMPLIANCE NOTES

THIS SET OF DOCUMENTS HAS BEEN DESIGNED TO COMPLY WITH 2010 & FLORIDA ACCESSIBILITY CODE FOR BUILDING CONSTRUCTION AND PER APPLICABLE FOOT NOTICES 304, 310 & 315. ALL RELATED SECTIONS OF THE CODES AND RELATED SECTIONS OF THESE DOCUMENTS AND COMPLIANCE IS REQUIRED BY ALL CONSTRUCTION TRAFFIC, CONSTRUCTION QUALITY AND COMPLIANCE OF THE FINAL PRODUCT ARE THE RESPONSIBILITY OF THE GENERAL CONTRACTOR. ALL ACCESSIBLE ROUTES MUST NOT EXCEED A SLOPE OF 1:20 (5.00%). CURB CUT RAMPS AND RAMPS MUST NOT EXCEED A SLOPE OF 1:12 (8.33%). RAMPS THAT EXCEED 6 FEET IN LENGTH REQUIRE HANDRAILS. ALL CROSS SLOPES MUST NOT EXCEED A SLOPE OF 1:50 (2.00%). PLANTERS, TREE GRATES AND OTHER ELEMENTS MUST BE FLUSH AND ADA COMPLIANT. PAVERS ON PEDESTRIAN PATH MUST BE FLUSH, FIRM, STABLE AND HAVE NO OPEN JOINTS. ALL DETECTABLE WARNING TO BE IN ACCORDANCE WITH MAH-DADE COUNTY ARTICLE 527 TESTED MATERIAL, TYPE & COLOR, LATEST EDITION (7/7/19).

SITE PLAN DRAWINGS FOR BAHIA CABANA 3001 HARBOR DRIVE, FORT LAUDERDALE, FL 33316



LOCATION MAP N.T.S.

LEGAL DESCRIPTION

- PARCEL 1
LOTS 12 AND 13, OCEAN HARBOR, ACCORDING TO THE PLAT THEREOF, AS RECORDED IN PLAT BOOK 28, PAGE 39, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA.
- PARCEL 1
ALL THAT PROPERTY LYING NORTHERLY OF THE NORTHERLY LINE OF LOTS 12 AND 13, OCEAN HARBOR PLAT, RECORDED IN PLT BOOK 28, PAGE 39, LYING BETWEEN THE EASTERLY LINE OF LOT 13, EXTENDED, AND THE WESTERN LINE OF LOT 12, EXTENDED, AND LYING SOUTHERLY OF THE LINES OF THE CITY OF FORT LAUDERDALE, AS SHOWN ON THAT SKETCH RECORDED IN DEED BOOK 884, AT PAGE 142, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA.
- SAD LANDS SITUATE, LYING AND BEING IN THE CITY OF FORT LAUDERDALE, BROWARD COUNTY, FLORIDA AND CONTAINING 42,211 SQUARE FEET OR 0.969 ACRES, MORE OR LESS.
- FLORIDA POWER & LIGHT COMPANY
MS. TIM RICHARDS
8801 W. SUNRISE BLVD.
PLANTATION, FLORIDA 33322
PHONE: (954) 476-2843
- BROWARD COUNTY TRAFFIC DIVISION
MS. JUAN C. NETO
2550 W. COMMERCIAL BLVD.
FT. LAUDERDALE, FLORIDA 33308
PHONE: (954) 347-2145
- CITY OF FORT LAUDERDALE
SEWER AND WATER DIVISION
MS. ALICIA GONZALEZ
100 N. ANDREWS AVENUE
FT. LAUDERDALE, FLORIDA 33301

UTILITY DEMOLITION NOTES

- EXISTING CONDITIONS SHOWN BASED UPON BEST AVAILABLE INFORMATION. CONTRACTOR SHALL VISIT THE SITE AND FAMILIARIZE HIMSELF WITH ALL EXISTING CONDITIONS PRIOR TO INITIATE WORK.
- ALL EXCAVATION SHALL PROCEED WITH EXTREME CAUTION AT ALL TIMES.
- ASPHALTIC PAVEMENT DESIGNATED FOR DEMOLITION SHALL BE REMOVED AND DISPOSED OF OFF-SITE AT AN APPROVED LOCATION. SAWCUT ALL PAVEMENT EDGES WHERE EXISTING PAVEMENT IS TO REMAIN.
- LIMEROCK SUBBASE SHALL BE SCARIFIED TO ITS FULL DEPTH AND SHAPED TO THE LINES AND GRADES SHOWN IN THESE DRAWINGS. IN NO CASE SHALL REMOVED LIMEROCK SUBBASE BE USED AS SUBBASE FOR NEW ROADWAYS, DRIVEWAYS, OR PARKING AREAS.
- THE CONTRACTOR SHALL CONTACT ALL UTILITY COMPANIES AND OWNER PRIOR TO DEMOLISHING ANY EXISTING UTILITIES.
- THE CONTRACTOR SHALL VERIFY LOCATION OF ALL UTILITY SERVICE LINES PRIOR TO START OF CONSTRUCTION AND DETERMINE THE REQUIREMENTS FOR THEIR PROTECTION.
- THE CONTRACTOR SHALL ARRANGE FOR THE PROPER DISCONNECTION AND CAPPING OF ALL UTILITIES BY THE APPROPRIATE CERTIFIED TECHNICIAN.
- THE CONTRACTOR SHALL BE RESPONSIBLE TO PROTECT ALL UTILITIES DESIGNATED TO REMAIN AND TO REPAIR ANY DAMAGED UTILITIES.
- ALL DEMOLITION DEBRIS, EQUIPMENT AND APPURTENANCES SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND DISPOSED OF OFF-SITE IN AN APPROVED MANNER, UNLESS OTHERWISE DESIGNATED BY THE OWNER.
- CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FOR ALL HAUL ROUTES, TO AND FROM THE SITE, FROM ALL REQUIRED AGENCIES/MUNICIPALITIES.

STRIPING AND SIGNAGE NOTES

- ALL STRIPING IN RIGHT-OF-WAY AND ALL ON-SITE STOP BARS SHALL BE THERMOPLASTIC WITH A Mixture OF 80 PERCENT GLASS SPHERES AND 20 PERCENT SHARP SILICA SAND APPLIED AT A RATE OF 0.20 PSF IN ACCORDANCE WITH FOOT SECTION 711.
- ALL SIGNAGE SHALL BE IN ACCORDANCE WITH "THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", LATEST EDITION, AND MATCH EXISTING.
- SIGN POSTS SHALL BE STEEL CHANNEL IN ACCORDANCE WITH VILLAGE COUNCIL PUBLIC WORKS DEPARTMENT STANDARDS AND SHALL BE BREAK-A-WAY.
- STOP SIGNS SHALL BE MOUNTED WITH 7" CLEAR FROM SIGN BOTTOM TO GRADE. WHERE INDICATED, ADDITIONAL SIGNAGE SHALL BE MOUNTED BELOW STOP SIGN.
- REFLECTIVE PAVEMENT MARKERS TO BE IN ACCORDANCE WITH FOOT STANDARD INDEX AND SECTION 706 AND SECTION 970 OF THE FOOT STANDARDS SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION. CONTRACTOR TO REPLACE ANY BROKEN OR MISSING RPM'S IN ALL WORK AREAS, WHETHER SPECIFICALLY SHOWN OR NOT.

UTILITY COMPANIES

- FLORIDA POWER & LIGHT COMPANY
MS. KARINA RAMIREZ
700 UNIVERSITY BLVD.
JUNO BEACH, FLORIDA 33408
PHONE: (954) 717-2081
- TECO PEOPLES GAS
MS. JOAN DOWNING
8416 PALM BEACH ROAD
TAMPA, FLORIDA 33619
PHONE: (813) 275-3785

INDEX OF DRAWINGS

SHT. NO.	TITLE
COVER	CIVIL COVER SHEET, LOCATION MAP AND NOTES
C-1	GENERAL NOTES
PGS-1	PAVEMENT MARKING AND SIGNAGE PLAN
PGD-1	PAVING-GRADING-DRAINAGE PLAN
PGD-2	SITE DETAILS
PGD-3	SITE SECTIONS
PGD-4	DRAINAGE DETAILS
WS-1	WATER AND SEWER NOTES
WS-2	WATER AND SEWER PLAN
WS-3	WATER DETAILS
WS-4	SEWER DETAILS
SWPPP-1	STORMWATER POLLUTION PREVENTION NOTES
SWPPP-2	STORMWATER POLLUTION PREVENTION PLAN



9400 South Dadeland Boulevard
Suite 601
Miami, FL 33156
305 / 378-6555
305 / 279-4553 fax

www.graef-usa.com

CERTIFICATE OF AUTHORIZATION NO. 4270
SEAL / SIGNATURE:



THIS ITEM HAS BEEN DIGITALLY SIGNED
AND SIGNED BY NELSON H. ORTIZ, P.E.
ON THE DATE ADJACENT TO THE SEAL.
PRINTED COPIES OF THIS DOCUMENT ARE
NOT CONSIDERED SIGNED AND SEALED
AND THE SIGNATURE MUST BE VERIFIED
ON ANY ELECTRONIC COPIES.

FOR THE FIRM: NELSON H. ORTIZ
PE-07058 (CIVIL)

PROJECT TITLE:

BAHIA CABANA
3001 HARBOR DRIVE
FORT LAUDERDALE, FL 33316
FOLIO NO. 504212240100

ADACHE GROUP ARCHITECTS
550 S. FEDERAL HIGHWAY
FT. LAUDERDALE, FL 33301

ISSUE:

NO. DATE REVISIONS BY

PROJECT INFORMATION:

PROJECT NUMBER: 2019-7065
DATE: 12/18/19
DRAWN BY: Z.H.
CHECKED BY: N.M.
APPROVED BY: N.H.O.
SCALE: AS SHOWN

SHEET TITLE:

CIVIL COVER SHEET, LOCATION MAP
AND NOTES

SHEET NUMBER:

COVER

X:\MIA\2019\08\08\08\08\Design\Drawings\HOT\ELC-1 (1.dwg)
9/2/2020 5:03 PM

GENERAL CONSTRUCTION NOTES

1. NO CONNECTIONS FOR THE PURPOSE OF OBTAINING WATER SUPPLY DURING CONSTRUCTION SHALL BE MADE TO ANY FIRE HYDRANT OR BLOW-OFF STRUCTURE WITHOUT FIRST OBTAINING PERMISSION AND A CONNECTION METER FROM THE CITY OF FORT LAUDERDALE.
2. THE CONTRACTOR WILL BE RESPONSIBLE FOR LOCATING, MOVING AND RELOCATING OR REPLACING ALL WATER SERVICES OR SEWER LATERALS WHICH ARE ENCOUNTERED DURING EXCAVATION. THE CONTRACTOR SHALL SUBMIT A WRITTEN PLAN FOR WATER SERVICE AND WASTEWATER SERVICE DISRUPTION FOR APPROVAL 7 (SEVEN) CALENDAR DAYS PRIOR TO THE ANTICIPATED DISRUPTION. THE CONTRACTOR SHALL NOTIFY THE PROPERTY OWNERS 48 HOURS IN ADVANCE OF ANY WORK ON THEIR SERVICE. THIS WORK SHALL BE CONSIDERED INCIDENTAL.
3. THE CONTRACTOR MUST USE EXTREME CARE TO AVOID DAMAGE OR DISRUPTION TO ANY EXISTING UTILITIES, WHETHER SHOWN ON THE PLANS OR NOT. ALL PLAN LOCATIONS ARE APPROXIMATE AND SHALL BE FIELD VERIFIED. CONTRACTOR IS TO CONTACT SUNDANCE STATE ONE CALL OF FLORIDA AT 1-800-432-4770 AND ALL OTHER PARTICIPATING UTILITIES 3 FULL BUSINESS DAYS PRIOR TO CONSTRUCTION FOR FIELD MARKUP LOCATIONS OF EXISTING UTILITIES AND FACILITIES.
4. THE CONTRACTOR MUST INFORM THE CITY AT LEAST 48-HOURS IN ADVANCE OF CONSTRUCTION, IN WRITING IF ANY CONFLICT IS DISCOVERED DURING POT HOLE OPERATIONS FOR CLARIFICATION BY THE CITY.
5. IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO NOTIFY THE UTILITIES DEPARTMENT OF THE CITY OF FORT LAUDERDALE AT LEAST TWO (2) BUSINESS DAYS IN ADVANCE TO COORDINATE ANY ACTIVITY TO BE PERFORMED BY THE CITY'S UTILITIES DEPARTMENT.
6. CONTRACTOR SHALL NOT DISTURB AREAS OUTSIDE EXISTING RIGHTS-OF-WAY.
7. IN GENERAL, EXISTING STRUCTURES AND UTILITIES ARE NOTED AS EXISTING AND/OR SHOWN IN THIN LINES. NEW CONSTRUCTION IS IN HEAVY LINES AND/OR UNDEVELOPED.
8. ALL WORK WITHIN STATE DEPARTMENT OF TRANSPORTATION (FDOT) RIGHT-OF-WAYS SHALL BE IN CONFORMANCE WITH FDOT SPECIFICATIONS AND PERMIT REQUIREMENTS.
9. ALL WORK WITHIN BROWARD COUNTY RIGHT-OF-WAYS SHALL BE IN CONFORMANCE WITH THE BROWARD COUNTY MINIMUM STANDARDS AND/OR REQUIREMENTS.
10. CONTRACTOR SHALL COMPLY WITH ALL LOCAL, CITY, COUNTY AND STATE REGULATIONS PERTAINING TO THE CLOSING OF PUBLIC STREETS FOR USE OF TRAFFIC DURING CONSTRUCTION.
11. CONTRACTOR SHALL PREPARE AND SUBMIT MAINTENANCE OF TRAFFIC (MOT) PLANS TO FDOT, CITY OF FORT LAUDERDALE, BROWARD COUNTY AS REQUIRED FOR WORK TO BE DONE WITHIN R/W PRIOR TO COMMENCEMENT OF WORK. SPECIFIC AGENCY MOT REQUIREMENTS ARE THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
12. STATIONS SHOWN ON THE DRAWINGS ARE BASED ON THE ESTABLISHED BASELINE AND SHALL NOT BE CONSIDERED AS DISTANCES OR AS A MEASURE OF THE LINEAR FOOTAGE OF PIPE TO BE INSTALLED.
13. THE GENERAL INTENT IS TO PROVIDE SEWER SERVICE LATERALS FOR EACH PROPERTY. ALL LATERAL LOCATIONS SHALL BE FIELD ADJUSTED.
14. CONTRACTOR SHALL MAINTAIN ACCESS TO PRIVATE PROPERTY AT ALL TIMES.
15. ALL OPEN TRENCHES AND HOLES ADJACENT TO ROADWAY OR WALKWAY SHALL BE PROPERLY MARKED AND BARRICADED TO ASSURE THE SAFETY OF BOTH VEHICULAR AND PEDESTRIAN TRAFFIC.
16. TRENCHES OR HOLES NEAR WALKWAYS, IN ROADWAYS OR THEIR SHOULDERS SHALL NOT BE LEFT OPEN DURING NIGHT TIME HOURS WITHOUT ADEQUATE PROTECTION.
17. CONTRACTOR SHALL PROMPTLY REPAIR AND RESTORE EXISTING PAVEMENT, SIDEWALKS, CURBS, DRIVEWAYS, PIPES, RESIDENTIAL AND COMMERCIAL SPRINKLER LINES, CONDUIT, CABLES, ETC. AND LANDSCAPE AREAS DAMAGED AS A RESULT OF CONSTRUCTION ACTIVITIES.
18. CONTRACTOR SHALL PROVIDE TEMPORARY FENCING AS REQUIRED BY AGENCIES HAVING JURISDICTION OVER THE PROJECT AND/OR WHEN REQUIRED FOR PUBLIC SAFETY.
19. THE CONTRACTOR SHALL BE RESPONSIBLE AT ALL TIMES THROUGHOUT THE DURATION OF CONSTRUCTION UNTIL ACCEPTANCE OF WORK FOR THE PROTECTION OF EXISTING AND NEWLY INSTALLED UTILITIES FROM DAMAGE OR DISRUPTION OF SERVICE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TAKING SUCH MEASURES AS NECESSARY TO PROTECT THE HEALTH, SAFETY AND WELFARE OF THOSE PERSONS HAVING ACCESS TO THE WORK SITE.
20. LOCATION OF AIR RELEASE VALVES MAY BE FIELD ADJUSTED BY THE ENGINEER OR CITY OF FORT LAUDERDALE AS NECESSARY.
21. CONTRACTOR SHALL ADJUST TO GRADE ALL EXISTING UTILITY CASTINGS INCLUDING VALVE BOXES, MANHOLES, HAND HOLES, PULL BOXES, RILES AND SIMILAR STRUCTURES IN CONSTRUCTION AREA TO BE OVERLAIN WITH ASPHALT.
22. EXISTING TRAFFIC SIGNALS SHALL BE RESTORED UPON COMPLETION PER FDOT STANDARDS. COST SHALL BE CONSIDERED INCIDENTAL. CONTRACTOR SHALL REPAIR OR REPLACE DAMAGED TRAFFIC SIGNAL BOXES FOR BROWARD COUNTY TRAFFIC ENGINEERING DIVISION.
23. CONTRACTOR SHALL RESTORE EXISTING PAVEMENT AND PAVEMENT MARKINGS/DAMAGE TO ORIGINAL PRE-CONSTRUCTION CONDITION OR AS OTHERWISE SPECIFIED IN CONTRACT DOCUMENTS. THIS WORK SHALL BE CONSIDERED INCIDENTAL.
24. ALL CONSTRUCTION WITHIN FDOT R/W MUST COMPLY WITH FDOT SPECIFICATIONS, STANDARDS, AND PERMIT REQUIREMENTS. NO WORK SHALL COMMENCE WITHIN FDOT R/W'S WITHOUT AN FDOT PERMIT. FULL LANE WIDTH RESTORATION TO MATCH EXISTING PAVED SECTION IS REQUIRED IN ACCORDANCE WITH FDOT STANDARDS FOR PROPOSED WORK WITHIN FDOT R/W.
25. SEWER LATERALS SHALL BE PROVIDED FOR EACH PROPERTY.
26. CONTRACTOR SHALL VERIFY ALL UTILITY LOCATIONS AND ELEVATIONS BEFORE STARTING CONSTRUCTION.
27. ELEVATIONS SHOWN HEREON ARE BASED ON THE NORTH AMERICAN VERTICAL DATUM 1988.

