

**REQUESTS:**

**T14012:** Amendment to the City’s Unified Land Development Regulations (ULDR); Recommend adoption of the Northwest Regional Activity Center – Mixed Use northeast (NWRAC-MU<sub>ne</sub>), Northwest Regional Activity Center – Mixed Use east (NWRAC-MU<sub>e</sub>) & Northwest Regional Activity Center – Mixed Use west (NWRAC-MU<sub>w</sub>) Zoning Districts; providing for each new district the purpose and intent; list of permitted and conditional uses, dimensional requirements and other applicable design standards and development regulations.

**3Z13:** Rezoning; Rezone from Residential Low Rise Multifamily/Medium Density (RM-15) District, Community Business (CB) District, General Business (B-2), Heavy Commercial/Light Industrial Business (B-3) District and General Industrial (I) to Northwest Regional Activity Center – Mixed Use northeast (NWRAC-MU<sub>ne</sub>), Northwest Regional Activity Center Mixed Use east (NWRAC-MU<sub>e</sub>) & Northwest Regional Activity Center – Mixed Use west (NWRAC-MU<sub>w</sub>) Zoning Districts.

<b>Case Numbers</b>	T14012 ULDR Text Amendment and 3Z13 Rezoning						
<b>Applicant</b>	City of Fort Lauderdale						
<b>General Location</b>	NW 6 <sup>th</sup> Street (Sistrunk Boulevard) corridor from the Florida East Coast Railway to the east to NW 24 <sup>th</sup> Avenue to the west & NW 7 <sup>th</sup> Avenue from NW 6 <sup>th</sup> Street (Sistrunk Boulevard) to the north and NW 2 <sup>nd</sup> Street to the south & from the Florida East Coast Railway to the east to Andrews Avenue mid-block to the west to Sunrise Boulevard to the north and NW 6 <sup>th</sup> Street (Sistrunk Boulevard) to the south.						
<b>Property Size</b>	Approximately 130 acres						
<b>Existing Zoning</b>	Community Business District (CB), General Business (B-2), Heavy Commercial/Light Industrial Business (B-3) District, General Industrial (I) and Residential Low Rise Multifamily/Medium Density (RM-15) District						
<b>Proposed Zoning</b>	Northwest Regional Activity Center – Mixed Use northeast (NWRAC-MU <sub>ne</sub> ) Northwest Regional Activity Center – Mixed Use east (NWRAC-MU <sub>e</sub> ) Northwest Regional Activity Center – Mixed Use west (NWRAC-MU <sub>w</sub> ) Zoning Districts						
<b>Existing Use</b>	Those uses allowed in the existing CB, B-2, B-3, I & RM-15 zoning districts						
<b>Future Land Use Designation</b>	Northwest Regional Activity Center						
<b>Applicable ULDR Sections</b>	47-13 Regional Activity Center Districts 47-20 Parking and Loading Requirements						
	<b>Existing</b>					<b>Proposed</b>	
	<b>CB</b>	<b>B-2</b>	<b>B-3</b>	<b>I</b>	<b>RM-15</b>	<b>NWRAC-MU<sub>ne</sub> &amp; NWRAC-MU<sub>e</sub></b>	<b>NWRAC-MU<sub>w</sub></b>
<b>Maximum Building Height</b>	150 ft	150 ft	150 ft	150 ft	35 ft	* 65 ft	* 45 ft
<b>Minimum Lot Size (sf)</b>	N/A	N/A	N/A	N/A	5,000	N/A	
<b>Minimum Lot Width</b>	N/A	N/A	N/A	N/A	50 ft	N/A	
<b>Maximum FAR</b>	N/A	N/A	N/A	N/A	N/A	N/A	
<b>Minimum Front Yard</b>	**5 ft	**5 ft	**5 ft	**5 ft	25 ft	*** 0 ft (Primary Street) / 5 ft (NW 7 <sup>th</sup> Ave) & all Secondary Streets	
<b>Minimum Corner Yard</b>	5 ft	5 ft	5 ft	5 ft	***25 ft		
<b>Minimum Side Yard</b>	None	None	None	None	5 ft	0 ft	
<b>Abutting Residential</b>	10 ft	15 ft	20 ft	30 ft		15 ft	
<b>Minimum Rear Yard</b>	None	None	None	5 ft	15 ft	0 ft	

Abutting Residential	15 ft	15 ft	20 ft	30 ft	15 ft	15 ft	
<b>Maximum Shoulder Height</b>	N/A	N/A	N/A	N/A	N/A	65 ft (5 stories)	N/A
Abutting Residential	N/A	N/A	N/A	N/A	N/A	45 ft	
<b>Minimum Tower Stepback</b>	N/A	N/A	N/A	N/A	N/A	12 ft (Primary St) 15 ft (Secondary St)	N/A
Abutting Residential	N/A	N/A	N/A	N/A	N/A	15 ft	
<b>Maximum Tower Floorplate</b>	N/A	N/A	N/A	N/A	N/A	Max Commercial 32,000 sq ft Max Residential 12,000 sq ft	
<p>* SP Level II, subject to City Commission Approval, if development requests within the NWRAC-MUe &amp; NWRAC-MUe Zoning District exceeds sixty-five (65) feet in height, up to a maximum of one hundred ten (110) feet, or within the NWRAC-MUw zoning district that exceeds forty-five (45) feet in height, up to a maximum of sixty-five (65) feet.</p> <p>** 5-foot yard is the minimum, but in no case shall the front yard be less than 35-feet from the centerline of the abutting right-of-way</p> <p>*** 25% of lot width but not less than 10 ft nor greater than 25 ft.</p> <p>**** Setback is based on street cross-sections as indicated in the NW-RAC Illustrations of Design Standards</p>							
	<b>Existing</b>				<b>Proposed</b>		
<b>Parking – Residential</b>	As required by Section 47-20, Parking and Loading Requirements				As required by Section 47-20, Parking and Loading Requirements.  Bicycle Parking – 1 per 10 dwelling units		
<b>Parking – Non-Residential</b>	As required by Section 47-20, Parking and Loading Requirements				Exempt for development between 0 and 2,500 gross square feet / 60% of parking as required by Section 47-20, Parking and Loading Requirements for the remainder.  Bicycle Parking – 1 per 20 parking spaces provided		
<b>Notification Requirements</b>	Sign Notice 15 days prior to meeting, Newspaper notice 10 days prior to meeting date / Mail notice to all property owners within the proposed area to be rezoned and all property owners within 300-ft of the proposed area to be rezoned/sign notice						
<b>Action Required</b>	Recommend Approval, Approval with Conditions or Denial of the proposal to the City Commission						
<b>Authored By</b>	Linda Mia Franco, AICP, Principal Planner Anthony Greg Fajardo, Zoning Administrator						

**PROJECT DESCRIPTION:**

To further enhance the area and meet the vision and goals of the community as indicated in the 2006 Sistrunk Boulevard Urban Design Improvement Plan and the 2008 Northwest/Progresso /Flagler Heights Implementation Plan, this proposal includes the creation of three new zoning districts. The Northwest Regional Activity Center – Mixed Use northeast (NWRAC-MUe), Northwest Regional Activity Center – Mixed Use east (NWRAC-MUe) and Northwest Regional Activity Center – Mixed Use west (NWRAC-MUw) zoning district.

The plans referenced above, envision a vibrant community, with a successful mix of business and residential uses defined with walkable streets and quality buildings through the creation of guidelines that

would enhance the pedestrian realm and give clear intent for an active street level and an exceptional public realm experience.

As part of the implementation process to date, the City has taken action to address some of the issues of concern for the area. Through the CRA, the City has established programs for improvements to certain existing sites. In 2013, the City revised the ULDR to prohibit new liquor and convenience stores within the Northwest Regional Activity Center (“NWRAC”) land use, based on crime statistics that indicated these uses have had a negative impact on the area. In addition, the Fort Lauderdale Housing Authority has recently completed several quality projects in the general area furthering goals of livability, sustainability and sense of community, with additional projects planned in the future.

The new zoning districts will establish the basis for appropriate uses that will enhance development and redevelopment efforts in the study area, as well as require development sites to meet specific design standards, through implementation of design standards intended to promote an active urban area, while allowing flexibility and creative design solutions to meet the intent of the ordinance, as has been successfully realized with development projects subject to the Downtown Master Plan area, as well as the South Andrews Avenue Master Plan.

### **BACKGROUND:**

A significant amount of analysis for the area has taken place to date. In 2006, the Sistrunk Boulevard Urban Design Improvement Plan was created to study appropriate land uses and development types for the Sistrunk Boulevard corridor. This study recommended amendments to the Unified Land Development Regulations (“ULDR”) to facilitate the implementation of changes to the study area that were identified through community input and would facilitate implementation of the plan, identify opportunities and develop strategic priority plans.

In 2008 the Northwest/Progresso /Flagler Heights Implementation Plan (“NPF CRA”) was created with the intent of being an agent for change in the target area. This area included the boundaries of the community redevelopment area (“CRA”), which is included within the Northwest Regional Activity Center (“NWRAC”) land use. This plan set goals to accomplish positive redevelopment opportunities through revitalization of the area, recognition of history and enhancing the general image of the area.

The approach to the Sistrunk Boulevard/NW 6<sup>th</sup> Street planning process was built on a strategy of consensus involving extensive public outreach. The strategy was designed with three major goals in mind:

- To elicit community input & sustain participation throughout the planning process;
- To inform all stakeholders about the steps, progress, and outcomes of the process;
- To achieve community consensus and buy-in regarding the proposed improvements and initiatives.

The project initially began in December 2002 with a series of individual and small group interviews with over 100 community stakeholders. The list of interviewees comprised neighborhood associations, business and civic organizations, members of the general public, and City and County representatives including staff and elected officials. The input from the interviews informed the design of the various corridor elements.

A report was produced summarizing the results of this initial sequence of interviews. Based on the input received during the stakeholder meetings, the consultant team presented preliminary design options for the street and urban design framework at two public workshops held on February 27 and March 1 of 2003. While more than 100 persons attended these two meetings, the CRA also hosted over a dozen additional presentations before individual community, faith-based, and neighborhood groups after the public workshops to maximize public involvement in this early stage. This process led to a third public workshop, held on June 28, 2003, during which consensus-based concepts for land use mix and development types, roadway and streetscape design were discussed resulting from the preceding series of meetings and were presented to an audience of over 75 citizens. The workshop was conducted as an interactive forum which encouraged extensive input. The proposed corridor concepts were then presented to the CRA Advisory Board on July 16, 2003 for endorsement.

Progress on the project continued for two years, during which the City conducted some additional meetings, published a newsletter and periodically updated the community on development of the plan. The overall process provided citizens with numerous opportunities to offer input, suggestions and recommendations, which are addressed in the Urban Design Improvement Plan. As a result, in 2006, the Sistrunk Boulevard Urban Design Improvement Plan was completed and accepted by the City Commission, followed by numerous community meetings that resulted in the 2008 Northwest/Progresso /Flagler Heights Implementation Plan (“NPF CRA”), which focused on creating a successful implementation strategy.

In addition, as part of the process to adopt the current proposed amendments, the following outreach efforts were incorporated to date:

- March 2013 to November 2013 - completed over 11 Neighborhood Outreach Meetings that included various community workshops, which were attended by residents of the community, stakeholders representing the Midtown Business Association, developers, and realtors.
- January 2014 – Completed Downtown Update:  
Adoption of Transit Oriented Development (“TOD”) Guidelines, where some stakeholders within the “triangle” area, located west of the FEC tracks and just south of Sunrise Boulevard within the NWRAC Land Use indicated they wanted to be included in the TOD Guidelines, which encourage reduction in parking standards to promote alternative means of transit and support mixed-use development. As a result, the City Commission asked staff to include this area as part of the NWRAC Master Plan initiatives.
- June 19, 2014 – The Urban Land Institute (“ULI”) hosted a Technical Assistance Panel (“TAP”) to review and comment on the Sistrunk Corridor Zoning Code changes.
- September 25, 2014 – Open House Meeting at the Department of Sustainable Development to present final recommended Zoning Code changes for the NWRAC Master Plan to the community and stakeholders prior to presenting to the Planning and Zoning Board and City Commission.
- October 22, 2014 - Presented to the CRA Advisory Board
- November 4, 2014 – Presented to the Chamber’s Government Affairs Committee

Staff has also been in contact with numerous local churches and other community members on various occasions, offering opportunities for comments and questions. A mailer was provided to all property owners impacted by the proposal within the study area, as well as to those outside the study area within 300-feet, and information is provided on the City’s website regarding the proposed amendments.

In summary, this on-going 12-year effort, which has to date included a significant amount of community outreach and input, is primed for implementation of changes to the built environment that will result in enriched sense of place and successful overall environment. While the proposed recommendations attempt to accurately capture the preferences of both the development community and residents not all voiced requests can always be accommodated. However, staff strongly believes the proposed recommendations strike a suitable balance for all parties, while addressing sound planning principles to meet the larger goals.

**STAFF RECOMMENDATION:**

**Proposed Zoning District:** The proposed ULDR amendments will rezone the Sistrunk Boulevard and NW 7th Avenue corridors to “Mixed-Use” zoning districts (referred to as NWRAC-MUe, NWRAC-MUe and NWRAC-MUw, respectively, as shown in the Study Area Map attached as **Exhibit 1**). These proposed districts will provide three areas with design standards established to preserve the character of these areas and in keeping with the recommendations of the Implementation and Improvement Plans.

**Proposed Uses:** The permitted and conditional uses included in the current zoning districts consist of CB, B-2, B-3, I and RM-15 (please see the existing and proposed Zoning Map is attached as **Exhibit 2**). To promote redevelopment of the study area with active urban uses, the proposed zoning districts will

include uses as indicated in the CB zoning district, as well as some low-intensity B-2 zoning district uses. All other intensive (quasi-industrial and industrial) uses that are currently permitting in the B-2, B-3 and I zoning districts will be restricted to promote a livable and active urban environment. In addition Pawn Shop will not be permitted within the new zoning districts. Any current use that will become non-conforming may remain subject to Section 47-3, *Nonconforming Uses, Structures and Lots*.

<b>USE TABLE</b>		
<i>Existing Zoning Districts</i>	<b>NWRAC-MUe (east) and NWRAC-MUw (west)</b>	<b>NWRAC-MUe (northeast)</b>
<b>CB</b> (Community Business Zoning District)	Limited CB Uses	N/A
<b>B-2</b> (General Business Zoning District)	Limited B-2 Uses	Limited B-2 Uses
<b>B-3</b> (Heavy Commercial/Light Industrial Zoning District)	Limited B-3 Uses	N/A
<b>I</b> (General Industrial Zoning District)	N/A	N/A
<b>RM-15</b> (Residential Low-Rise Multifamily/Medium Density Zoning District)	Residential Uses	Residential Uses
<b>NOTE:</b> The intent is to allow a mix of residential and non-residential uses throughout the NWRAC-MU zoning districts, which are intended to be compatible with the existing residential urban fabric and promote redevelopment efforts by contributing to a livable and active urban area.		

**Parking:** The recent streetscape improvements along Sistrunk Boulevard created additional on-street parking that adds to the existing parking supply. In addition, the area has struggled with redevelopment due to current parking requirements that can stifle adaptive reuse of existing buildings when additional parking is required. Staff is proposing a reduced parking standard in the area as was successfully done in the South-RAC. These recommendations are consistent with parking requirements for the Downtown-RAC Transitional Mixed Use (“TMU”) zoning districts as well. The intent of this recommendation is to encourage existing properties that may not have adequate land area to meet current parking requirements to redevelop and also serves as a redevelopment incentive to focus more on the pedestrian and public realm experience while promoting alternative means of transit.

The NWRAC-MU (northeast, east and west) Zoning Districts proposed parking requirements are as follows:

<b>PARKING REQUIREMENTS</b>		
<i>Use</i>	<i>Parking Space Requirements</i>	<i>Loading Zone Requirements</i>
<b>Residential</b>	See Parking requirements for uses as provided in Table 1.  Bicycle Parking – 1 per 10 dwelling units	N/A
<b>Non-Residential</b>	Exempt for development between 0 and 2,500 gross square feet in area. All development greater than 2,500 gross square feet shall be required to provide 60% of the existing parking space requirements for uses as provided in Table 1.  Bicycle Parking – 1 per 20 parking spaces provided	See Loading requirements for uses as provided in Table 2.

The proposed NWRAC-MU zoning districts allow for a reduced parking requirement for non-residential uses only. All residential uses are required to provide the full parking count as outlined in Section 47-20, Parking and Loading Requirements of the ULDR.