GENERAL NOTES - TRAFFIC CONTROL PLAN

1. THE TRAFFIC CONTROL PLANS FOR THE PROJECT SHALL COMPLY WITH THE LATEST EDITION OF THE ROADWAY AND TRAFFIC DESIGN STANDARDS, INDEX NO. 600-460. MUTCD AND THE STANDARD SPECIFICATIONS. THE CONTRACTOR'S RESPONSE TIME TO ALL REPORTED MALFUNCTIONS OF TRAFFIC SIGNALS WITHIN THE PROJECT LIMITS SHALL BE NO MORE THAN TWO (2) HOURS AND SHALL RESTORE ALL MALFUNCTIONING TRAFFIC SIGNAL EQUIPMENT TO ITS LEVEL OF OPERATION PRIOR TO THE MALFUNCTIONING WITHIN TWENTY-FOUR (24) HOURS. DURING THIS TIME THE CONTRACTOR SHALL PROVIDE AT HIS EXPENSE TEMPORARY TRAFFIC CONTROL DEVICES, FLAGGER PERSONNEL, AND LAW ENFORCEMENT PERSONNEL AS NECESSARY TO MAINTAIN A SAFE AND EFFICIENT FLOW OF TRAFFIC AT THE AFFECTED WORKS ZONE. THE ENGINEER OF THE CITY OF FORT LAUDERDALE SHALL APPROVE ALL MODIFICATIONS PRIOR TO THEIR IMPLEMENTATION.
2. THE CONTRACTOR SHALL MAINTAIN PROPER OPERATION OF ALL TRAFFIC SIGNAL LOOP ASSEMBLIES AND LOOP DETECTORS WITHIN THE PROJECT LIMITS. THE CONTRACTOR SHALL CORRECT ALL LOOP ASSEMBLY/DETECTOR MALFUNCTIONS WITHIN 24 HOURS OF NOTIFICATION OF SUCH MALFUNCTIONS BY THE ENGINEER.
3. THE AGENCY RESPONSIBLE FOR MAINTENANCE OF THE TRAFFIC SIGNALS AND RELATED EQUIPMENT IS BROWARD COUNTY TRAFFIC ENGINEERING.
4. A REGULATORY SPEED OF 25 MPH SHALL BE POSTED WITHIN THE LIMITS OF THE WORK ZONE.
5. EXISTING SIGNS AND PAVEMENT MARKINGS THAT CONFLICT WITH CONSTRUCTION SIGNS AND MARKINGS SHALL BE REMOVED DURING CONSTRUCTION. ALL EXISTING SIGNS THAT ARE REMOVED SHALL BE STOCKPILED IN A SECURE PLACE AND REINSTALLED AFTER CONSTRUCTION. REMOVE AND REPLACE ANY GROUND MOUNT SIGN BY USE OF INDEX NO. 811.
6. THE CONTRACTOR SHALL MAINTAIN EXISTING DRAINAGE PATTERNS AND PREVENT ADVERSE FLOODING OF THE TRAVEL LANES DURING CONSTRUCTION.
7. THE CONTRACTOR SHALL OBTAIN WRITTEN AUTHORIZATION FROM THE CITY OF FORT LAUDERDALE FOR ANY AND ALL CONSTRUCTION ACTIVITIES TO BE PERFORMED AT NIGHT. NO LANE CLOSURE SHALL BE ALLOWED BETWEEN THE HOURS OF 6:00 AM TO 8:00 AM AND 4:00 PM TO 7:00 PM, MONDAY THROUGH FRIDAY UNLESS APPROVED BY THE ENGINEER.
8. THE CONTRACTOR SHALL NOTIFY THE APPROPRIATE UTILITY COMPANY TWO (2) BUSINESS DAYS IN ADVANCE OF ANY EXCAVATION INVOLVING ITS UTILITIES SO THAT A COMPANY REPRESENTATIVE CAN BE PRESENT. THE LOCATION OF THE UTILITIES SHOWN IN THE PLANS ARE APPROXIMATE ONLY. THE EXACT LOCATION SHALL BE DETERMINED BY THE CONTRACTOR DURING CONSTRUCTION. SEE SPECS FOR LIST OF UTILITY COMPANIES.
9. TRAFFIC CONTROL ON ALL COUNTY RIGHTS-OF-WAY SHALL MEET THE ADDITIONAL REQUIREMENTS OF THE BROWARD COUNTY ENGINEERING DEPARTMENT.
10. THE AGENCY RESPONSIBLE FOR MAINTENANCE OF THE TRAFFIC SIGNALS AND RELATED EQUIPMENT IS BROWARD COUNTY TRAFFIC ENGINEERING.
11. CONTRACTOR SHALL PREPARE AND SUBMIT MAINTENANCE OF TRAFFIC PLAN (MOT) WHERE REQUIRED BY FEDERAL, STATE, COUNTY, OR LOCAL AGENCIES HAVING JURISDICTION. CONTRACTOR SHALL OBTAIN ALL REQUIRED APPROVALS AND PERMITS ASSOCIATED WITH THE MOT'S. ALL MOT'S TO BE ADOPTED.
12. THE CONTRACTOR SHALL ALSO COORDINATE THE CONSTRUCTION SCHEDULE WITH FDOT, BROWARD COUNTY AND THE CITY OF FORT LAUDERDALE TO AVOID LANE CLOSURES WHICH WOULD ADVERSELY AFFECT TRAFFIC DURING RUSH HOUR.

MAINTENANCE OF TRAFFIC SCHOOL/PEDESTRIAN

1. THE MAINTENANCE OF TRAFFIC PLAN, PROVIDED BY THE CONTRACTOR, SHALL INCLUDE PROVISIONS FOR PEDESTRIAN AND/OR SCHOOL STUDENT TRAFFIC AS WELL AS VEHICULAR TRAFFIC. THE FOLLOWING ARE MINIMUM REQUIREMENTS:
 - A. THE SAFE WALK ROUTE FOR ALL SCHOOL STUDENTS WITHIN THE VICINITY OF THE CONSTRUCTION ZONE SHALL BE MAINTAINED DURING THE TIMES STUDENTS ARE ARRIVING AT OR LEAVING SCHOOL. IF THE CURRENT WALKING ROUTE IS NOT BE MAINTAINED, THEN A TEMPORARY ROAD-ROCK 4' WALK WAY SHALL BE CREATED. THE SAFE WALK ROUTE SHALL BE SEPARATED FROM THE CONSTRUCTION ACTIVITY BY THE 4' OR MORE ORANGE CONSTRUCTION FENCE FOR THE ENTIRE LENGTH OF THE PROJECT OR THE LENGTH OF THE WALK ROUTE, WHICHEVER IS LESS.
 - B. ALL CONSTRUCTION EQUIPMENT AROUND ANY DESIGNATED CROSSLANE SHALL CEASE TO OPERATE DURING THE TIMES STUDENTS ARE ARRIVING AT OR LEAVING SCHOOL. ALL CONSTRUCTION EQUIPMENT ADJACENT TO A DESIGNATED WALK ROUTE SHALL CEASE OPERATING UNLESS SATISFACTORILY BARRICADED FROM THE WALK ROUTE.
 - C. IN THE CASE THAT A DESIGNATED CROSSING OR ANY PORTION OF THE DESIGNATED WALK ROUTE CAN NOT BE MAINTAINED, THEN THE CONTRACTOR SHALL NOTIFY THE SPECIAL PROJECTS COORDINATOR AT BROWARD COUNTY TRAFFIC ENGINEERING DIVISION, (854) 847-2871. A MINIMUM OF TEN (10) WORKING DAYS PRIOR TO CLOSING THAT ROUTE IN ORDER THAT AN ALTERNATE CROSSING/ROUTE CAN BE ESTABLISHED.
 - D. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO INSTALL ANY NECESSARY PAVEMENT, ROAD ROCK, PAVEMENT MARKING AND SIGNAGE AND/OR ANY PEDESTRIAN SIGNALIZATION AND/OR SIGNAL MODIFICATION TO ACCOMMODATE AN EXISTING OR ALTERNATE WALK ROUTE.
 - E. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE STATE CERTIFIED SCHOOL CROSSING GUARDS OR OFF DUTY POLICE OFFICERS TO CROSS STUDENTS AT ANY LOCATIONS OTHER THAN THOSE PREVIOUSLY DESIGNATED. THE CONTRACTOR MAY USE FLAG MEN, BUT IF THEY ARE STATE CERTIFIED AS A SCHOOL CROSSING GUARD.
 - F. TENETS (SCHOOL) PRIOR TO THE BEGINNING OF CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE SPECIAL PROJECTS COORDINATOR AT BROWARD COUNTY TRAFFIC ENGINEERING DIVISION, (854) 847-2871, TO ARRANGE A PRE-CONSTRUCTION - SCHOOL SAFETY MEETING.
 - G. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY THE BROWARD COUNTY SCHOOL BOARD, PUBLIC TRANSPORTATION DEPARTMENT, (754) 331-4440, TO ARRANGE A PRE-CONSTRUCTION - SCHOOL SAFETY ROUTE MEETING. THE MEETING SHALL BE HELD AT THE PROJECT LOCATION AND SHALL BE OPEN TO ALL BROWARD COUNTY SCHOOL BUS ROUTES AND TO MAKE ANY NECESSARY ARRANGEMENTS FOR ROUTINGS. THIS MEETING SHALL INCLUDE THE SPECIAL PROJECTS COORDINATOR FROM BROWARD COUNTY TRAFFIC ENGINEERING DIVISION, (854) 847-2871.
 - H. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING A SAFE AND ADEQUATE WALKING SURFACE FOR ALL SCHOOL CHILDREN/PEDESTRIANS. THE SAFE WALK ROUTE SHALL BE PART OF THE MAINTENANCE OF TRAFFIC PLAN.

GENERAL NOTES-BROWARD COUNTY TRAFFIC ENGINEERING DEPT.

ALL PARTIES NOTE THE FOLLOWING:

DIRECTIONAL BORES, UTILITY CONNECTIONS, THE PLACEMENT OF MOT AND ADVANCE SIGNAGE MAY BY THEIR PLACEMENT, DAMAGE/DESTROY THE COMMUNICATIONS CABLE/CONDUIT LOCATED INSIDE AND OUTSIDE THE PROJECT AREA. ADDITIONALLY, CURB/UTTER/SIDEWALK REMOVAL/PLACEMENT, RELOCATION OF TREES, LANDSCAPE ACTIVITIES AND IRRIGATION ACTIVITIES ARE POTENTIAL CAUSES FOR DAMAGE TO BCTED'S COMMUNICATION CABLE/CONDUIT. ALL PARTIES SHALL EXERCISE EXTREME CAUTION WHEN WORKING IN PROXIMITY TO THE COMMUNICATIONS CABLE/CONDUIT.

ANY ABOVE PROJECT ACTIVITY, INCIDENTAL OR OTHERWISE, WHICH IMPACTS OR DAMAGES THE COMMUNICATIONS CABLE/CONDUIT, SHALL BE SUBJECT TO THE FOLLOWING NOTES AND CONDITIONS BELOW.

COMMUNICATIONS NOTES

THE AGENCY RESPONSIBLE FOR MAINTENANCE OF THE TRAFFIC SIGNALS AND RELATED EQUIPMENT IS BROWARD COUNTY TRAFFIC ENGINEERING DIVISION (BCTED). ALL SYSTEM COMMUNICATIONS EQUIPMENT, CABLEING AND RELATED MATERIAL SHALL COMPLY WITH BROWARD COUNTY'S LATEST EDITION OF MINIMUM STANDARDS AS EXPRESSED IN THE "STANDARDS AND SPECIFICATIONS - COMMUNICATION INFRASTRUCTURE" DOCUMENT. ALSO REFER TO THE COUNTY'S COMMUNICATIONS PROCEDURES AND POLICES. BROWARD COUNTY TRAFFIC ENGINEERING DIVISION WILL NOT ACCEPT ANY PROJECTS THAT DO NOT MEET THESE STANDARDS AND SPECIFICATIONS. FOR A COPY OF THESE STANDARDS REFER TO BROWARD COUNTY WEB SITE AT WWW.BROWARD.ORG/TRAFFIC UNLESS PUBLICATIONS OR CONTACT ROBERT BLOUNT, COMMUNICATIONS MANAGER AT RBLOUNT@BROWARD.ORG.

INCLUDE BROWARD COUNTY TRAFFIC ENGINEERING (BCTED) IN ANY NOTICE OF UTILITY OWNERSHIP OR WITHIN A UTILITY OWNERS/CONTACT PERSON TABLE AS: BROWARD COUNTY TRAFFIC ENGINEERING 954-487-2745 (808 BLOUNT).

IF FIBER OPTIC PULL BOXES ALREADY EXIST AT AN INTERSECTION, NO ADDITIONAL PULL BOXES WILL BE INSTALLED.

ALL BCTED COMMUNICATIONS CABLE/CONDUIT SHALL BE LOCATED IN A MINIMUM OF 48 HOURS IN ADVANCE.