**Dimensional Requirements:** Currently there are a variety of dimensional requirements corresponding to the existing zoning districts located within the study area. These requirements are associated with CB, B-2, B-3, I and RM-15 zoning districts. These differing dimensional requirements yield a variety of development types within the study area and can come into conflict with the intent behind the proposed zoning districts.

The proposed NWRAC-MU zoning districts are intended to standardize dimensional requirements to achieve a consistent form of pedestrian-oriented development within the study area. The intent of these standardized dimensional requirements is as follows:

- Provide a consistent build-to-line (street-edge) to enhance the pedestrian experience.
- Providing delineation of a street-edge through the limitation of space between buildings along the street.
- Enhancing the pedestrian experience through regulation of sidewalk width.
- Providing sufficient light and air through setback regulations and address transition zones between non-residential abutting residential.

The proposed NWRAC-MU<sub>ne</sub> (FEC tracks west to mid-block of Andrews Avenue, south of Sunrise Blvd.) & NWRAC-MU<sub>e</sub> (located east of NW 7<sup>th</sup> Avenue) will allow a maximum height of sixty-five (65) feet by right and up to ten (10) stories or one hundred ten (110) feet as a Site Plan Level II review with City Commission approval. This allows for more intensive development to occur within Sistrunk Boulevard and NW 7<sup>th</sup> Avenue, which connects the study area to Broward Boulevard, the Downtown and the Arts and Entertainment District (please see Study Area Map attached as **Exhibit 1**).

The proposed NWRAC-MU<sub>w</sub> (located west of NW 7<sup>th</sup> Avenue) will be lower in height by allowing a maximum height of forty-five (45) feet by right and up to five (5) stories or sixty-five (65) feet as a Site Plan Level II review with City Commission approval (please see Study Area Map attached as **Exhibit 1**).

The height recommendations for this design proposal have been previously studied in the 2008 NPF CRA Implementation Plan, which references the same recommendations in reports by the Marjorie Davis Charrette (1999) and the Wallace, Roberts and Todd Study (2005).

The comparison of the existing vs. proposed dimensional regulations are noted as part of the request above and are attached as **Exhibit 3**.

**Design Standards:** To further implement the proposed NWRAC-MU zoning districts, the proposal includes design standards for the area consisting of clearly-defined graphic examples of what proposed development within the NWRAC-MU districts should consist of. This document is titled the NWRAC-MU: Illustration of Design Standards (“NWRAC-MU Design Standards”) and it provides clear and specific standards for any proposed development within the area through the use of form-based codes and examples of successful design elements. The design standards will offer more certainty in the review and approval process than current requirements. In effect, this will provide a means by which to address “neighborhood compatibility” by offering clear and specific standards that property owners and developers can successfully address, providing for a mix of uses while maintaining the character of neighboring areas.

The proposed design standards for the study area are consistent with principles established by existing master planned areas within the City (Downtown RAC; South RAC; Beach RAC). In addition, they are consistent with the recommendations of the 2006 Sistrunk Boulevard Urban Design Improvement Plan and the 2008 NPF CRA Implementation Plan.

The design standards address such issues as transition zones between proposed development within the study area and existing residential development abutting and outside the study area, by providing criteria for additional yard setback, lower shoulder height and additional tower setback that addresses compatibility; establishes a street cross-section to provide for a consistent street edge through a build-to-line; suitable pedestal and shoulder heights, as well as tower regulations (if towers are proposed where applicable); requirements for on-street parking and street trees; examples of street furniture; and provisions for open space requirements among others.

Please see the NWRAC-MU: Illustration of Design Standards attached hereto as **Exhibit 4**.

**Additional Amendments:** Related amendments are being proposed to Section 47-13, Regional Activity Center Districts to incorporate the NWRAC-MU zoning districts. These include amendments to the use tables to include the new zoning districts and a consolidation of text regarding the existing South Regional Activity Center – South Andrews (SRAC-SA) zoning districts. While these amendments require revisions

to the SRAC-SA language, the SRAC-SA intent, content and dimensional requirements have not been changed and remain the same as adopted previously. In addition, to further streamline the listed uses, the RAC use tables have been revised to eliminate redundancy. However, none of the previous uses for the existing zoning districts have been removed or altered in any way.

These include the following:

- General consolidation of Automotive Uses;
- Consolidation of Trade/Business School with School in the Public Purpose category;
- Relocation of Pharmacy to remain consistent with alphabetization;
- General consolidation of Dry Cleaner, Laundering Plant.

For more information, please see the draft NWRAC-MU Zoning Ordinance, attached as **Exhibit 5**.

**Development Review Process:** While the creation of form-based codes and design standards offers a more predictable built environment for developers and the broader community, a streamlined development review process is being proposed to further encourage quality development and redevelopment.

The current requirements for development review depend on a variety of thresholds. These can include site plan level I, site plan level II (Development Review Committee), site plan level III (Planning and Zoning Board); and site plan level IV (City Commission), depending on the type of development proposed. The requirements add to the uncertainty of development within the City as well as adding cost and time to the review process. In order to provide a level of certainty to the development review process, as well as streamline it where appropriate, staff is recommending a similar approach to the Downtown-RAC and the South-RAC, which has proven to be successful.

The proposed requirements for development review within the NWRAC-MU zoning districts consist of the following:

DEVELOPMENT PERMIT APPROVAL PROCESS						
	Existing Zoning Districts					Proposed Zoning Districts
	CB	B-2	B-3	I	RM-15	NWRAC-MU
<b>Residential</b>	Not Permitted	Not Permitted	Not Permitted	Not Permitted	* SP Level I	SP Level II
<b>Mixed-Use</b>	SP Level III	SP Level III	Not Permitted	Not Permitted	** SP Level III	*** SP Level II
<b>Non-Residential</b>	* SP Level I	* SP Level I	* SP Level I	* SP Level I	* SP Level II	SP Level II
<b>Conditional Uses</b>	SP Level III	SP Level III	SP Level III	SP Level III	SP Level III	SP Level III

**NOTE:** Development requests within the NWRAC-MUe & NWRAC-MUw zoning district that exceeds sixty-five (65) feet in height, up to a maximum of one hundred ten (110) feet, or within the NWRAC-MUw zoning district that exceeds forty-five (45) feet in height, up to a maximum of sixty-five (65) feet shall be reviewed as SP Level II, subject to City Commission Approval. Conditional Use proposals shall be reviewed as SP Level III.

<b>SP Level I</b> Administrative Review (AR)	*	Site Plan level II for nonresidential development >5,000 sf or residential > 4 units
<b>SP Level II</b> Development Review Committee (DRC)	**	Subject to location criteria
<b>SP Level III</b> Planning and Zoning Board (PZB)	***	Mix of uses (both residential and non-residential in nature) shall be permitted

The City of Fort Lauderdale, through the Vision Plan and Strategic Plan, has set the goal of neighborhood enhancement by revising the City’s Code of Ordinances to allow neighborhood development to become more mixed-use, diverse, sustainable and affordable throughout Fort Lauderdale. This amendment is intended to further the goals of the *2035 Vision Plan: Fast Forward Fort Lauderdale*, adopted by the City Commission on April 16, 2013. This plan collected over 1,500 unique ideas from various members of the community through an extensive outreach program and developed a vision of the City of Fort Lauderdale for the year 2035 (to read the entire 2035 Vision Plan please go to <http://www.fortlauderdale.gov/vision/>).

In addition the *Press Play Fort Lauderdale Strategic Plan 2018* was adopted by the City Commission to establish a 5-year period in which to accomplish specific goals and objectives as outlined in the Vision Plan (to read the entire 2018 Strategic Plan please go to <http://www.fortlauderdale.gov/pressplay/>).

These two documents lay the framework for changes to the City's Unified Land Development Regulations to help reach the City's long term vision of neighborhood enhancement by allowing mixed-use development, and redevelopment flexibility that is pedestrian-friendly, visually-interesting, safe, comfortable and connected to various modes of transportation that will help to physically and socially connect our community.

This ULDR amendment is part of the *Press Play Fort Lauderdale Strategic Plan 2018* initiative, included in the *Infrastructure* and *Neighborhood Enhancement* Cylinders of Excellence, specifically advancing:

- Goal 6: Be an inclusive community made up of district, complementary, and diverse neighborhoods.
- Objective 1: Evolve and update the land development code to balance neighborhood quality, character, and livability through sustainable development.
- Initiative 1: Examine land-use patterns and neighborhood development trends to recommend changes to the Unified Land Development Regulations (ULDR) for optimal neighborhood growth, including parking, landscaping, setbacks, change of use and reuse, etc.
- Initiative 6: Amend the Northwest Regional Activity Center (NWRAC) zoning regulations and implement design guidelines.

**Comprehensive Plan Consistency:**

Staff has determined the proposed amendments described herein are consistent with the City's Comprehensive Plan. The specific Goals, Objectives and Policies are as follows:

- ELEMENT: Future Land Use Element
- GOAL: Goal 1
- OBJECTIVE: Objective 1.8 & 1.10
- POLICIES: Policy 1.8.3 & 1.10.4

The proposed Northwest Regional Activity Center-Mixed Use zoning districts (NWRAC-MUe, NWRAC-MUe & NWRAC-MUw) general further the City's Comprehensive Plan in that the Northwest Regional Activity Center future land use designation was established to provide for positive redevelopment by permitting a mix of uses that allows for retail along with residential uses and stand-alone multifamily/residential development.

The NWRAC-MU zoning districts promote the intent of the Northwest Regional Activity Center by permitting a mix of uses to occur and by encouraging active retail, office and residential uses throughout the study area.

**PLANNING & ZONING BOARD REVIEW OPTIONS:**

**T14012:** The Planning and Zoning Board acting as the Local Planning Agency shall determine whether the proposed amendments to the Unified Land Development Regulations ("ULDR") are consistent with the City of Fort Lauderdale's Comprehensive Plan and whether the Planning and Zoning Board recommends approval of the proposed amendments to the City Commission;

and

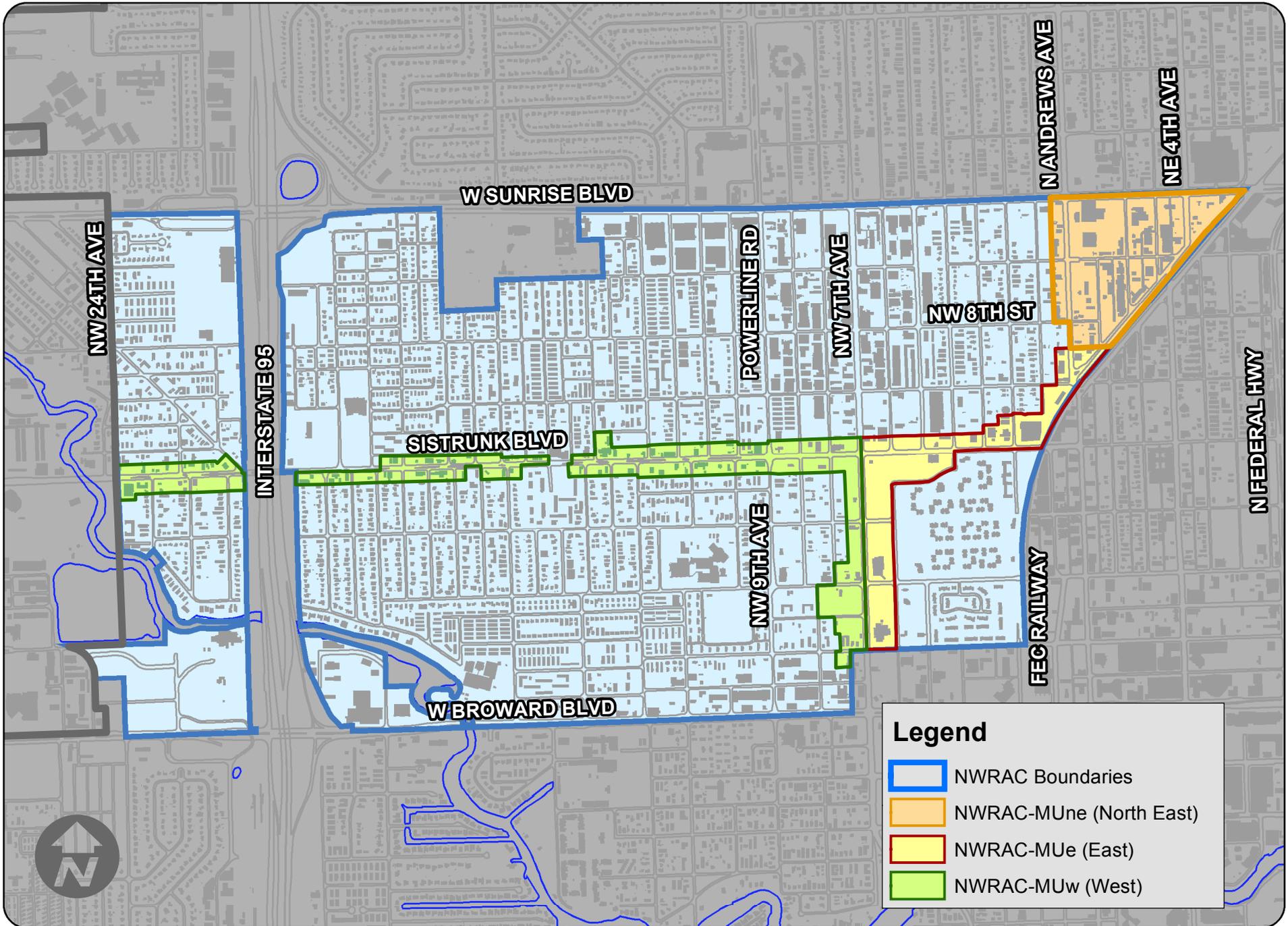
**3Z13:** If the Planning and Zoning Board determines that the application meets the criteria for rezoning, the recommendation shall be forwarded to the City Commission for consideration.

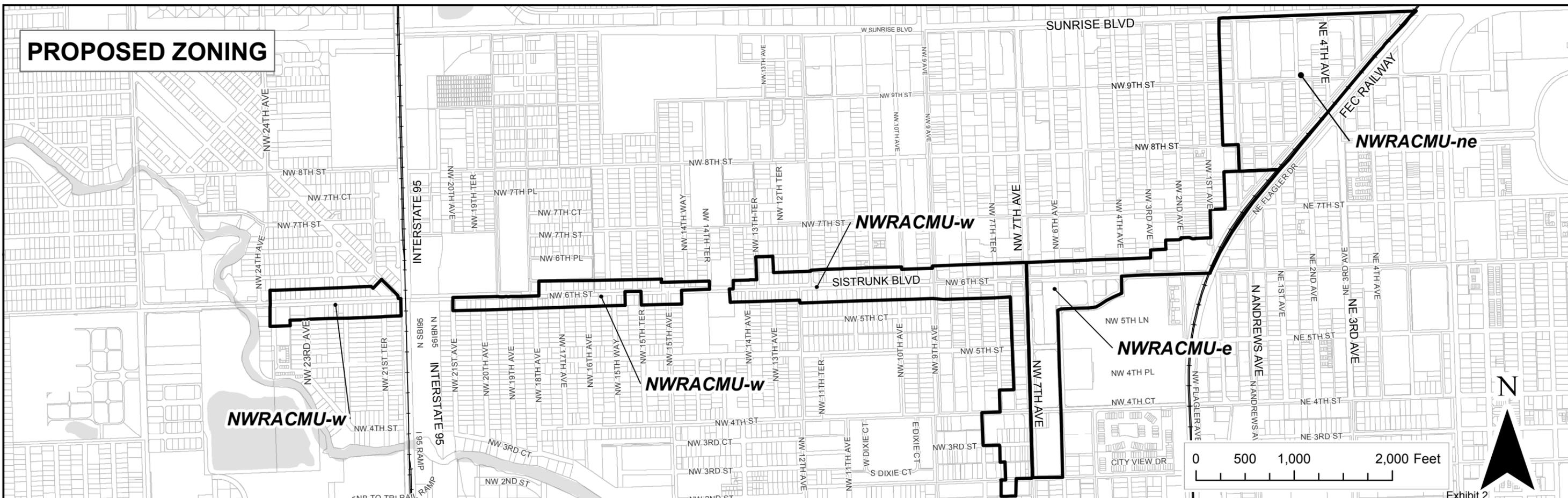
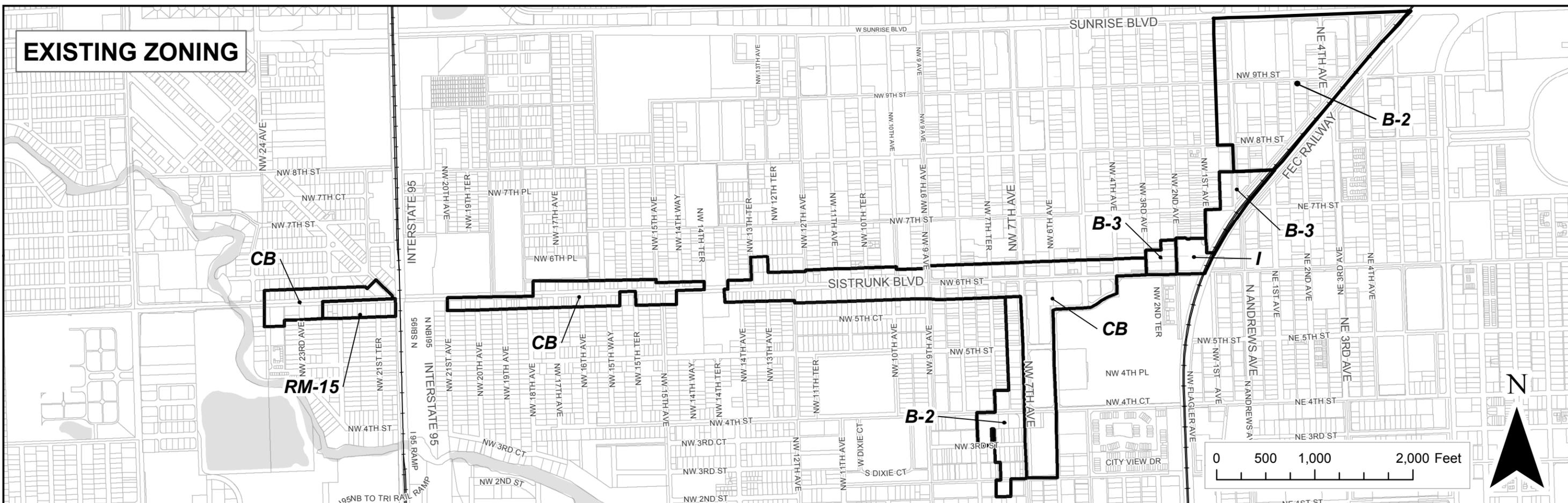
If the Planning and Zoning Board determines that the criteria for rezoning have not been met, the Board shall deny the application for appeal to the City Commission as provided in Section 47-26.B, Appeals, shall apply.