BROWARD COUNTY TRAFFIC ENGINEERING DIVISION PROCEDURE FOR NOTIFICATION OF COMMUNICATION DISRUPTION

WHEN COMMUNICATIONS TO AN INTERSECTION MUST BE DISRUPTED BY A CONTRACTOR TO PERFORM WORK, THE CONTRACTOR SHALL PROVIDE AN ADVANCE TWO DAY NOTICE IN WRITING TO THE BROWARD COUNTY TRAFFIC ENGINEERING DIVISION. THIS NOTIFICATION SHALL BE CONVEYED VIA ELECTRONIC MAIL (EMAIL) TO THE COMMUNICATIONS MANAGER AT RBLOUNT@BROWARD.ORG. NOTIFICATION SHALL INCLUDE CONTACT PERSON, TELEPHONE NUMBER, PURPOSE, LOCATION AND DURATION. THE DISRUPTION SHALL LAST FOR NO MORE THAN 3 CONSECUTIVE BUSINESS DAYS, WHERE POSSIBLE. THE DISRUPTION SHALL BE AT OFF PEAK HOURS BEGINNING AT 8:00AM AND ENDING AT 3:00PM.

EARTH-WORK NOTES

1. ALL DEMOLITION DEBRIS AND EXCESS MATERIAL SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND DISPOSED OF AT AN APPROVED OFF-SITE LOCATION, IN AN APPROVED MANNER, UNLESS OTHERWISE DESIGNATED BY THE OWNER.
2. CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FOR ALL HAUL ROUTES, TO AND FROM THE SITE, FROM ALL REQUIRED AGENCIES/MUNICIPALITIES.

Gräef

9400 South Dadeland Boulevard
Suite 601
Miami, FL 33156
305 / 378 6555
305 / 279 4553 fax

www.graef-usa.com

CERTIFICATE OF AUTHORIZATION NO. 4270

SEAL / SIGNATURE



THIS ITEM HAS BEEN DIGITALLY SIGNED
AND SEALED BY NIELSON H. ORTIZ P.E.
ON THE DATE ADJACENT TO THE SEAL.
PRINTED COPIES OF THIS DOCUMENT ARE
NOT CONSIDERED SIGNED AND SEALED
AND THE SIGNATURE MUST BE VERIFIED
ON ANY ELECTRONIC COPIES.

FOR THE FIRM: NELSON H. ORTIZ (JUL)

PROJECT TITLE:

BAHIA CABANA

3001 HARBOR DRIVE

FORT LAUDERDALE, FL 33316

FOLIO NO. 504212240100

ADACHE GROUP ARCHITECTS

550 S. FEDERAL HIGHWAY

FT. LAUDERDALE, FL 33301

ISSUE:

NO. DATE REVISIONS BY

PROJECT INFORMATION:

PROJECT NUMBER: 2019-7065

DATE: 12/18/19

DRAWN BY: Z.H.

CHECKED BY: N.M.

APPROVED BY: N.H.O.

SCALE: AS SHOWN

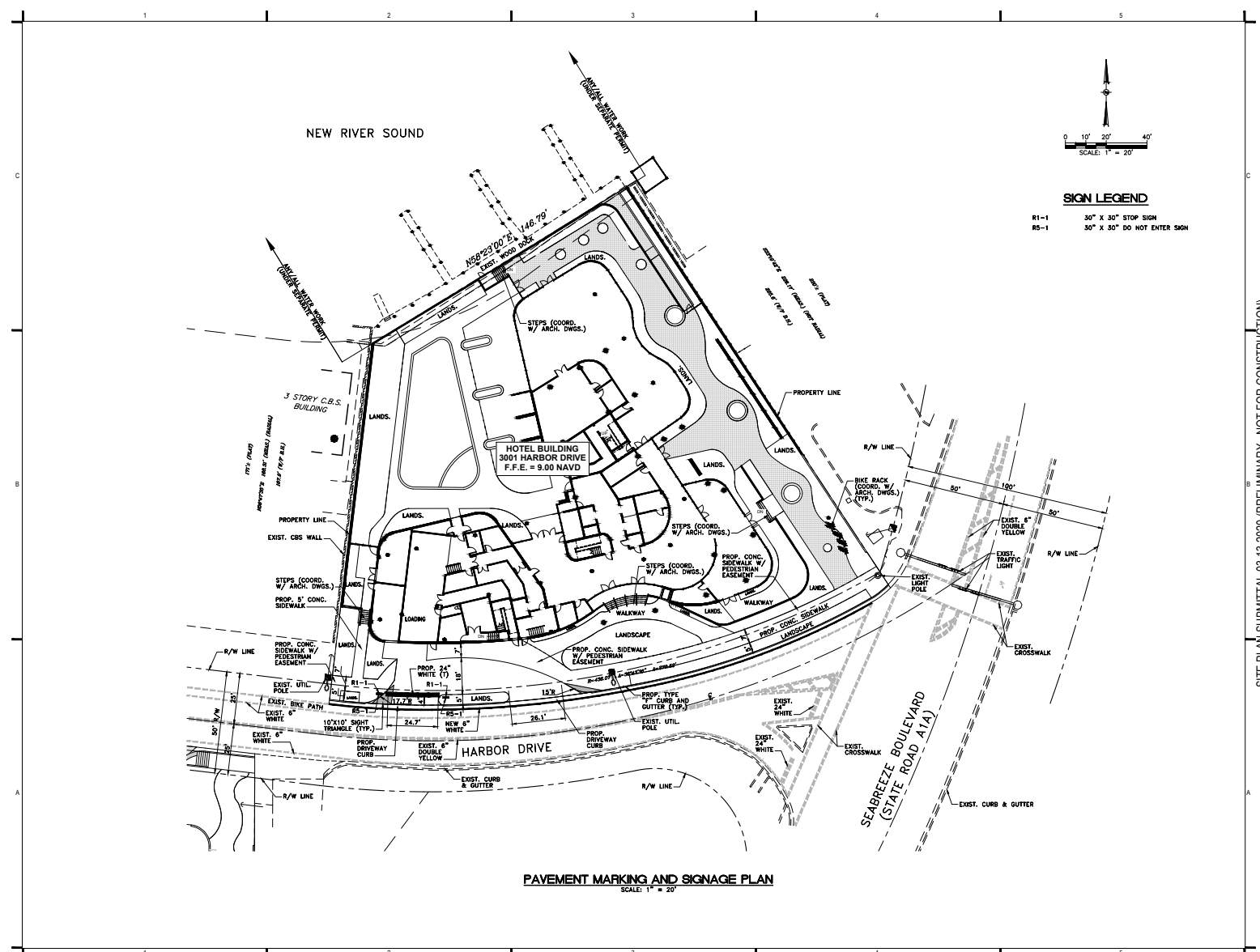
SHEET TITLE:

GENERAL NOTES

SHEET NUMBER:

C-1

\\NA001\2019\2020\1005\Design\Drawings\HOTEL\PMAS-1
20200313 1:51 PM



9400 South Dadeland Boulevard
Suite 601
Miami, FL 33156
305 / 378 5555
305 / 279 4553 fax

www.graef-usa.com

CERTIFICATE OF AUTHORIZATION NO. 4270
SEAL / SIGNATURE:



THIS ITEM HAS BEEN DIGITALLY SIGNED
AND SEALED BY NELSON H. ORTIZ, P.E.
ON THE DATE ADJACENT TO THE SEAL.
PRINTED COPIES OF THIS DOCUMENT ARE
NOT CONSIDERED SIGNED AND SEALED
AND THE SIGNATURE MUST BE VERIFIED
ON ANY ELECTRONIC COPIES.

FOR THE FIRM: NELSON H. ORTIZ
PE-57558 (CIVIL)

PROJECT TITLE:

BAHIA CABANA
3001 HARBOR DRIVE
FORT LAUDERDALE, FL 33316
FOLIO NO. 504212240100

ADACHE GROUP ARCHITECTS
550 S. FEDERAL HIGHWAY
FT. LAUDERDALE, FL 33301

ISSUE:

NO. DATE REVISIONS BY

PROJECT INFORMATION:

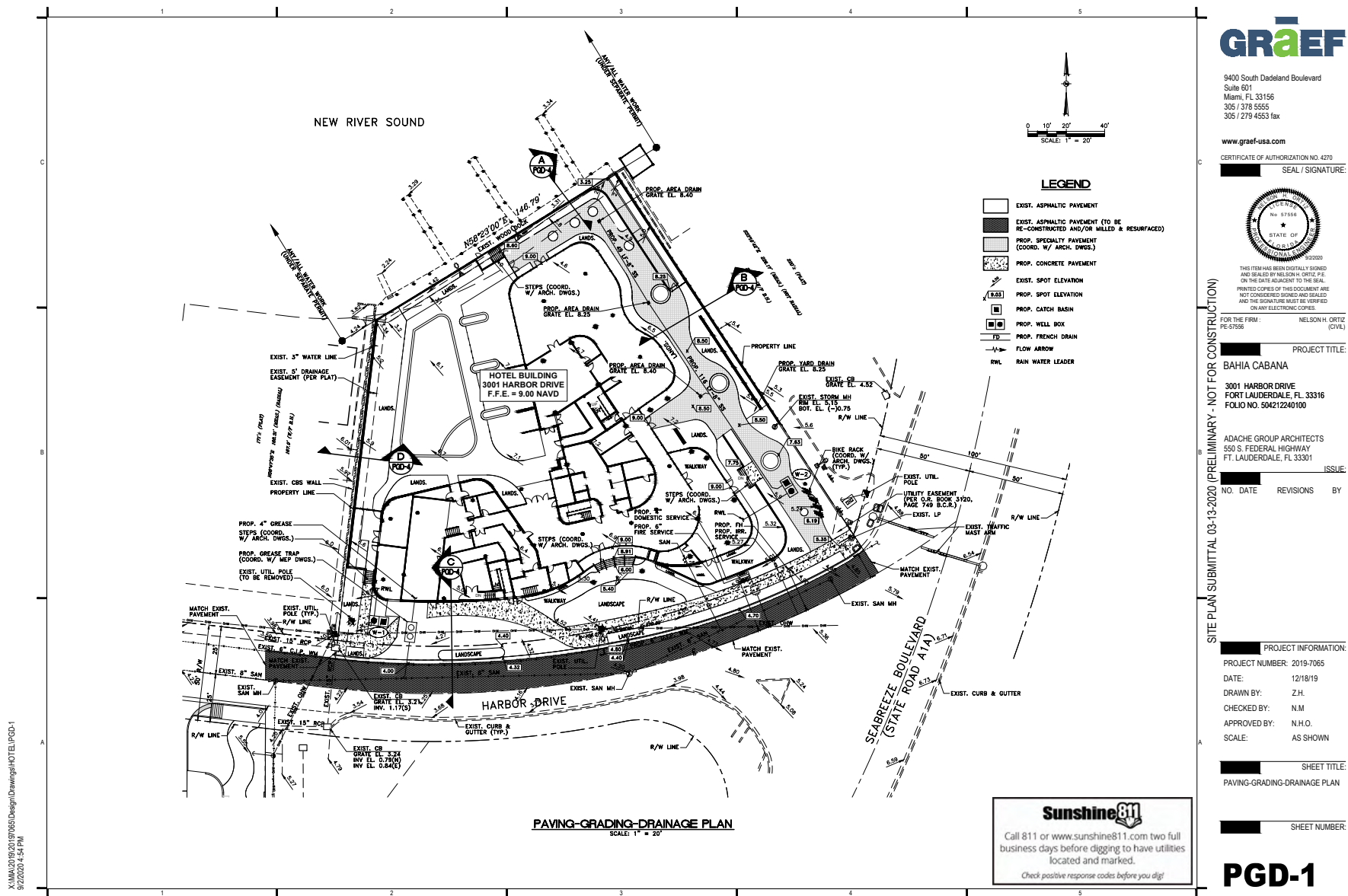
PROJECT NUMBER: 2019-7065
DATE: 12/18/19
DRAWN BY: Z.H.
CHECKED BY: N.M.
APPROVED BY: N.H.O.
SCALE: AS SHOWN

SHEET TITLE:

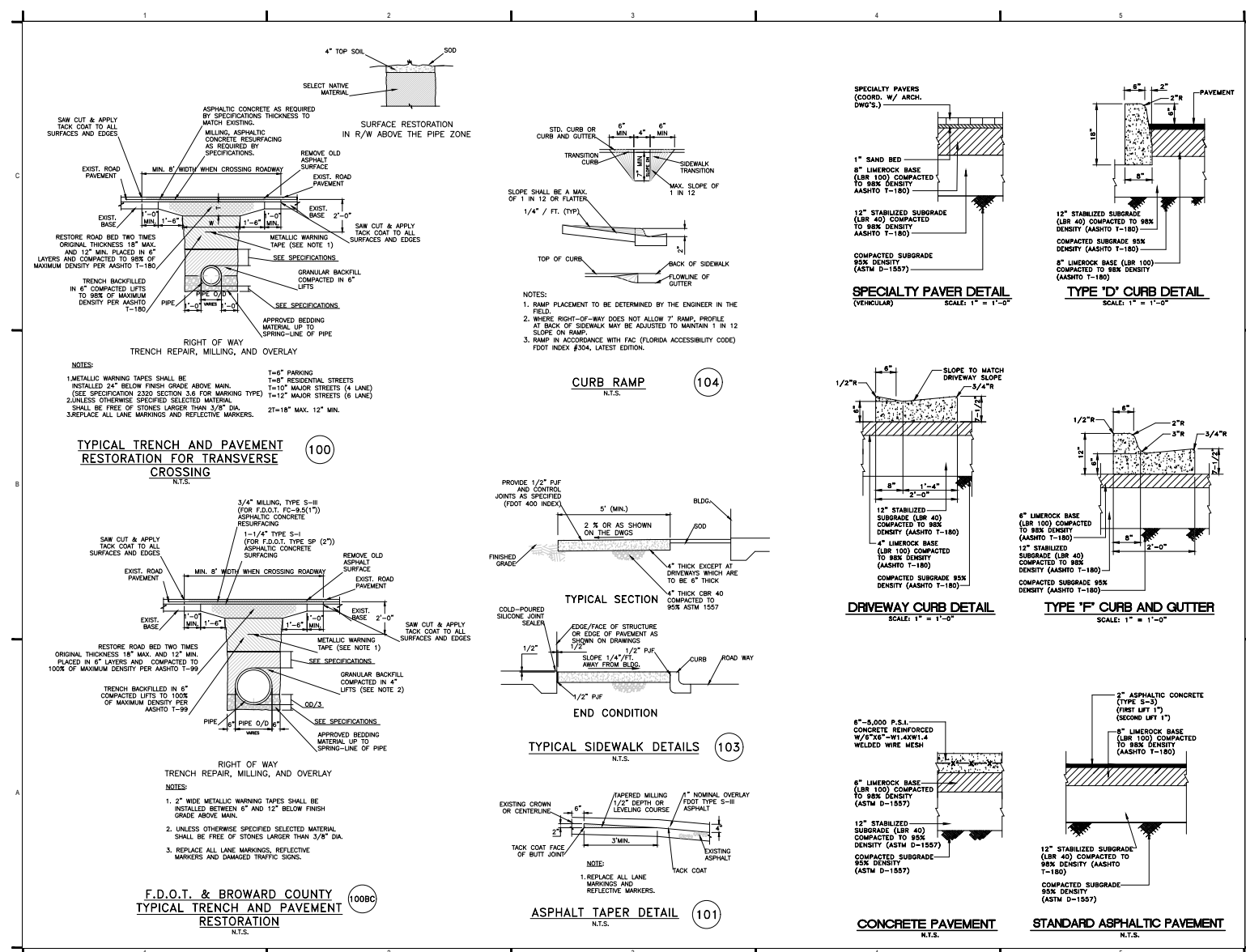
PAVEMENT MARKING AND
SIGNAGE PLAN

SHEET NUMBER:

PMS-1



\\cam001\proj\2019\0878\Design\Drawings\HOTEL\PGD-2
20200315 15:51 PM



GRaEF

9400 South Dadeland Boulevard
Suite 601
Miami, FL 33156
305 / 378-6555
305 / 279-4553 fax

www.graef-usa.com

CERTIFICATE OF AUTHORIZATION NO. 4270

SEAL / SIGNATURE:

FOR THE FIRM: NELSON H. ORTIZ (CIVIL)

PROJECT TITLE:
BAHIA CABANA

3001 HARBOR DRIVE
FORT LAUDERDALE, FL 33316
FOLIO NO. 504212240100

ADACHE GROUP ARCHITECTS
550 S. FEDERAL HIGHWAY
FT. LAUDERDALE, FL 33301

ISSUE:

NO. DATE REVISIONS BY

PROJECT INFORMATION:

PROJECT NUMBER: 2019-7065

DATE: 12/18/19

DRAWN BY: Z.H.

CHECKED BY: N.M.

APPROVED BY: N.H.O.

SCALE: AS SHOWN

SHEET TITLE:
SITE DETAILS

SHEET NUMBER:

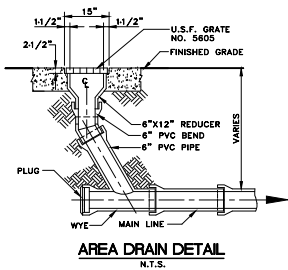
PGD-2

\\NA0027\9501017\0651\Drawings\HOTEL\PGD-3
2/2/2021 1:51 PM

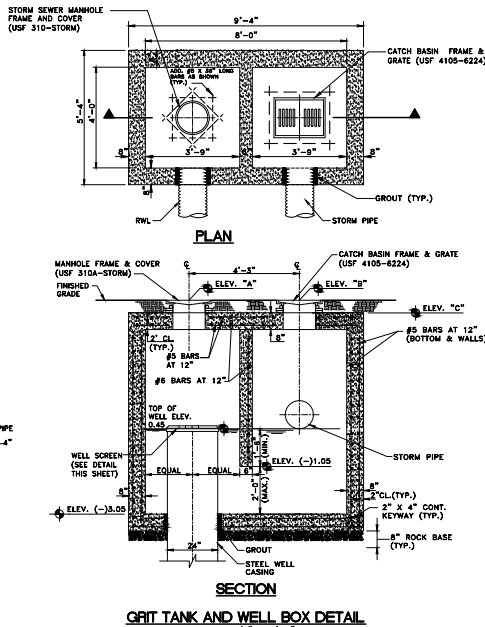
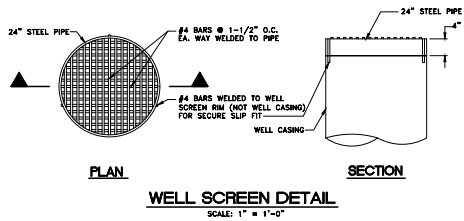
BC REF. No.

DRAINAGE WELL NOTES

1. DRAINAGE WELLS SHALL BE DRILLED TO AN ESTIMATED TOTAL DEPTH OF 90 FEET TO DISCHARGE RUNOFF TO AN AQUIFER HAVING A MINIMUM TOTAL DISSOLVED SOLIDS OF 10,000 PPM (MG/L).
2. WELLS SHALL HAVE AN UPPER CASING OF BLACK STEEL PIPE OF A MINIMUM OF 24 INCHES IN DIAMETER AND 1/8 INCH THICKNESS, WITH DRIVE SHOULDER, WHICH SHALL BE DRIVEN TO A FIRM SOIL AND GROUT SEALED AT LEAST 3 FEET BELOW THE CAP OF THE UPPERMOST POOR FORMATION TO BE DEVELOPED. CASING ENDS SHALL BE SQUARE TO THE AXIS OF THE SECTION AND SHALL BE WELDED TOGETHER AS DRIVING PROGRESSES.
3. WELLS SHALL BE AS PLUMB AND TRUE TO LINE AS GOOD WORKMANSHIP WILL PROVIDE.
4. COMPLETED WELLS SHALL BE THOROUGHLY AGITATED AND DEVELOPED.
5. CONTRACTOR SHALL SUBMIT THE METHOD OF TESTING OF THE WELLS FOR CAPACITY TO THE ENGINEER FOR APPROVAL BEFORE THE SHOP DRAWING. SUBMITTAL: THE APPROVED TEST SHALL RUN FOR A PERIOD OF 4 HOURS. CONTRACTOR SHALL PROVIDE CERTIFIED STATEMENT BY A PROFESSIONAL GEOLOGIST THAT THE WELL WILL COMPLY WITH THE DESIGN DISCHARGE RATE UPON COMPLETION.
6. THE CONTRACTOR SHALL PROVIDE ALL NECESSARY EQUIPMENT FOR CONDUCTING THE WELL TEST AND SATISFACTORILY DISPOSING OF THE WATER PUMPED FROM THE WELL. THE CONTRACTOR SHALL PROVIDE A MINIMUM OF 48 HOURS NOTICE TO THE OWNER AND ENGINEER PRIOR TO THE START OF THE TEST. CONTRACTOR IS TO COORDINATE THE DISPOSAL OF WATER PUMPED FROM THE WELL WITH THE OWNER.
7. TEST SHALL BEGIN AT HIGH TIDE. THE WATER LEVEL IN THE DRAINAGE WELL AND THE BAY SHALL BE RECORDED EVERY 15 MINUTES FROM 30 MINUTES PRIOR TO THE START OF TEST UNTIL PUMPING IS STOPPED, AND EVERY 3 MINUTES THEREAFTER DURING THE RECOVERY UNTIL THE LEVEL IN THE WELL INCREASES BY LESS THAN 0.1 FEET PER HOUR.
8. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE NECESSARY FLORIDA D.P.P. WELL DRILLING PERMITS AND COMPLY WITH ALL THE PROVISIONS THEREOF.
9. DESIGN CAPACITY OF WELLS IS 200 GPM WITH MAX. DRAWDOWN OF 24 INCHES (400 GRAVITY OF HEAD). CONTRACTOR SHALL PROVIDE ENGINEER WITH WELL TEST REPORTS PRIOR TO DEMOBILIZATION.



GRIT TANK AND WELL BOX SCHEDULE					
ID	ELEV. "A"	ELEV. "B"	ELEV. "C"	WELL	LONGITUDE
W-1	5.50	5.50	4.50	YES	26°06'35.77"N 80°06'24.82"W
W-2	5.50	5.50	4.50	YES	26°06'36.22"N 80°06'22.51"W



GRaEF
9400 South Dadeland Boulevard
Suite 601
Miami, FL 33156
305 / 378-6555
305 / 279-4553 fax

www.graef-usa.com

CERTIFICATE OF AUTHORIZATION NO. 4270

SEAL / SIGNATURE:



THIS SEAL HAS BEEN DIGITALLY SIGNED
AND VALIDATED BY NIELSON H. ORTIZ, P.E.
ON THE DATE ADJACENT TO THE SEAL.
PRINTED COPIES OF THIS DOCUMENT ARE
NOT CONSIDERED SIGNED AND SEALED
AND THE SIGNATURE MUST BE VERIFIED
ON ANY ELECTRONIC COPIES.

FOR THE FIRM: NELSON H. ORTIZ (CH)

PROJECT TITLE:

BAHIA CABANA

2001 HARBOR DRIVE
FORT LAUDERDALE, FL 33316
FOLIO NO. 504212240100

ADACHE GROUP ARCHITECTS
550 S. FEDERAL HIGHWAY
FT. LAUDERDALE, FL 33301

ISSUE:

NO. DATE REVISIONS BY

PROJECT INFORMATION:

PROJECT NUMBER: 2019-7065

DATE: 12/18/19

DRAWN BY: Z.H.

CHECKED BY: N.M.

APPROVED BY: N.H.O.

SCALE: AS SHOWN

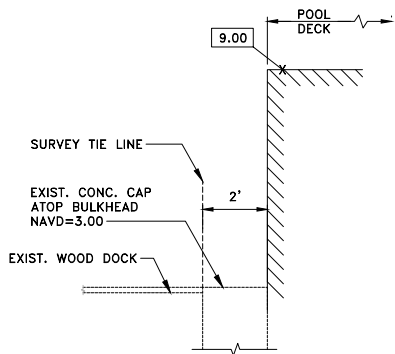
SHEET TITLE:

DRAINAGE DETAILS

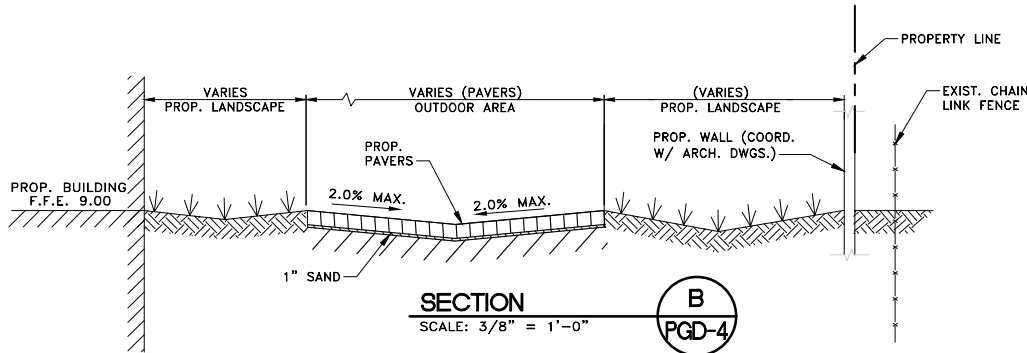
SHEET NUMBER:

PGD-3

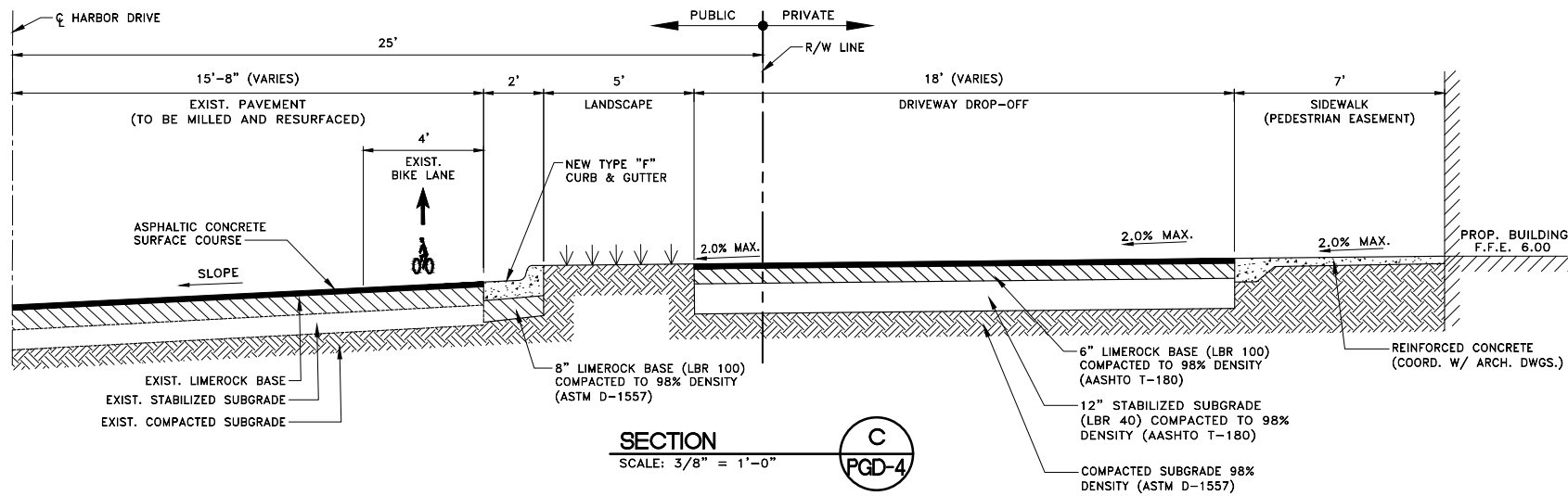
C:\Users\Administrator\Documents\PROJECT DOCS\2019\7065 Bahia Cabana\Design\Drawings\HOTEL\PGD-4
9/16/2020 12:36 PM



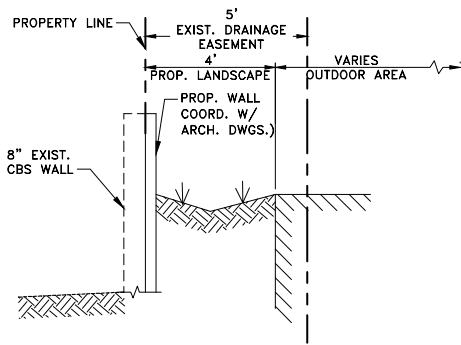
SECTION A
SCALE: 3/8" = 1'-0"



SECTION B
SCALE: 3/8" = 1'-0"



SECTION C
SCALE: 3/8" = 1'-0"



SECTION D
SCALE: 3/8" = 1'-0"

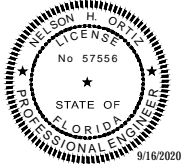


9400 South Dadeland Boulevard
Suite 601
Miami, FL 33156
305 / 378 5555
305 / 279 4553 fax

www.graef-usa.com

CERTIFICATE OF AUTHORIZATION NO. 4270

SEAL / SIGNATURE:



THIS ITEM HAS BEEN DIGITALLY SIGNED
AND SEALED BY NELSON H. ORTIZ, P.E.
ON THE DATE ADJACENT TO THE SEAL.
PRINTED COPIES OF THIS DOCUMENT ARE
NOT CONSIDERED SIGNED AND SEALED
AND THE SIGNATURE MUST BE VERIFIED
ON ANY ELECTRONIC COPIES.

FOR THE FIRM: NELSON H. ORTIZ
PE-57556 (CIVIL)

PROJECT TITLE:

BAHIA CABANA

3018 HARBOR DRIVE
FORT LAUDERDALE, FL 33316
Folio No. 504212240220

ADACHE GROUP ARCHITECTS
550 S. FEDERAL HIGHWAY
FT. LAUDERDALE, FL 33301

ISSUE:

NO. DATE REVISIONS BY

PROJECT INFORMATION:

PROJECT NUMBER: 2019-7065

DATE: 12/18/19

DRAWN BY: Z.H.

CHECKED BY: N.M.

APPROVED BY: N.H.O.

SCALE: AS SHOWN

SHEET TITLE:

SITE SECTIONS

SHEET NUMBER:

PGD-4

WATER SYSTEM NOTES

PIPE D.I.P.

1. DUCTILE IRON WATER MAIN PIPE SHALL CONFORM TO THE REQUIREMENTS OF A.N.S.I./A.W.W.A. C-151/A-21.21-02 AND LINED AND COATED PER A.N.S.I./A.W.W.A. C-104/A-21.4-03, 20" AND SMALLER PIPE SHALL BE PRESSURE CLASS 350, 24" AND LARGER, PIPE SHALL BE PRESSURE CLASS 250.
2. ALL DIP SHALL HAVE ADEQUATE PROTECTIVE MEASURES AGAINST CORROSION AND IT SHALL BE USED ONLY IF AS DETERMINED BY THE DESIGN ENGINEER, BASED ON FIELD CONDITIONS.
3. ALL DIP SHALL BE INSTALLED IN ACCORDANCE WITH A.N.S.I./A.W.W.A. C-800-99, OR LATEST REVISION.

PIPE P.V.C.