**EXHIBITS:**

1. Study Area Map
2. Existing and proposed Zoning map
3. Comparison of Dimensional Regulations
4. NWRAC-MU: Illustration of Design Standards
5. Draft NWRAC-MU Zoning Ordinance







## COMPARISON OF DIMENSIONAL REGULATIONS

DIMENSIONAL REGULATIONS							
	<i>Existing Zoning Districts</i>					<i>Proposed Zoning Districts</i>	
	CB	B-2	B-3	I	RM-15	NWRAC-MUe & NWRAC-MUe	NWRAC-MUw
<b>Max Height</b>	150 ft	150 ft	150 ft	150 ft	35 ft	* 65 ft	* 45 ft
<b>Min Lot Size</b>	N/A	N/A	N/A	N/A	5,000 sq ft	N/A	
<b>Min Lot Width</b>	N/A	N/A	N/A	N/A	50 ft	N/A	
<b>Max FAR</b>	N/A	N/A	N/A	N/A	N/A	N/A	
<b>Min Front Yard</b>	**5 ft	**5 ft	**5 ft	**5 ft	25 ft	**** 0 (Primary Street) /5 ft (NW 7 <sup>th</sup> Ave & all Secondary Streets)	
<b>Min Corner Yard</b>	5 ft	5 ft	5 ft	5 ft	*** 25 ft		
<b>Min Side Yard</b>	None	None	None	None	5 ft	0 ft	
Abutting Residential	10 ft	15 ft	20 ft	30 ft	5 ft	15 ft	
<b>Min Rear Yard</b>	None	None	None	5 ft	15 ft	0 ft	
Abutting Residential	15 ft	15 ft	20 ft	30 ft	15 ft	15 ft	
<b>Max Shoulder Height</b>	N/A	N/A	N/A	N/A	N/A	65 ft	N/A
Abutting Residential	N/A	N/A	N/A	N/A	N/A	45 ft	45 ft
<b>Min Tower Stepback</b>	N/A	N/A	N/A	N/A	N/A	12 ft (Primary St) 15 ft (Secondary St)	N/A
Abutting Residential	N/A	N/A	N/A	N/A	N/A	15 ft	15 ft
<b>Max Tower Floorplate</b>	N/A	N/A	N/A	N/A	N/A	Commercial 32,000 sq ft Residential 12,000 sq ft	N/A
<p>SP Level II, subject to City Commission Approval, if development requests within the NWRAC-MUe &amp; NWRAC-MUw Zoning District exceeds sixty-five (65) feet in height, up to a maximum of one hundred ten (110) feet, or within the NWRAC-MUw zoning district that exceeds forty-five (45) feet in height, up to a maximum of sixty-five (65) feet.</p> <p>* 5-foot yard is the minimum, but in no case shall the front yard be less than 35-feet from the centerline of the abutting right-of-way</p> <p>** 25% of lot width but not less than 10 ft nor greater than 25 ft.</p> <p>*** Setback is based on street cross-sections as indicated in the NW-RAC Illustrations of Design Standards</p>							



# FORT LAUDERDALE

## NORTH WEST REGIONAL ACTIVITY CENTER

NW-RAC Illustrations of Design Standards  
THE CITY OF FORT LAUDERDALE, FLORIDA

**EXHIBIT 4 - Case#s  
T14012 ULDR Text  
Amendment and 3Z13  
Rezoning - DRAFT**



# TABLE OF CONTENTS

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<b>ACKNOWLEDGEMENTS</b>	ii
<b>INTRODUCTION</b>	iii
<b><u>NORTH WEST – REGIONAL ACTIVITY CENTER</u></b>	
<b>DEFINITIONS</b>	vi
<b>SECTION 1</b>	
<b><u>NORTH WEST – REGIONAL ACTIVITY CENTER</u></b>	
<b>CHAPTER 1: Intent</b>	1.1
<b>CHAPTER 2: Street Design Standards</b>	2.1
<b>CHAPTER 3: Street Design Examples</b>	3.1
<b>CHAPTER 4: Building Design Standards</b>	4.1
Street Classification	4.2
Summary of Dimensional Standards	4.3
Parking Facilities and Design	4.4
Framing the Street	4.7
Height	4.10
Towers and Floorplate	4.11
Architecture	4.13
Shade Structure	4.18
Active Uses	4.19
5 <sup>th</sup> Façade/Rooftop	4.22
Light & Noise	4.23
<b>SECTION 2</b>	
<b><u>RESERVED</u></b>	

# ACKNOWLEDGEMENTS

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## FORT LAUDERDALE CITY COMMISSION

John P. “Jack” Seiler	Mayor
Romney Rogers	Vice-Mayor, Commissioner – District IV
Robert L. McKinzie	Commissioner – District III
Bobby B. DuBose	Former Commissioner – District III
Dean Trantalis	Commissioner – District II
Bruce Roberts	Commissioner – District I