4. ALL P.V.C. MAINS SHALL BE SERIES 1120, CLASS 150 (OR 16) PRESSURE PIPE, CONFORMING TO A.N.S.I./A.W.W.A. C-900-97, OR LATEST REVISION, AND SHALL HAVE PUSH ON JOINTS, AND IRON PIPE CO.
5. ALL P.V.C. PIPE SHALL BE INSTALLED IN ACCORDANCE WITH THE LINE-BELL PLASTIC PIPE ASSOCIATION'S "GUIDE FOR INSTALLATION OF P.V.C. PRESSURE PIPE FOR MUNICIPAL WATER DISTRIBUTION SYSTEM". WATER DISTRIBUTION PIPE SHALL BE OF "BULST" COLOR. ALL WATER MAIN INSTALLATIONS SHALL COMPLY WITH THE COLOR CODING REQUIREMENTS OF CHAPTER 82-555.320 F.A.C. (FLORIDA ADMINISTRATIVE CODE).
6. DETECTOR TAPE ON ALL P.V.C. MAINS SHALL BE INSTALLED 18" ABOVE THE WATER MAIN.
7. ALL P.V.C. MAINS MUST HAVE #8 COPPER WIRE, SINGLE STRAND, PLACED ON TOP OF PIPE, SHALL BE ELECTRICALLY CONTINUOUS OVER THE ENTIRE LENGTH OF THE PIPE, AND FASTENED EVERY 10' WITH A #12 WIRE.

FITTINGS

8. FITTINGS SHALL BE DUCTILE IRON MEETING A.N.S.I./A.W.W.A. C153/21.00 AND SHALL BE COATED WITH 6 TO 8 MIL THICKNESS COAL TAR EPOXY CONFORMING TO THE REQUIREMENTS OF A.N.S.I./A.W.W.A. C550-05 AND C116/A21.03.
9. RESTRAINT JOINT PIPE SHALL BE USED FOR ALL BENDS, TEES, CROSSES, PLUGS, AND FIRE HYDRANTS. THRUST BLOCKS SHALL NOT BE ALLOWED.
10. RESTRAINT GLANDS/MECHANICAL JOINT RESTRAINT SHALL BE USED ONLY IF AUTHORIZED BY THE ENGINEER AND SHALL CONFORM TO A.N.S.I./A.W.W.A. STANDARDS C 111/A-21.11-00, OR LATEST REVISION.
11. ALL GLANDS SHALL BE MANUFACTURED FROM DUCTILE IRON AS LISTED BY UNDERWRITER'S LABORATORY FOR 250 P.S.I. MINIMUM WATER PRESSURE RATING.
12. GLANDS SHALL BE CLOM CORPORATION MODEL F-108S, STANDARD FIRE PROTECTION EQUIPMENT COMPANY, OR APPROVED EQUAL.

VALVES

13. TAPPING VALVES SHALL BE MUELLER H667 OR APPROVED EQUAL.
14. TAPPING SLEEVES SHALL BE MUELLER H615 OR APPROVED EQUAL.
15. GATE VALVES 3" OR LESS SHALL BE NISCO T-133 OR T-136 WITH MALLEABLE HAND WHEELS. NO SUBSTITUTIONS ALLOWED.
16. GATE VALVES 4" OR LARGER SHALL MEET A.W.W.A. C-500-02 SPECIFICATION (LATEST REVISION). VALVES SHALL BE MUELLER CO. OR APPROVED EQUAL.
17. ALL VALVES SHALL BE FURNISHED WITH EXTENSION TYPE CAST IRON VALVE BOXES OF PROPER LENGTH FOR PIPE DEPTH. ALL BOXES SHALL CONTAIN WITH A.W.W.A. SPECIFICATIONS WITH A SHAFT OF NO LESS THAN 5 INCHES AND HAVE THE WORD "WATER" CAST IN THE COVER. BASE OF VALVE BOX SHALL HAVE A FLARED SECTION TO FIT OVER STUFFING BOX OF VALVE.

HYDRANTS

18. FIRE HYDRANTS SHALL BE BREAKAWAY MUELLER CO. CENTURION MODEL 8A-423, OR METROPOLITAN 250 ESDY COMPRESSION TYPE F.A. OR APPROVED EQUAL.
19. FIRE HYDRANTS SHALL BE INSTALLED WITH THE CENTER OF THE NOZZLE 16" ABOVE FINISHED GRADE.
20. DEAD-END WATER MAINS 6" OR LARGER SHALL TERMINATE WITH A FIRE HYDRANT.

PLACEMENT

21. THE MINIMUM DEPTH OF COVER OVER WATER MAINS IS 30" EXCEPT WHERE SHOWN DIFFERENTLY ON PLANS.
22. A CONTINUOUS AND UNIFORM BEDDING SHALL BE PROVIDED. BACKFILL MATERIAL SHALL BE TAMPED IN LAYERS AROUND THE PIPE AS SHOWN ON THE PLANS AND/OR CITY OF FORT LAUDERDALE CONSTRUCTION STANDARDS AND SPECIFICATIONS, JANUARY 1982. ROCKS OR STONES LARGER THAN 3/4" DIAMETER FOUND IN THE TRENCH SHALL BE REMOVED FOR A DEPTH OF AT LEAST 4" BELOW THE BOTTOM OF THE PIPE.
23. PIPE DEFLECTION SHALL NOT EXCEED 75% OF THE MAXIMUM DEFLECTION RECOMMENDED BY THE MANUFACTURER.

SEPARATION

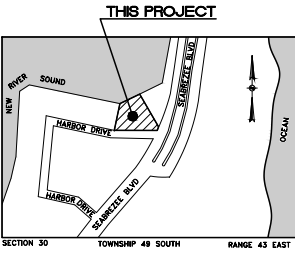
24. SANITARY SEWERS AND FORCE MAINS SHOULD CROSS UNDER WATER MAINS WHENEVER POSSIBLE. SANITARY SEWERS AND FORCE MAINS CROSSING WATER MAINS SHALL BE LAID TO PROVIDE A MINIMUM VERTICAL DISTANCE OF 18" BETWEEN THE INVERT OF THE UPPER PIPE AND THE CROWN OF THE LOWER PIPE, WHICHEVER POSSIBLE.
25. WHERE SANITARY SEWER FORCE MAINS MUST CROSS A WATER MAIN WITH LESS THAN 18" VERTICAL SEPARATION, BOTH THE SEWER AND WATER MAIN SHALL BE CONSTRUCTED OF DUCTILE IRON PIPE (DIP) AT THE CROSSING. SUFFICIENT LENGTH OF DIP MUST BE USED TO PROVIDE A MINIMUM SEPARATION OF 10 FEET BETWEEN ANY TWO JOINTS. ALL JOINTS ON THE WATER MAIN WITHIN 20 FEET OF THE CROSSINGS MUST BE MECHANICALLY RESTRAINED. A MINIMUM VERTICAL CLEARANCE OF 6" MUST BE MAINTAINED AT ALL CROSSINGS.
26. A MINIMUM 10 FOOT HORIZONTAL SEPARATION SHALL BE MAINTAINED BETWEEN ANY TYPE OF SEWER AND WATER MAIN IN PARALLEL INSTALLATIONS WHENEVER POSSIBLE.
27. THE PREFERRED SEPARATION BETWEEN WATER MAINS AND SEWER MAINS SHALL BE 10 FEET. IN CASES WHERE IT IS NOT POSSIBLE TO MAINTAIN A 6 FOOT HORIZONTAL SEPARATION BETWEEN THE WATER MAINS AND SEWER MAINS, ONE OF THE FOLLOWING CONDITIONS MUST BE MET: THE MINIMUM SEPARATION BETWEEN WATER AND SEWER MAINS SHALL BE 3 FEET.

SEPARATION (CONT'D)

- 27.a. THE WATER MAIN MUST BE LAID IN A SEPARATE TRENCH OR ON AN UNDISTURBED EARTH SHELF LOCATED ON ONE SIDE OF THE SEWER OR FORCE MAIN AT SUCH ELEVATION THAT THE BOTTOM OF THE WATER MAIN IS AT LEAST 18 INCHES ABOVE THE TOP OF THE SEWER.
- 27.b. THE SEWER OR FORCE MAIN IS ENCASED IN CONCRETE OR A WATERTIGHT CARRIER PIPE.
- 27.c. BOTH THE SEWER AND THE WATER MAIN ARE CONSTRUCTED OF PRESSURE PIPE TESTED TO 150 P.S.I.
28. WHERE IT IS NOT POSSIBLE TO MAINTAIN A VERTICAL DISTANCE OF 18" IN PARALLEL INSTALLATIONS, THE WATER MAIN SHALL BE CONSTRUCTED OF DIP AND THE SANITARY SEWER OR FORCE MAIN SHALL BE CONSTRUCTED OF DIP WITH A MINIMUM VERTICAL CLEARANCE OF 6". THE WATER MAIN SHOULD BE ABOVE THE SEWER. JOINTS ON THE WATER MAIN SHALL BE LOCATED AS FAR APART AS POSSIBLE FROM THE JOINTS ON THE SEWER OR FORCE MAIN (STAGGERED JOINTS).
29. ALL CROSSINGS SHALL BE ARRANGED SO THAT THE SEWER PIPE JOINTS AND THE WATER MAIN PIPE JOINTS ARE EQUIDISTANT FROM THE POINT OF CROSSING (PIPES CENTERED ON THE CROSSING).
30. WHERE A NEW PIPE CONFLICTS WITH AN EXISTING PIPE WITH LESS THAN 18" VERTICAL CLEARANCE, THE NEW PIPE SHALL BE ARRANGED TO MEET THE CROSSING REQUIREMENTS ABOVE.

TESTING, DISINFECTION

31. PIPE SHALL BE TESTED UNDER CONSTANT PRESSURE OF 150 P.S.I. FOR A MINIMUM TEST PERIOD OF 2 HOURS AND SHALL NOT EXCEED THE LEAKAGE REQUIREMENTS AS PER A.N.S.I./A.W.W.A. SPECIFICATIONS OF C-500-05 LEAKAGE FORMULA:
 $Q = \text{ALLOWABLE LEAKAGE, IN GALLONS PER HOUR}$
 $Q = (SD/P) / 148,000$
 $D = \text{DIAMETER OF THE PIPE TESTED, IN INCHES}$
 $L = \text{TOTAL LENGTH OF PIPE TESTED, IN FEET}$
 $P = \text{AVERAGE TEST PRESSURE, IN POUNDS PER SQUARE INCH}$
32. THE CITY OF FORT LAUDERDALE PUBLIC SERVICES DEPARTMENT WILL TAKE ALL BACTERIOLOGICAL TESTS TO BE SCHEDULED VIA INSPECTOR. IF OTHERWISE SPECIFIED IN CONTRACT DETAILED SPECIFICATION AND/OR AUTHORIZED BY THE CHIEF OF RECORD, BACTERIOLOGICAL TESTS MAY BE PERFORMED BY A CERTIFIED ENVIRONMENTAL TESTING LABORATORY.
33. DISINFECTION OF MAINS SHALL COMPLY WITH A.N.S.I./A.W.W.A. C-651-05 STANDARD. BACTERIOLOGICAL SAMPLING POINTS SHALL BE DESIGNATED ON THE ENGINEERING PLANS. MINIMUM ONE SAMPLING POINT AT EACH END. MAXIMUM SPACE BETWEEN SAMPLING POINTS IS 1200 FEET.
34. CONNECTIONS TO EXISTING MAINS SHALL BE MADE UNDER THE DIRECTION OF THE CITY OF FORT LAUDERDALE.
35. THERE SHALL BE NO CONNECTION TO AN EXISTING WATER MAIN UNTIL PRESSURE AND BACTERIOLOGICAL TESTS HAVE BEEN CONDUCTED AND THE RESULTS ARE APPROVED AND ACCEPTED BY THE CITY OF FORT LAUDERDALE.
36. ALL METER SERVICE CONNECTIONS SHALL BE BRONZE FROM PLUG VALVE. NO GATE VALVES ARE TO BE USED (2" OR LESS).
37. SERVICE SADDLES SHALL BE DUCTILE IRON WITH STAINLESS STEEL STRAPS. SADDLES SHALL BE DOUBLE STRAP TYPE. ALL SERVICE SADDLES SHALL CONFORM TO A.N.S.I./A.W.W.A. C 111/A-21.11-00 AND A.S.T.M. A508.
38. ALL SERVICE LINES SHALL BE COPPER TUBING, TYPE "K", OR PLASTICIZED POLYETHYLENE 3408, A.S.T.M. D-2937, S.D.R. 9, 200 P.S.I.



LEGAL DESCRIPTION

PARCEL 1
LOTS 26 AND 27, OCEAN HARBOR, ACCORDING TO THE PLAT THEREOF, AS RECORDED IN PLAT BOOK 26, PAGE 39, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA.

PARCEL 8
ALL THAT PROPERTY LYING NORTHERLY OF THE NORTHERLY LINE OF LOTS 12 AND 13, OCEAN HARBOR PLAT, RECORDED IN PLT BOOK 26, PAGE 39, LYING BETWEEN THE EASTERLY LINE OF LOT 13, EXTENDED, AND THE WESTERLY LINE OF LOT 12, EXTENDED, AND LYING SOUTHERLY OF THE LANDS OF THE CITY OF FORT LAUDERDALE, AS SHOWN ON THAT EXCHG RECORDED IN DEED BOOK 694, AT PAGE 142, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA.

SAID LANDS SEQUATE, LYING AND BEING IN THE CITY OF FORT LAUDERDALE, BROWARD COUNTY, FLORIDA AND CONTAINING 42,111 SQUARE FEET OR 0.969 ACRES, MORE OR LESS.

Sunshine811

Call 811 or www.sunshine811.com two full business days before digging to have utilities located and marked.
Check positive response codes before you dig!

GRaEF

9400 South Dadeland Boulevard
Suite 601
Miami, FL 33156
305 / 378 5555
305 / 279 4553 fax

www.graef-usa.com

CERTIFICATE OF AUTHORIZATION NO. 4270
SEAL / SIGNATURE:



THIS ITEM HAS BEEN DIGITALLY SIGNED
AND SIGNED BY NELSON H. ORTIZ, P.E.
ON THE DATE ADJACENT TO THE SEAL.
PRINTED COPIES OF THIS DOCUMENT ARE
NOT CONSIDERED SIGNED AND SEALED
AND THE SIGNATURE MUST BE VERIFIED
ON ANY ELECTRONIC COPIES.

FOR THE FIRM: NELSON H. ORTIZ (CIVIL)
PE-07056

PROJECT TITLE:
BAHIA CABANA

3001 HARBOR DRIVE
FORT LAUDERDALE, FL 33316
FOLIO NO. 504212240100

ADACHE GROUP ARCHITECTS
550 S. FEDERAL HIGHWAY
FT. LAUDERDALE, FL 33301

ISSUE:
NO DATE REVISIONS BY

PROJECT INFORMATION:
PROJECT NUMBER: 2019-7065

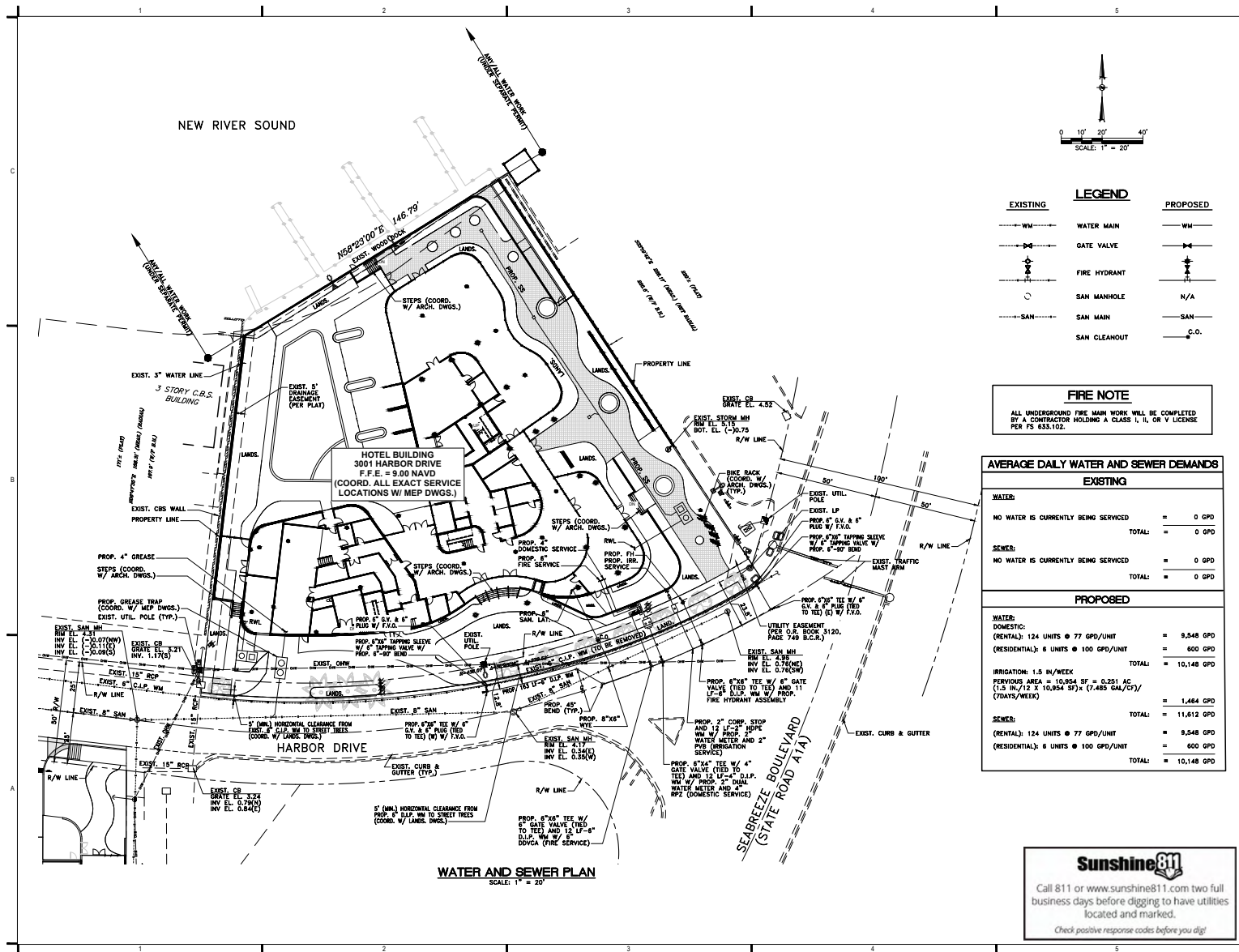
DATE: 12/18/19
DRAWN BY: Z.H.
CHECKED BY: N.M.
APPROVED BY: N.H.O.
SCALE: AS SHOWN

SHEET TITLE:
WATER AND SEWER NOTES

SHEET NUMBER:

WS-1

\\CAM02\05\05\2019\05\05\Design\Drawings\HOTEL\WS-1
20200215 15:41 PM



9400 South Dadeland Boulevard
Suite 601
Miami, FL 33156
305 / 378 5555
305 / 279 4553 fax

www.graef-usa.com

CERTIFICATE OF AUTHORIZATION NO. 4270
SEAL / SIGNATURE:



THIS ITEM HAS BEEN DIGITALLY SIGNED
AND SEALED BY NELSON H. ORTIZ, P.E.
ON THE DATE ADJACENT TO THE SEAL.
PRINTED COPIES OF THIS DOCUMENT ARE
NOT CONSIDERED SIGNED AND SEALED
AND THE SIGNATURE MUST BE VERIFIED
ON ANY ELECTRONIC COPIES.

FOR THE FIRM: NELSON H. ORTIZ (CIVIL)
PE-30358

PROJECT TITLE:
BAHIA CABANA

3001 HARBOR DRIVE
FORT LAUDERDALE, FL 33316
FOLIO NO. 504212240100

ADACHE GROUP ARCHITECTS
550 S. FEDERAL HIGHWAY
FT. LAUDERDALE, FL 33301

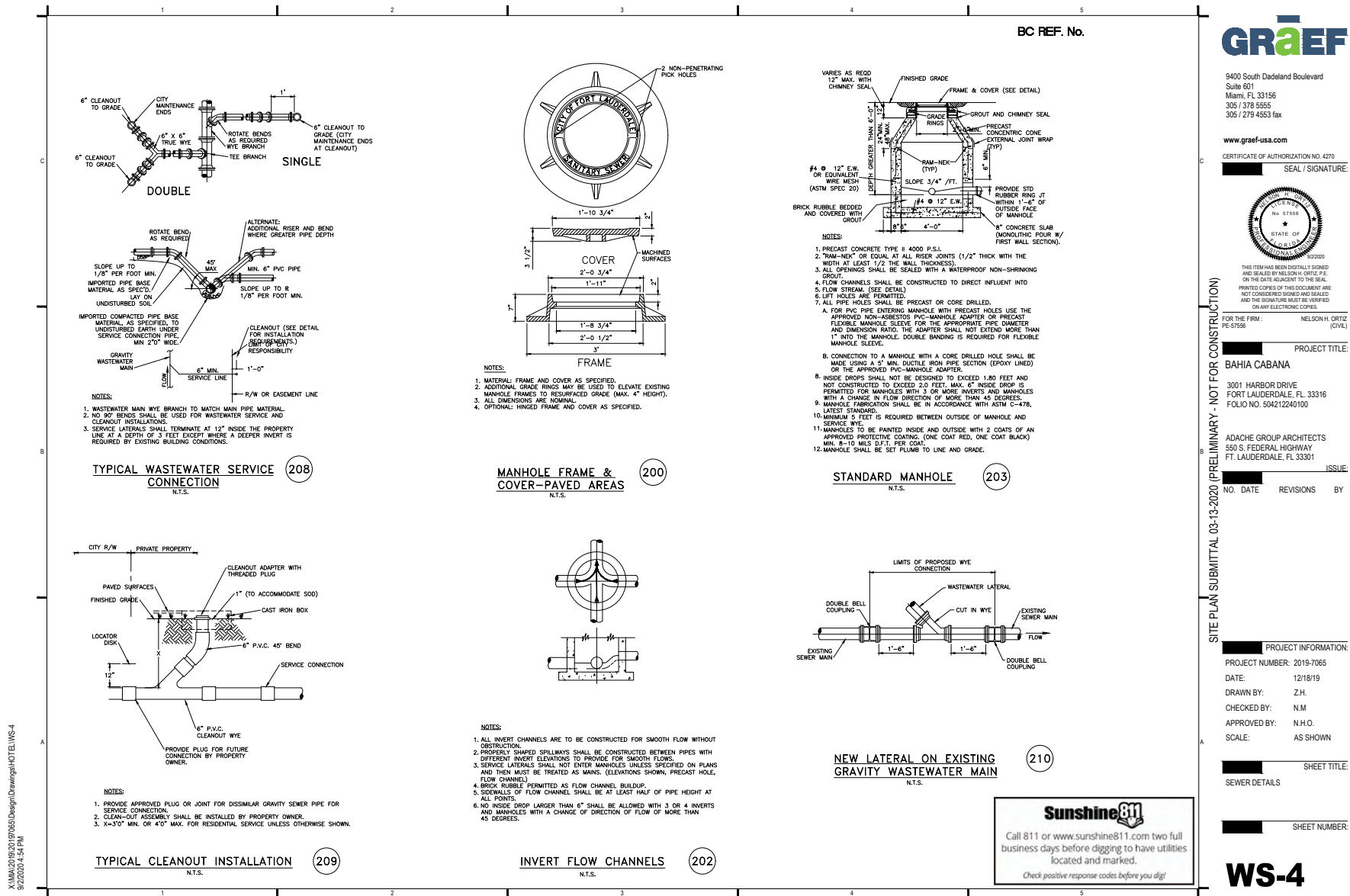
ISSUE:
NO. DATE REVISIONS BY

PROJECT INFORMATION:
PROJECT NUMBER: 2019-7065
DATE: 12/18/19
DRAWN BY: Z.H.
CHECKED BY: N.M.
APPROVED BY: N.H.O.
SCALE: AS SHOWN

SHEET TITLE:
WATER AND SEWER PLAN

SHEET NUMBER:

WS-2



STORMWATER POLLUTION PREVENTION PLAN (SWPPP) NOTES

A. GENERAL NOTES

- ENVIRONMENTAL CONTROL FEATURES AS PROVIDED IN PLANS ARE TO BE INSTALLED AT ALL AREAS OF EXCAVATION OR FILL FOR DRAINAGE SYSTEM OR STRUCTURE CONSTRUCTION PRIOR TO SUCH EXCAVATION OR FILL. INLET ENTRANCES ARE ALSO TO BE PROTECTED FROM SILTATION AS DETAILED IN THE FOOT STANDARDS.
- ALL ENVIRONMENTAL CONTROL FEATURES ARE TO BE MAINTAINED THROUGHOUT THE LIFE OF THE PROJECT IN ACCORDANCE WITH EPA'S NPDES REQUIREMENTS. THE CONTRACTOR MUST ENSURE THAT ALL EROSION CONTROL FEATURES FUNCTION PROPERLY AT ALL TIMES.
- ALL EROSION AND MATERIAL DEPOSITS MUST BE CONTAINED WITHIN THE PROJECT LIMITS.
- TURBIDITY BARRIERS SHALL EXTEND TO THE BOTTOM OF WATER BODY WITH ONE CONTINUOUS PANEL.
- CONTRACTOR'S SUBMITTALS:
THE CONTRACTOR IS RESPONSIBLE FOR SUBMITTING THE FOLLOWING ITEMS TO FDEP IN ORDER FOR THE NPDES PERMIT TO BE PROCESSED:
 - IF THIS SWPPP IS ADOPTED BY THE CONTRACTOR, THE SWPPP SHEETS AND OTHER PLAN SHEETS SHALL BE INCLUDED BY REFERENCE IN THE NOTICE OF INTENT (NOI) SUBMITTAL. THE CONTRACTOR MAY ELECT TO SUBMIT A REVISED SWPPP THAT IS IN COMPLIANCE WITH THE STORM WATER REGULATIONS AT 40 CFR 122.262(a)(4) AND STATE WATER QUALITY STANDARDS.
 - SIGNED CONTRACTOR CERTIFICATION FORMS (2). THESE FORMS MUST BE SIGNED BY BOTH THE PRIME CONTRACTOR AND SUB-CONTRACTOR (IF APPLICABLE) PERFORMING SOIL-DISTURBING ACTIVITIES.
 - HAZARDOUS MATERIAL SPILL CONTROL PLAN.
 - STAGING AREAS, STOCKPILE LOCATIONS AND STABILIZATION PRACTICES.
 - BRIDGE CONSTRUCTION METHOD AND SCHEDULING.
 - DEWATERING PLAN (IF APPLICABLE).
- NO SOIL DISTURBING ACTIVITIES CAN BE CONDUCTED UNTIL THE NPDES PACKAGE HAS BEEN MAILED TO THE FDEP.
- THE CONTRACTOR'S MODIFIED SWPPP MUST ALSO RECEIVE APPROVAL FROM FDOT PERMITS OFFICE PRIOR TO IMPLEMENTATION.

- ANY DAMAGED OR INEFFECTIVE BAGS ARE TO BE REPLACED WITH NEW ONES. THE LOCATION OF ROCK BAG INSTALLATION IS AS MENTIONED IN EROSION CONTROL PLANS. THE PROJECT ENGINEER MAY SPECIFY OTHER AREAS AS NECESSARY.
- DITCH BOTTOM INLETS SHALL BE PROTECTED FROM SEDIMENT INTAKE UNTIL PROJECT IS COMPLETE. ELEVATION OF GROUND OUTLET INLET TOP SHALL NOT BE HIGHER THAN INLET TOP. ROCK BAGS SHALL BE INSTALLED AROUND INLET TOP.
- COMPLETED INLETS IN PAVED AREAS SHALL ALSO BE PROTECTED WITH ROCK BAGS TO PREVENT SEDIMENT INFLUX FROM OTHER AREAS.
- CURB INLETS ALSO SHALL BE PROTECTED FROM SEDIMENT INTAKE UNTIL THE PROJECT IS COMPLETE. ALL FILL, EXCAVATION AND GRAZED AREAS SHALL BE PROTECTED AGAINST EROSION BY METHODS STATED IN "SECTION 804" FOOT STANDARDS SPECIFICATIONS FOR BRIDGE AND ROAD CONSTRUCTION.
- STOCKPILED MATERIALS SHALL NOT BE LEFT IN EROSION PRONE AREAS UNLESS PROTECTED BY COVER AND/OR OTHER APPROPRIATE EROSION CONTROL MEASURES.
- INSPECTION OF EROSION CONTROL MEASURES AND CONDITION OF ADJACENT PROPERTIES SHALL BE PERFORMED DAILY BY THE CONTRACTOR'S REPRESENTATIVE AND THE PROJECT ENGINEER. DEFICIENCIES SHALL BE NOTED AND CORRECTED.
- ANY OFFSITE SEDIMENT DISCHARGE TO A MUNICIPALLY SEPARATE STORM SEWER SYSTEM ARISING FROM THE CONTRACTOR'S ACTIVITIES SHALL HAVE EROSION CONTROLS PROVIDED FOR THOSE INLETS.
- THE AGGREGATE LAYER OF ALL CONSTRUCTION ENTRANCE GRAVEL BEDS MUST BE AT LEAST 6 INCHES THICK. IT MUST EXTEND THE FULL WIDTH OF THE VEHICULAR IMPRESS AND EGRESS AREA. THE LENGTH OF THE ENTRANCE MUST BE AT LEAST 50 FEET. THE ENTRANCE MUST WIDEN AT ITS CONNECTION TO THE ROADWAY IN ORDER TO ACCOMMODATE THE TURNING RADII OF LARGE TRUCKS.

B. SITE DESCRIPTION

- CONSTRUCTION ACTIVITY:
DEMOLITION, DRAINAGE AND UTILITY INSTALLATION, BUILDING AND ROAD CONSTRUCTION, LAND DEVELOPMENT.
PROJECT LIMITS:
HARBOR DRIVE TO THE SOUTH, WATERWAY TO THE NORTH, AND PRIVATE PROPERTY TO THE EAST, AND WEST.
PROJECT DESCRIPTION
LAND DEVELOPMENT ON AN EXISTING VACANT LOT LOCATED AT 3001 HARBOR DRIVE. PROJECT WILL INCLUDE THE CONSTRUCTION OF A NEW 11-STORY HOTEL.
2. MAJOR SOIL DISTURBING ACTIVITIES:
CLEARING AND GRUBBING, REMOVAL OF EXISTING PAVEMENT AND BUILDING, EXCAVATION FOR STORMWATER FACILITIES AND OTHER UTILITIES, PILE DRIVING, PLACEMENT OF FILL FOUNDATIONS, EXCAVATION AND REPLACEMENT WITH FILL IN AREA OF WASTE MATERIAL.
3. TOTAL PROJECT AREA: 0.99 ACRES
TOTAL AREA TO BE DISTURBED: 0.99 ACRES
4. LOCATIONS OF DRAINAGE AREAS AND OUTFALLS:
SEE ATTACHED PLAN.
5. THE DRAINAGE SYSTEM FOR THIS PROJECT IS A CLOSED SYSTEM. RECEIVING WATERS IN THE EVENT OF A MAJOR STORM EVENT WILL BE THE INTRACASTAL WATERWAY APPROXIMATELY 125 FT WEST OF THE PROJECT SITE AT LATITUDE AND LONGITUDE 28°08'37.5"N/ 80°06'24.7"W.
6. AREA OF DISCHARGE FOR THIS PROJECT IS 0.99 ACRES = 43,124 SF
7. SOILS ARE CLASSIFIED AS URBAN LAND COMPLEX AND URBAN LAND. QUALITY OF DISCHARGE IS LIMEROCK FILL AND SAND.
8. LATITUDE AND LONGITUDE OF DIRECT DISCHARGE POINT IS NOT APPLICABLE SINCE THIS IS A CLOSED SYSTEM WITH NO DIRECT DISCHARGE TO ADJACENT SURFACE WATERS. HOWEVER, LATITUDE AND LONGITUDE OF THIS PROJECT AS INDICATED IN THE NOI IS 28°08'37.5"N/ 80°06'24.8"W AND LATITUDE AND LONGITUDE OF THE INTRACASTAL WATERWAY IS 28°08'37.5"N/ 80°06'24.7"W.