## CITY OF FORT LAUDERDALE

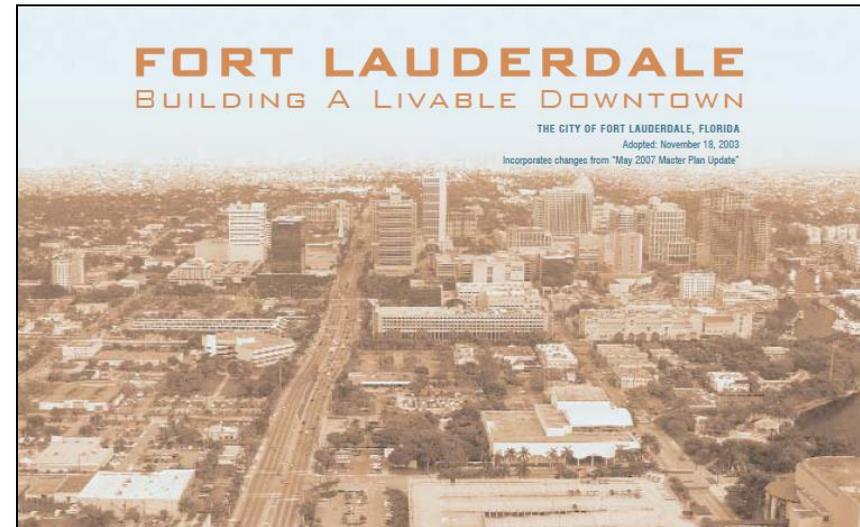
Lee Feldman	City Manager
Susanne Torriente	Assistant City Manager
D’Wayne Spence	Assistant City Attorney
Greg Brewton	Department of Sustainable Development Director
Jenni Morejon	Department of Sustainable Development Director Designee
Ella Parker, AICP	Urban Design & Planning Manager
Anthony Greg Fajardo	Zoning Administrator (Co-Project Manager)
Linda Mia Franco, AICP	Principal Planner (Project Manager)
Alfred Battle	Community Redevelopment Agency Director
Rollin Maycumber	Planner II
Thomas Lodge	Planner II

## OTHER ORGANIZATIONS AND INDIVIDUALS

Midtown Business Association  
Northwest Progresso Flagler Heights Community  
Redevelopment Agency  
CRA Advisory Board  
Various Neighborhood and Civic Associations

As with the Fort Lauderdale *Downtown Master Plan*, the *New River Master Plan* (both developed by **Beyer Blinder Belle Architects & Planners, LLP**), the *Central Beach Maser Plan* (developed by **Sasaki Associates, Inc.**) and the *SRAC-SA: Illustrations of Design Standards* (developed in house by the **City of Fort Lauderdale Department of Sustainable Development**), the format of this document has been developed to follow the established outline consistent with these existing and adopted documents. This has been done to provide a consistency and familiarity for the community as well as City Staff in the use and administration of these design standards.

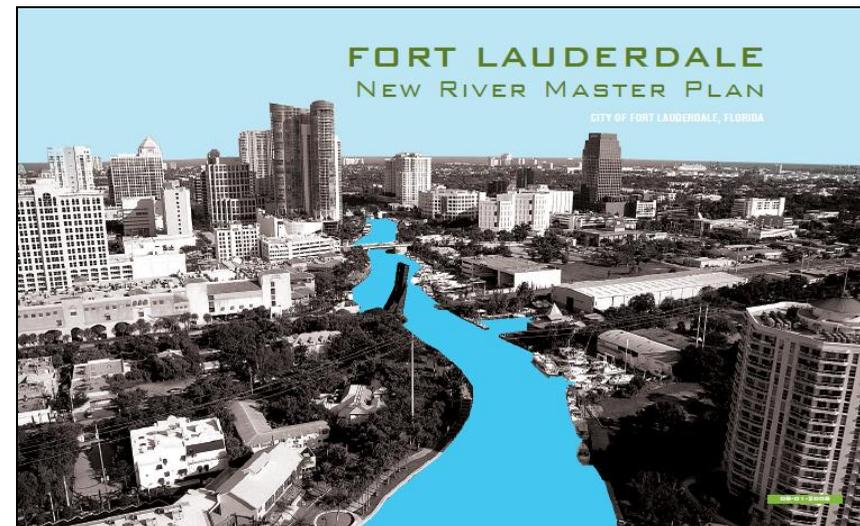
On behalf of the City of Fort Lauderdale, staff from the Department of Sustainable Development would like to thank these consultants for their hard work and for providing the foundation to further sound urban planning principles into parts of Fort Lauderdale beyond the Downtown and the Central Beach.



Downtown Master Plan, Beyer Blinder Belle, LLP



Central Beach Master Plan, Sasaki Associates, Inc.



New River Master Plan, Beyer Blinder Belle, LLP

# INTRODUCTION

## Northwest Regional Activity Center

As a means to provide the opportunity for positive redevelopment in the area north west of the City's Downtown, the Northwest Regional Activity Center (NW-RAC) Land Use District was established in 2000 to permit and encourage the existing mix of professional office, retail and residential uses within the area to remain and grow. In addition, the Northwest / Progresso / Flagler Heights Community Redevelopment Agency (NPF-CRA) was established in 1995 and amended in 2001 to outline the communities desired public and private improvements for the area through funding programs and the use of tax increment funds.

The NW-RAC is generally bounded by Sunrise Boulevard to the north, the FEC Railroad Corridor to the East, Broward Boulevard to the south, and NW 24<sup>th</sup> Avenue to the west (Figure 1.1).

In 2006, the Fort Lauderdale City Commission and the Community Redevelopment Agency (CRA) hired Wallace Roberts & Todd, LLC (WRT) to help the community explore concepts for the revitalization and improvement of the Sistrunk corridor, and make recommendations on preferred strategies and concepts for building design guidelines and site development standards called the *Sistrunk Boulevard/NE 6<sup>th</sup> Street Urban Design Improvement Plan* ("Sistrunk Plan"). The Sistrunk Plan focused on that portion of NE 6<sup>th</sup> Street between the western and eastern boundaries of the NW-RAC and those properties fronting both sides of the corridor (Figure 1.2).

The Sistrunk Plan outlined a framework and implementation plan for the corridor, aiming to transform the area from a relatively under-utilized resource to a bicycle and pedestrian-friendly urban corridor that offers a mix of uses to serve nearby neighborhoods and bring the corridor back to the vibrant and thriving business, shopping, cultural and entertainment area that it once was.

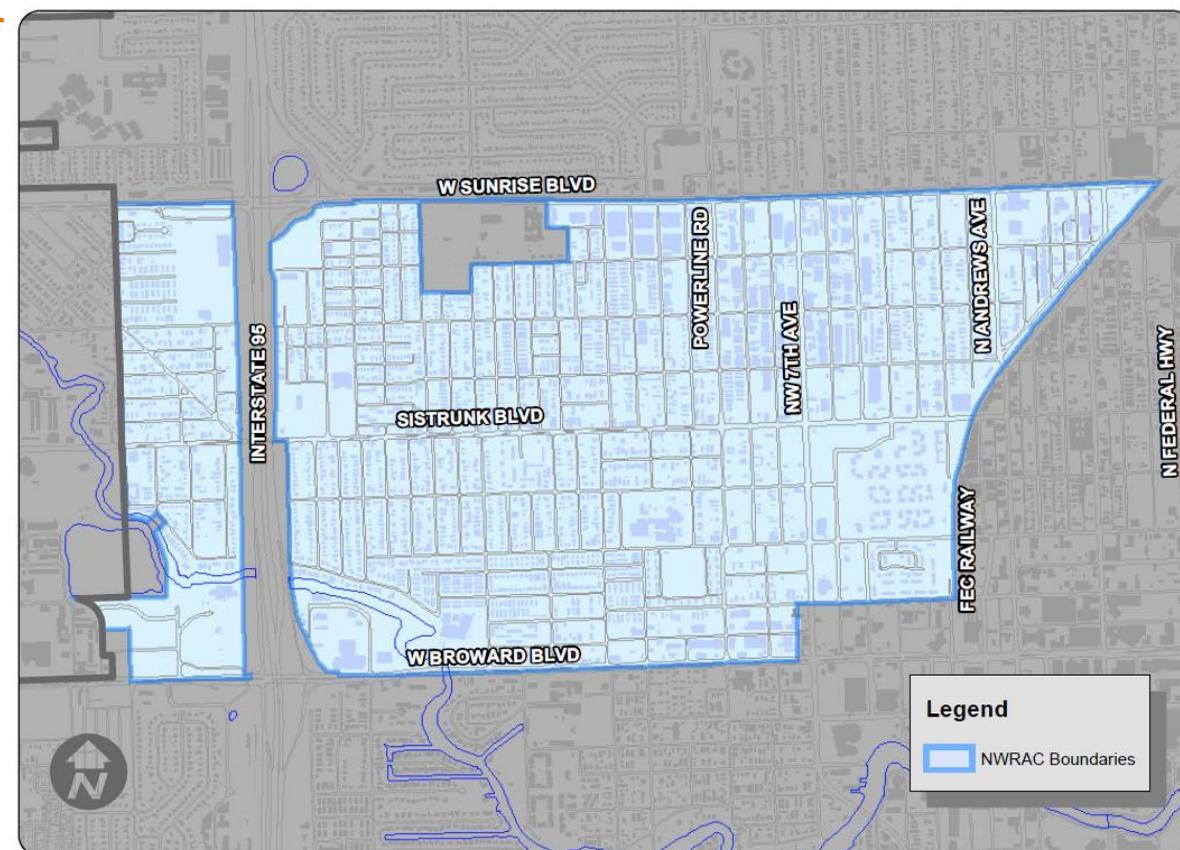


Figure 1.1, Northwest Regional Activity Center (NW-RAC) Land Use District

In 2008, the City Commission hired Urban Design Associates, Glatting Jackson and PMG Associates to develop the *Northwest / Progresso / Flagler Heights Implementation Plan* to further the goals of the Northwest/Progresso/Flagler Heights Community Redevelopment Area (NPF-CRA), which was established in November of 1995 to further outline the communities desire for public and private improvements.

As part of this implementation and through CRA funds, the City of Fort Lauderdale started and completed the NE / NW 6<sup>th</sup> Street (Sistrunk Blvd.) Streetscape / Enhancement Project, which reduced portions of the corridor from four lanes to three lanes, placed overhead utilities underground, provided on-street parking, created wider sidewalks, added decorative elements such as streetlights and trash receptacles, upgraded landscaping and street trees and provided new bus shelters. This project went a long way towards the overall goals of the Sistrunk Plan, however in order to realize the complete vision for the corridor amendments to the local zoning ordinance became necessary, resulting in the design standards contained in this document for the Northwest Regional Activity Center – Mixed Use (NWRAC-MU) zoning districts.

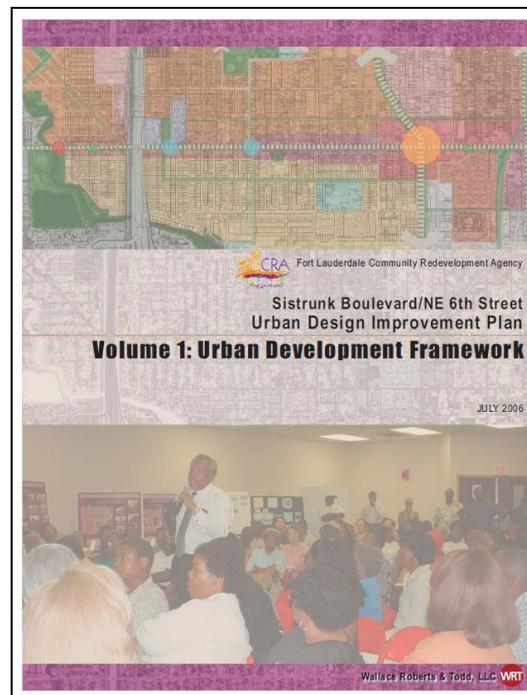


Figure 1.2, Sistrunk Boulevard/NE 6<sup>th</sup> Street Urban Design Improvement Plan, 2006, Wallace Roberts & Todd, LLC (WRT)

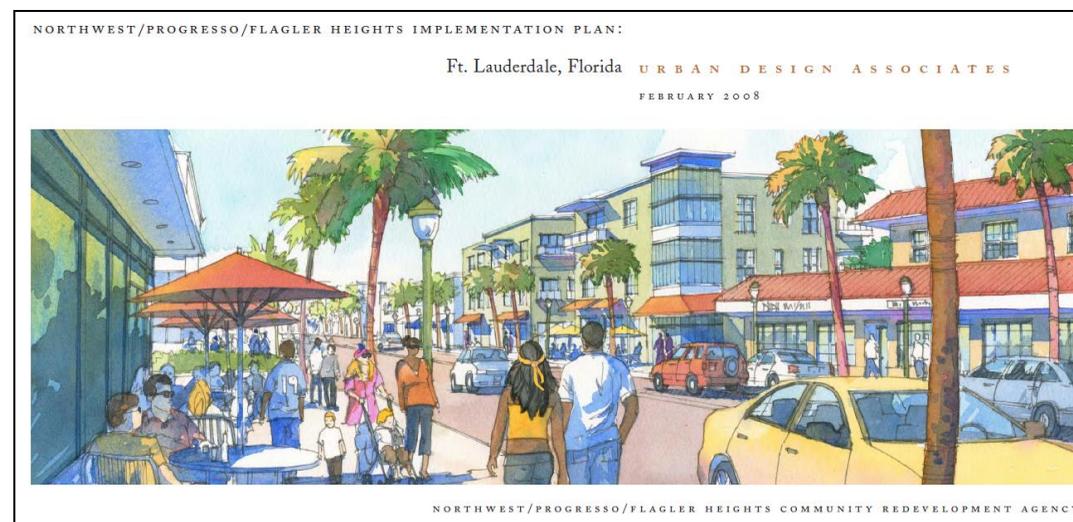


Figure 1.3, Northwest / Progresso / Flagler Heights Implementation Plan, 2008, Urban Design Associates, Glatting Jackson, PMG Associates

---

The fundamental planning principles identified in the Sistrunk Plan and summarized below may be considered applicable to the entire NW-RAC area, providing a framework for possible future NW-RAC zoning districts and regulations.

- Residential and mixed-use development to create a dynamic urban area complete with both daytime and evening activity.
- Architecture on a human scale through appropriate building form and massing that relates to the streets with minimal setbacks and active occupied spaces, especially at grade.
- Landscaping that enhances the streetscape experience and shades the pedestrian with green space consolidated into usable parks and plaza areas.
- Parking is designed in such a way that on-site movement and storage of vehicles is as imperceptible as possible and minimally, if at all, interferes with pedestrian pathways.
- Design of the streets, parking areas, and public realm that reinforces guidelines of safe neighborhood design and promotes the objectives of Crime Prevention through Environmental Design (CPTED).

It should be acknowledged that the subsequent chapters in this document predominately address those portions of the study area as identified in the Sistrunk Plan, corresponding with the new NWRAC zoning districts.

# DEFINITIONS

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<b>Floorplate:</b>	The gross square footage (GSF) for any floor of a tower. Does not include balconies that are open on three sides
<b>Pedestal:</b>	The portion of a building extending from the ground to the shoulder.
<b>Shoulder:</b>	The portion of a building below the horizontal setback between a tower and a pedestal.
<b>NWRAC-MU:</b>	The overall area comprised of both the NWRAC Land Use and NWRAC-MU zoning districts.
<b>NWRAC-MU Standards:</b>	The Illustrations of Design Standards as part of the creation of the NWRAC zoning districts adopted as part of this ordinance on _____, 2015 and incorporated as if fully set out herein.
<b>Stepback:</b>	The horizontal dimension that defines the distance between the face of the tower and the face of the pedestal.
<b>Streetscape:</b>	Exterior public space beginning at the face of a building extending into the adjacent right-of-way, which includes travel lanes for vehicles and bicycles, parking lanes for cars, and sidewalks or paths for pedestrians. Streetscape may also include, but shall not be limited to, landscaped medians and plantings, street trees, benches, and streetlights as well as fences, yards, porches, and awnings.
<b>Streetwall:</b>	The building façade adjacent to the street, along or parallel to the lot-line.
<b>Story:</b>	The complete horizontal section of a building, having one continuous or practically continuous floor.
<b>Tower:</b>	The portion of a building extending upward from the pedestal.





# SECTION 1

## Northwest Regional Activity Center

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Illustrations of Design Standards

September 2014

# INTENT

## THE ROLE OF DESIGN STANDARDS

The NWRAC zoning district (Figure 1.4) is a result of the Sistrunk Boulevard/NE 6<sup>th</sup> Street Urban Design Improvement Plan (Sistrunk Plan) accepted by the Fort Lauderdale City Commission on January \_\_, 2015.

The Sistrunk Plan envisions the area as “strategically positioned to once again become a lively, exciting community, as well as the new “gateway” for Downtown Fort Lauderdale.”

In addition, the Northwest / Progresso / Flagler Heights Implementation Plan envisions a goal “to reestablish this area as the “heart” of the Northwest as it was historically. Combining the planned reconstruction of Sistrunk Boulevard with the revitalization of NW Seventh Avenue from Sistrunk Boulevard to Broward Boulevard will not only energize the CRA area internally but will help to connect the CRA area to the jobs and amenities of Downtown Fort Lauderdale.”