C. CONTROLS

- MARRATIVE - SEQUENCE OF SOIL DISTURBING ACTIVITIES AND IMPLEMENTATION OF CONTROLS.
- CONSTRUCTION OF THE 3001 HARBOR DRIVE HOTEL PROJECT DRAINAGE SYSTEM INCLUDES DRAINAGE WELLS AND EXFILTRATION TRENCHES BOUNDED BY HARBOR DRIVE TO THE SOUTH, WATERWAY TO THE NORTH, AND PRIVATE PROPERTY TO THE EAST, AND WEST.
- PRIOR TO COMMENCEMENT OF EACH PHASE OF CONSTRUCTION ACTIVITIES, ALL EROSION CONTROLS PERTAINING TO THAT PHASE MUST BE IMPLEMENTED.
- TEMPORARY STABILIZATION:
DISTURBED PORTIONS OF THE SITE (E.G. BARRIEMENT AT TEMPORARY RAMPS) WHERE CONSTRUCTION ACTIVITY CEASES FOR AT LEAST 31 DAYS, SHALL BE STABILIZED WITH TEMPORARY SOD OR TEMPORARY SEEDING AND MULCHING NO LATER THAN 14 DAYS FROM THE LAST CONSTRUCTION ACTIVITY IN THAT AREA.
- PERMANENT STABILIZATION:
DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITY PERMANENTLY CEASES SHALL BE STABILIZED WITH SOD NO LATER THAN 14 DAYS AFTER THE LAST CONSTRUCTION ACTIVITY.
- EROSION AND SEDIMENT CONTROLS:
 - STABILIZATION PRACTICES:
 - TEMPORARY SODDING
 - TEMPORARY GRASSING
 - PERMANENT SODDING, SEEDING OR SEED & MULCH
 - TEMPORARY MULCHING
 - ARTIFICIAL COVERING
 - BUFFER ZONES
 - PRESERVATION OF NATURAL RESOURCES
 - OTHER:
 - STRUCTURAL PRACTICES:
 - SAND BAGGING
 - SALE FENCES
 - ROCK BAGS
 - SEEDS
 - DIVERSION, INTERCEPTION, OR PERIMETER DITCHES
 - PIPE SLOPE DRAINAGE
 - FLUME
 - THUNDER BEDDING AT CONSTRUCTION EXIT
 - ATCH LANE
 - SEDIMENT TRAPS (DURING DE-WATERING)
 - SEDIMENT BASINS
 - STONE INLET SEDIMENT TRAP
 - STONE OUTLET STRUCTURES
 - CLONE AND GUTTERS
 - STORM SEWERS
 - LOW-LEVEL CONTROL DEVICES
 - TURBIDITY BARRIER
 - NOI MAP
 - DESCRIPTION OF STORM WATER MANAGEMENT:
THE STORMWATER MANAGEMENT SYSTEM CONSISTS OF CATCH BASINS AND STORM SEWERS THAT CONVEY STORMWATER INTO THE DRAINAGE WELLS AND EXFILTRATION TRENCHES THAT TREAT THE WATER QUALITY AND WATER QUANTITY REQUIREMENTS.
 - OTHER CONTROLS:
 - WASTE DISPOSAL:
IN APPROVED OFFSITE AREAS PROVIDED BY THE CONTRACTOR.
 - OFFSITE VEHICLE TRACKING:
 - HAUL ROADS DAMPPED FOR DUST CONTROL
 - LOADED HAUL TRUCKS TO BE COVERED WITH TARP/AULIN
 - EXCESS MUD OR LOAM REMOVED DAILY
 - STABILIZED CONSTRUCTION ENTRANCE
 - OTHER:
 - SANITARY WASTE:
ALL SANITARY WASTE WILL BE COLLECTED FROM THE PORTABLE UNITS AS NECESSARY OR AS REQUIRED BY LOCAL REGULATION OF A LICENSED SANITARY WASTE MANAGEMENT CONTRACTOR.
 - FERTILIZERS AND PESTICIDES:
FERTILIZER TO BE APPLIED ONLY IN THE MINIMUM AMOUNTS RECOMMENDED BY THE MANUFACTURER. ONCE APPLIED, FERTILIZER WILL BE WORKED INTO THE SOIL TO LIMIT EXPOSURE TO STORMWATER. IF STORED ON-SITE, STORAGE WILL BE IN COVERED BINS. THE CONTENT OF ANY PARTIALLY USED BAGS OF FERTILIZER WILL BE TRANSFERRED TO A SEPARATE PLASTIC BIN TO AVOID SPILLS.
 - NON-STORM WATER DISCHARGE (INCLUDING SPILL REPORTING): THE CONTRACTOR IS RESPONSIBLE FOR REPORTING SPILLS TO BROWARD COUNTY DEVELOPMENT & ENVIRONMENTAL REGULATION DIVISION. THE CONSTRUCTION OF THE FOUNDATIONS FOR THIS PROJECT MAY REQUIRE THE DEWATERING SYSTEM WILL BE FILTERED USING A WELL POINT SYSTEM IN ACCORDANCE WITH PERMIT REQUIREMENTS.

REMARKS:

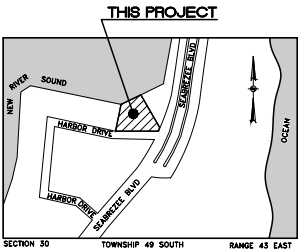
- IF CONTAMINATED SOIL OR GROUNDWATER IS ENCOUNTERED OR HAZARDOUS SPILLS OCCUR DURING CONSTRUCTION THE BROWARD COUNTY DEVELOPMENT & ENVIRONMENTAL REGULATION DIVISION SHALL BE CONTACTED AT THEIR HOTLINE (954) 519-1499.
- APPROVED STATE, LOCAL PLANS, OR STORM WATER PERMITS:
BROWARD COUNTY WATER MANAGEMENT DISTRICT, CITY OF FT. LAUDERDALE AND FLORIDA DEPARTMENT OF TRANSPORTATION.

D. MAINTENANCE

- ITEM:
SILT FENCE
ROCK BAGS
FLOATING TURBIDITY BARRIERS
CONSTRUCTION ENTRANCE
GRAVEL BED
- MAINTENANCE:
ALL CONTROLS SHALL BE MAINTAINED IN PROPER WORKING ORDER AT ALL TIMES DURING CONSTRUCTION.
IF A REPAIR IS NECESSARY, IT WILL BE INITIATED WITHIN 24 HOURS OF BEING NOTED IN CONTRACTOR'S DAILY INSPECTION REPORT.
WHEN THE CONSTRUCTION ENTRANCE GRAVEL BEDS BECOME LOADED WITH SEDIMENTS, REWORK BEDS TO DISPLACE SEDIMENT LOAD AND RE-ESTABLISH EFFECTIVENESS OF THE GRAVEL BEDS.

E. INSPECTION

- THE CONTRACTOR SHALL INSPECT ALL EROSION CONTROL FEATURES AT LEAST ONCE EVERY SEVEN CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A STORM OF 0.5" OR GREATER. IN ADDITION, A DAILY REVIEW OF THE LOCATION OF SILT FENCES SHALL BE MADE IN AREAS WHERE CONSTRUCTION ACTIVITIES HAVE CHANGED THE NATURAL CONTOUR AND DRAINAGE RUNOFF. IN ORDER TO INSURE THAT SILT FENCES AND OTHER EROSION CONTROL DEVICES ARE PROPERLY LOCATED FOR EFFECTIVENESS, A FORM ACCESSIBLE TO THE FDOT WILL BE USED TO REPORT ALL INSPECTION FINDINGS AND CORRECTIVE ACTIONS TAKEN AS A RESULT OF THE INSPECTION. EACH INSPECTION REPORT SHALL BE SIGNED AND SUBMITTED WEEKLY TO THE PROJECT ENGINEER. IF A REPAIR IS NECESSARY, IT WILL BE INITIATED WITHIN 24 HOURS OF BEING NOTED IN CONTRACTOR'S INSPECTION REPORT.



LEGAL DESCRIPTION

- PARCEL 1
LOTS 12 AND 13, OCEAN HARBOR, ACCORDING TO THE PLAT THEREOF, AS RECORDED IN PLAT BOOK 26, PAGE 59, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA.
- PARCEL 2
ALL THAT PROPERTY LYING NORTHERLY OF THE NORTHERLY LINE OF LOTS 12 AND 13, OCEAN HARBOR PLAT, RECORDED IN PLAT BOOK 26, PAGE 59, LYING BETWEEN THE EASTERLY LINE OF LOT 13, EXTENDED, AND THE WESTERLY LINE OF LOT 12, EXTENDED, AND LYING SOUTHERLY OF THE LANDS OF THE CITY OF FORT LAUDERDALE, AS SHOWN ON THAT SKECH RECORDED IN DEED BOOK 684, AT PAGE 142, OF THE PUBLIC RECORDS OF BROWARD COUNTY, FLORIDA.
- SAID LANDS SITUATE, LYING AND BEING IN THE CITY OF FORT LAUDERDALE, BROWARD COUNTY, FLORIDA AND CONTAINING 42,211 SQUARE FEET OR 0.969 ACRES, MORE OR LESS.

PERMIT SET

- MUST BE ON JOB SITE AT ALL TIMES DURING CONSTRUCTION

MATERIALS

- ALL MATERIALS USED AND INSTALLATIONS WITHIN THE PUBLIC RIGHT OF WAY OR EASEMENTS SHALL BE IN ACCORDANCE WITH BROWARD COUNTY HIGHWAY CONSTRUCTION AND ENGINEERING DIVISION SPECIFICATIONS.

NOTICE INSPECTION

- REQUIRED
24 HRS PRIOR TO COMMENCING ANY WORK IN THE PUBLIC RIGHT OF WAY CONTACT THE BROWARD COUNTY HIGHWAY CONSTRUCTION AND ENGINEERING DIVISION AT (954) 519-4600 FOR INSPECTION.

NOTE:

- APPROVAL OF THIS PLAN DOES NOT CONSTITUTE A PERMIT FOR CONSTRUCTION. A PERMIT FOR CONSTRUCTION MUST BE OBTAINED FROM THE BROWARD COUNTY HIGHWAY CONSTRUCTION AND ENGINEERING DIVISION PRIOR TO COMMENCING CONSTRUCTION IN THE PUBLIC RIGHT OF WAY.

- BROWARD COUNTY HIGHWAY CONSTRUCTION AND ENGINEERING DIVISION

- PLAN CONSISTENT WITH PLAN REQUIREMENTS

- PLAN SIGNATURE OF DAY APPROVAL FOR PAVING, GRADING AND DRAINAGE

- DATE: _____ DOES NOT INCLUDE APPROVAL OF PAVEMENT MARKINGS & SIGNS



9400 South Dadeland Boulevard
Suite 601
Miami, FL 33155
305 / 378 5555
305 / 279 4553 fax

www.graef-usa.com

CERTIFICATE OF AUTHORIZATION NO. 4270
SEAL / SIGNATURE



THIS ITEM HAS BEEN DIGITALLY SIGNED AND SEALED BY NELSON H. ORTIZ, P.E. ON THE DATE ADJACENT TO THE SEAL. PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED AND THE SIGNATURE MUST BE VERIFIED ON ANY ELECTRONIC COPIES.

FOR THE FIRM: NELSON H. ORTIZ (CIVIL)

PROJECT TITLE:
BAHIA CABANA

3001 HARBOR DRIVE
FORT LAUDERDALE, FL 33316
FOLIO NO. 504212240100

ADACHE GROUP ARCHITECTS
550 S. FEDERAL HIGHWAY
FT. LAUDERDALE, FL 33301

ISSUE:
NO. DATE REVISIONS BY

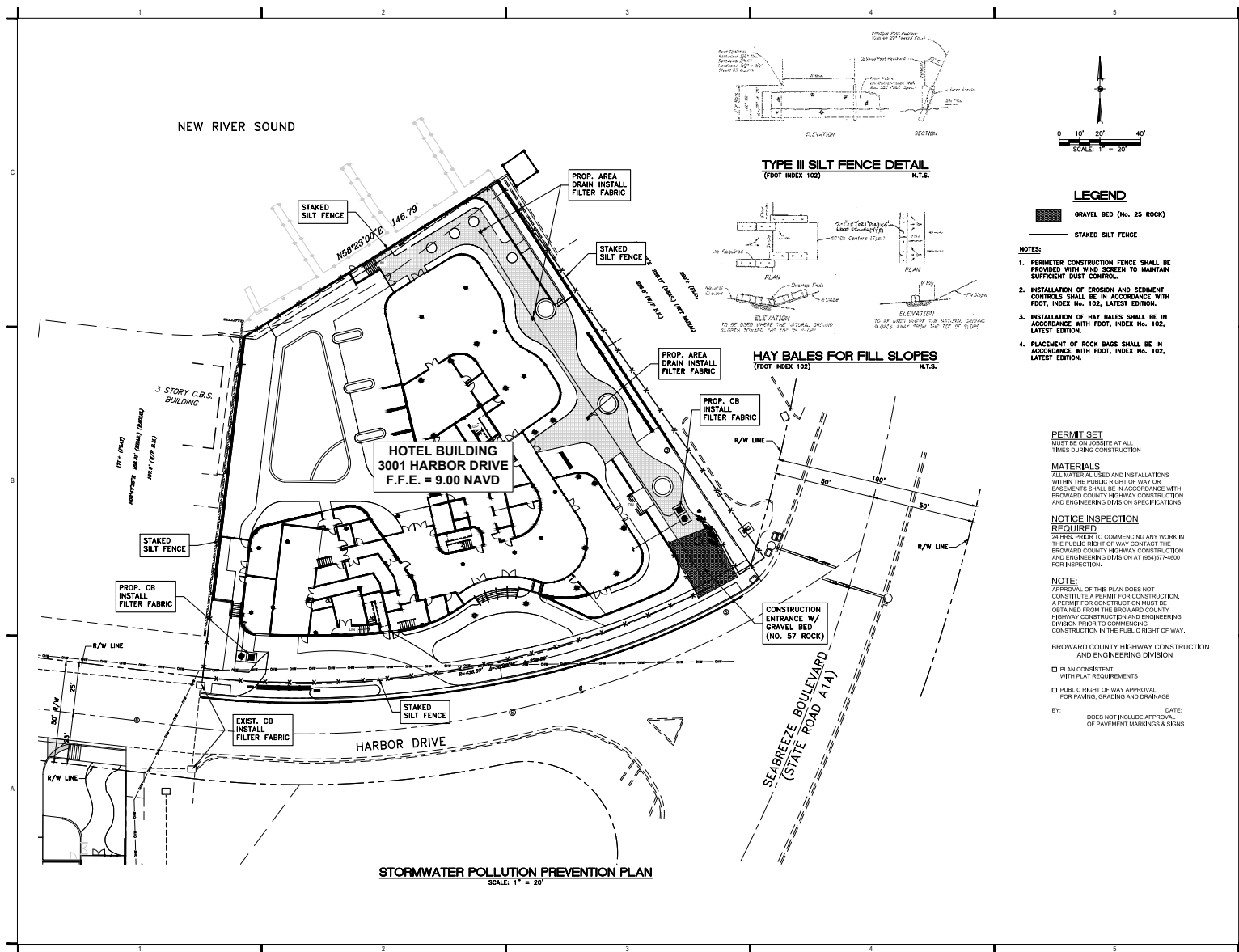
PROJECT INFORMATION:
PROJECT NUMBER: 2019-7065

DATE: 12/18/19
DRAWN BY: Z.H.
CHECKED BY: N.M.
APPROVED BY: N.H.O.
SCALE: AS SHOWN

SHEET TITLE:
STORMWATER POLLUTION PREVENTION NOTES

SHEET NUMBER:

SWPPP-1



GR&EF

9400 South Dadeland Boulevard
Suite 601
Miami, FL 33155
305 / 378 5555
305 / 279 4553 fax

www.graeef-usa.com

CERTIFICATE OF AUTHORIZATION NO. 4270

SEAL / SIGNATURE:



PROJECT TITLE:

BAHIA CABANA

3001 HARBOR DRIVE
FORT LAUDERDALE, FL 33316
FOLIO NO. 504212240100

ADACHE GROUP ARCHITECTS
550 S. FEDERAL HIGHWAY
FT. LAUDERDALE, FL 33301

NO. DATE REVISIONS BY

PROJECT INFORMATION:

PROJECT NUMBER: 2019-7065

DATE: 12/18/19

DRAWN BY: Z.H.

CHECKED BY: N.M.

APPROVED BY: N.H.O.

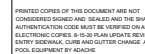
SCALE: AS SHOWN

SHEET TITLE:

STORMWATER POLLUTION
PREVENTION PLAN

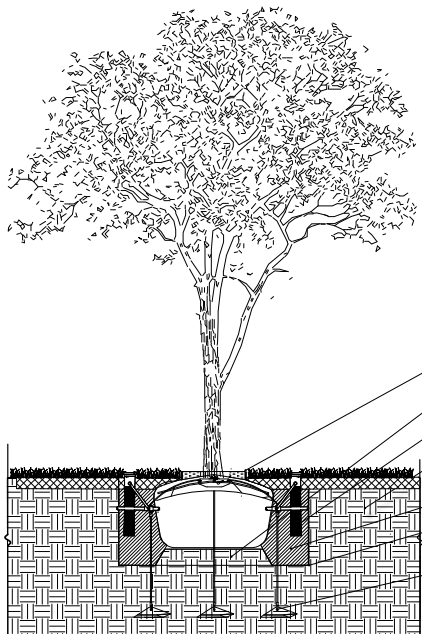
SHEET NUMBER:

SWPPP-2

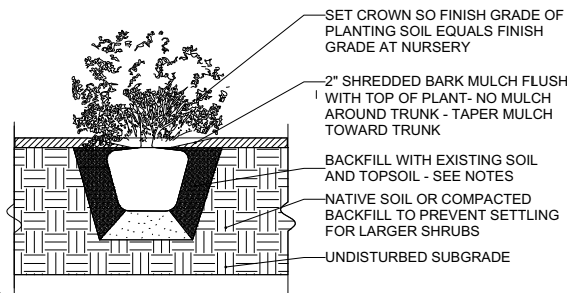


NO					
DEVELOPMENT PROJECT NAME: SANTA CATALINA CONDOROS					
BL#/DO#					
8007 HARBOR DRIVE FOOT LAUDERDALE FL					
PROJECT NUMBER: 10412140200					
8007 HARBOR DRIVE					
FOOT LAUDERDALE, FL					
PROJECT NUMBER: 10412140200					
DOMINANT SPECIES					
	91%				
	40.6%				
BUILDING FOOT PRINT GROUND LEVEL	1.0189				
IMPERVIOUS AREA	14.053				
TOTAL BUILDING AND IMPERVIOUS AREA	16.070				
NET LOT AREA	16.954				
LANDSCAPE REQUIREMENTS	AMOUNT	PROVIDED	REQUIRED	COMMENTS	
TOTAL LANDSCAPE AREA		16.954.00	10.866.50	25 % OF GROSS LOT SQ FT	
TREES					
TREE TOTAL		21	13	8 TREES PER LBS SQ FT OF NET LOT AREA	
SHADE TREES	7	100%		80% OF SHADE TREES ARE NATIVE	
HOW MANY SPECIES		9	5	5 SPECIES FOR SHADE TREE (11 OR MORE)	
GROUNDCOVER AND LAWN					
LOW GROUND COVER			97	600%	COVER NET LOT AREA
GRASS/GRASS AND GROUND COVER	2.900				
NATIVE DROUGHT TOLERANT		15%		50% OF SHRUBS IN LANDSCAPE AREA ARE NATIVE	
WATER-WASH AREA (EXCLUDES TURF)	0.00%			10% OF TURF EXCLUDING TURF OF LANDSCAPE AREA	
VIA AREA	DRIFT OFF ZONE	160 SQ FT	270 SQ FT	50 % OF VIA AREA VIA = 1352	
IMPONED SUSTAINABLE SUSTAINABLE PALM	PALM	3	1 TREE	PER 100 SQ FT = 1 TREE (4 SHRUBS)	
	FRUIT	71	5 SHRUBS		
		0		10% SHADE TREES	
		0	1	10% NATIVE SPECIES OF TREE	
		0	1	10% FLOWERING TREES	
		3	5	10% OPTIONAL TREES	
STREET TREES	8 Shade 3 Palm, 1		870 SQ LBS PER SQ FT	PER 100 SQ LBS PER SQ FT OVERHANG (10 TREES PER 100 SQ LBS TREES AT 1 TREE PER 20 SQ FT AND THE REMAINING TREES ARE NATIVE AS FLOWERING OR PALM TREES	
SHADE TREES FOR STREET TREES		8	4	100% OF STREET TREES ARE NATIVE	

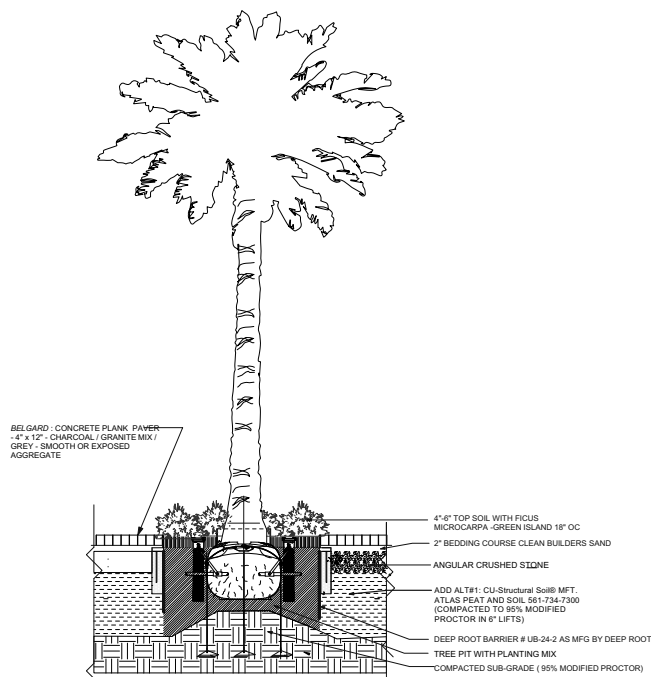
L-1



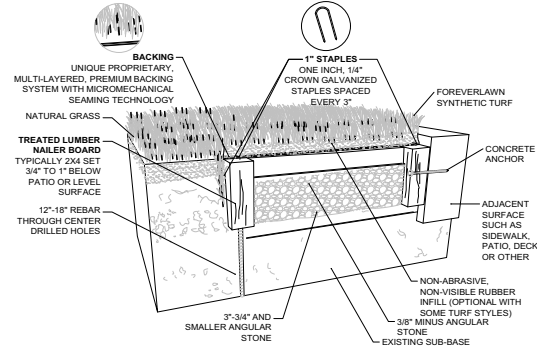
2 TREE PLANTING NTS



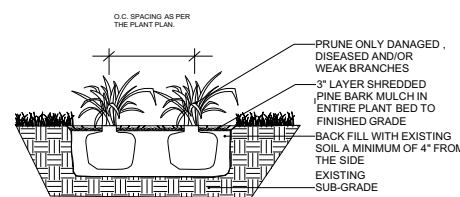
3 SHRUB PLANTING DETAIL NTS



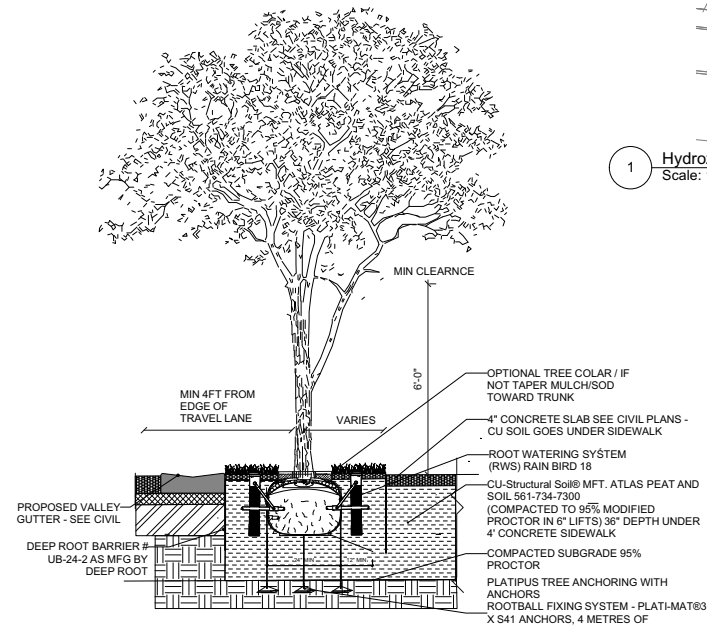
4 PALM PLANTING DETAIL WITH STRUCTURAL SOIL NTS



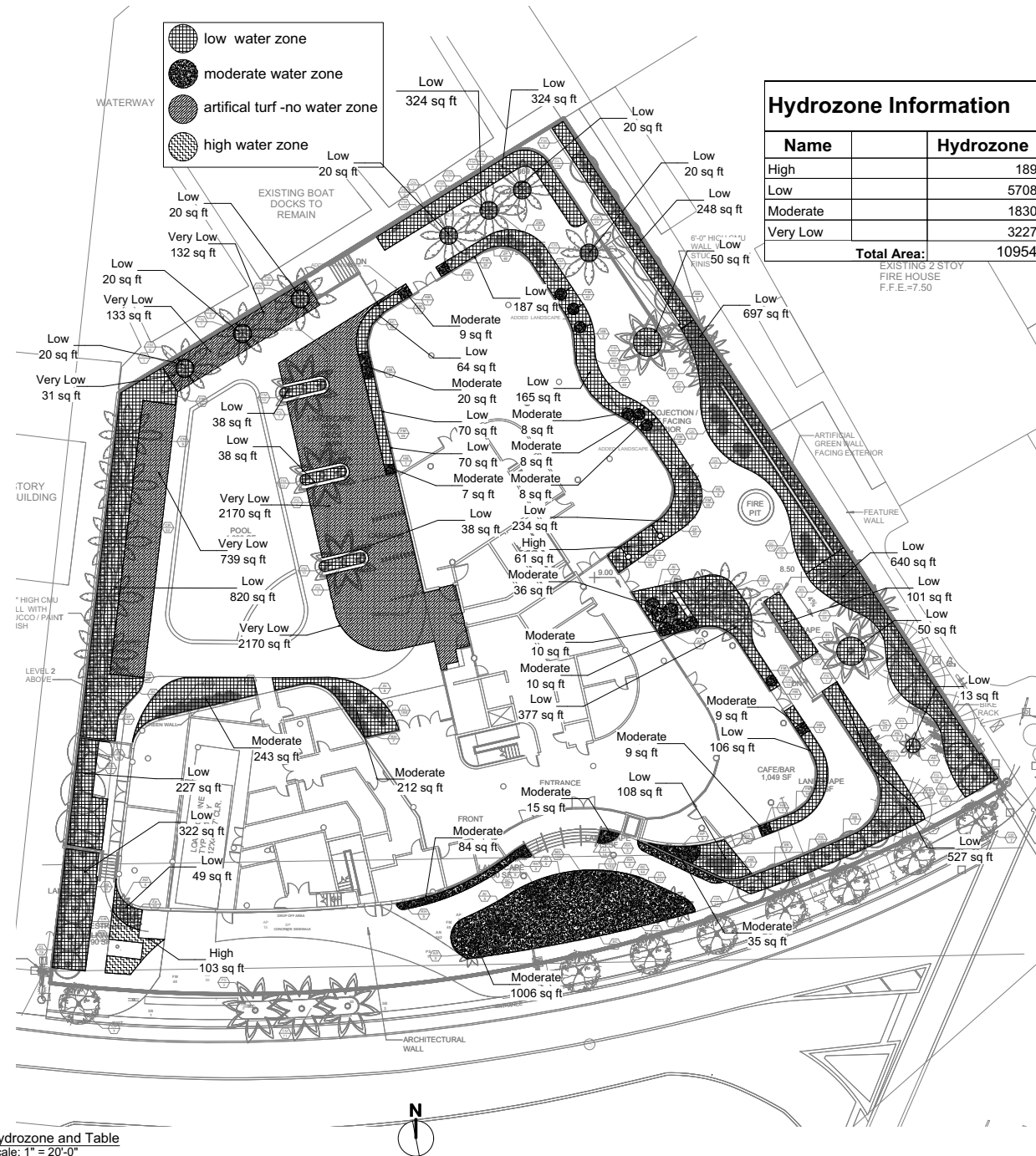
5 ARTIFICIAL TURF DETAIL NTS



6 GROUNDCOVER DETAIL NTS



7 TREE PLANTING AT SIDEWALK AND ROAD NTS



1 Hydrozone and Table Scale: 1\"/>

TOPO
GRAPHIC
landscape architecture & design

TOPO, GRAPHIC LLC
LAW 660853
844-564-5653
Miami Beach FL / Punta Gorda FL
info@topo-graphic.com

All ideas, designs, and indicated or represented by this drawing are owned by and are the exclusive property of TOPO, GRAPHIC LLC and were created, evolved and developed for the use in and in conjunction with the project herein referenced. No such ideas, designs, and plans shall be used or disclosed to any person, firm or corporation for any purpose whatsoever without the written permission of TOPO, GRAPHIC LLC.

All ideas, designs, and indicated or represented by this drawing are owned by and are the exclusive property of ecopacheco llc, and were created, evolved and developed for the use in and in conjunction with the project herein referenced. No such ideas, designs, and plans shall be used or disclosed to any person, firm or corporation for any purpose whatsoever without the written permission of ecopacheco llc.

ecopacheco llc
design consultants + project management
www.ecopacheco.com
(305)815 6626
juan@ecopacheco.com

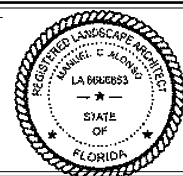
Bahia Cabana Hotel DRC Landscape Plans

3001 HARBOR DRIVE
FORT LAUDERDALE, FLORIDA

Project:

REVISIONS:	REV	#	date	comment
	1	6/14/2020	BY DRC	COMMENTS
	2	8/4/2020	PER DRC	COMMENTS 07.09.20
	3	8/15/2020	ENTRY CHANGE	

SEAL



Drawing Title: Hydro Zone and Code
Date: 3/16/2020
Drawn By:

Sheet No.:

L-2

1 of 1