The NWRAC zoning district design standards and regulations are based upon the following goals from the Sistrunk Plan:

- Buildings should be of high quality with minimal setbacks oriented to provide light and air at the street level.
- Street landscaping should reflect an urban setting, with regularly spaced trees contained in clearly defined zones.
- On-site parking should be placed in unobtrusive locations, generally behind buildings and at the interior of the block.

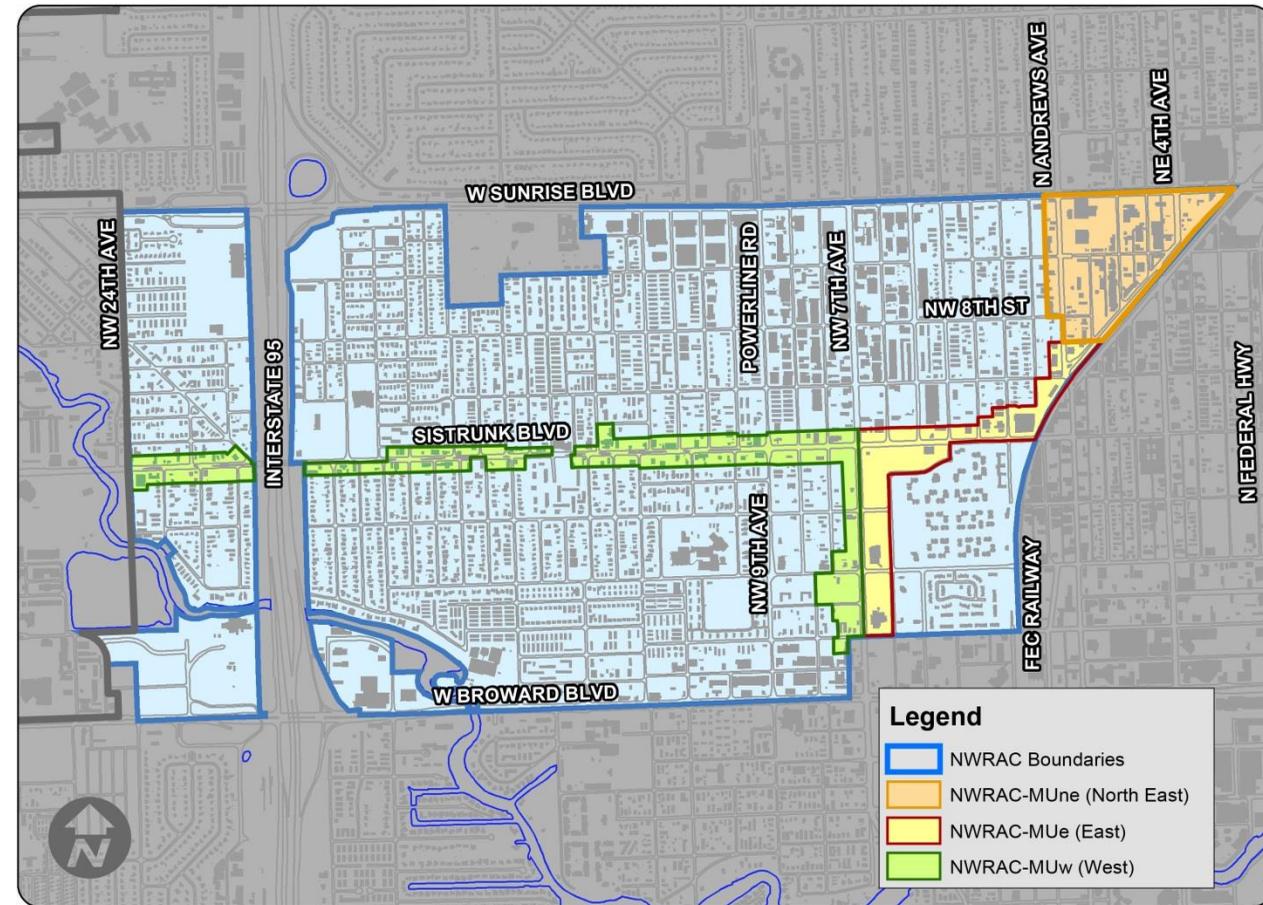


Figure 1.4, North West Regional Activity Center – Mixed Use (NWRAC-MU) Zoning Districts

- Ground floor uses should be active and interesting to pedestrians with occupied spaces.
- Plantings should be concentrated in areas where it can be of use, such as courtyards and pocket parks.
- Parking garages, where abutting a public way, should have occupied space at the ground level.

While zoning regulations are meant to be prescriptive, design standards are qualitative and reflective of a design-oriented approach that will allow flexibility to create the best possible urban environment. Specific design-based suggestions applied throughout the NWRAC-MU will help to achieve a number of both the Implementation and Improvement Plan's broader goals, especially those related to built form.

The standards included in this document are intended as a road map by which streets and buildings are designed and built in the NWRAC-MU, such that they contribute to the creation of a dynamic livable community, providing an urban fabric of walkable, tree-lined streets; distinct public spaces; high quality buildings designed and oriented to provide light, air, and active uses at the street level; all in the service of creating an exceptional urban environment.

Although following this road map will lead to a built environment that meets the intent of the NWRAC-MU zoning district, creative designs that vary from these standards, while clearly meeting their intent, will also be considered.



Figure 1.5, Northwest/Progresso/Flagler Heights Implementation Plan  
Sistrunk Boulevard at 10<sup>th</sup> Avenue (looking west)



Figure 1.6, Northwest/Progresso/Flagler Heights Implementation Plan  
Northwest 7th Avenue (looking north)

#### NOTE

References from the Implementation and Improvement Plans are listed in the margins of this document to identify relationships between the Plan visions and the NWRAC-MU design standards.

#### NOTE

Design standards are general in nature and cannot anticipate every site-specific condition. While the standards remain valid, they need to be interpreted in light of particular circumstances and conditions specific to the area under consideration.

#### NOTE

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## Plan Reference

Overall the goal should be to protect the existing efficient street grid by avoiding street closures and cul de sacs, maintaining alleys, prohibiting super blocks, and generally avoiding widening of streets. (NPFHIP 16)

## STREET DESIGN STANDARDS: NWRAC-MU

## S-1

**A fine-grained street grid is maintained, and right-of-ways are vacated only for strategic public planning purposes.**

Avoid street closings, except when absolutely necessary to improve prohibitively difficult-to-build parcels. Maintaining the finest-grained street grid is beneficial for a variety of reasons, including the maximizing of buildable street frontages and public access, and the increased distribution of traffic flows.

Avoid alley closings, except when absolutely necessary to improve prohibitively difficult-to-build parcels. Alleys are beneficial in the creation of a particular block type that is well suited for residential uses. Parking directly off of the alley can serve residential buildings that line the streets. Alleys can also provide access to entrances into parking structures and accommodate service needs.

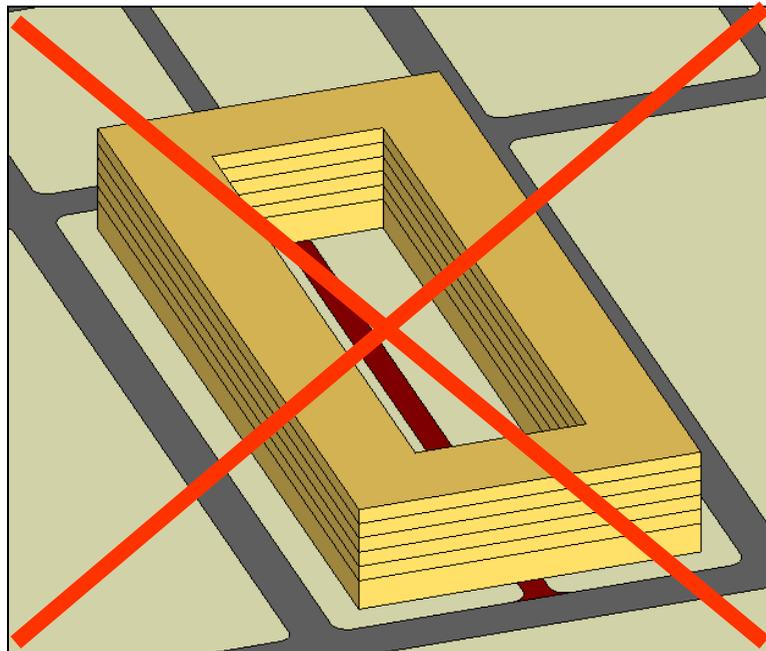


Figure 2.1

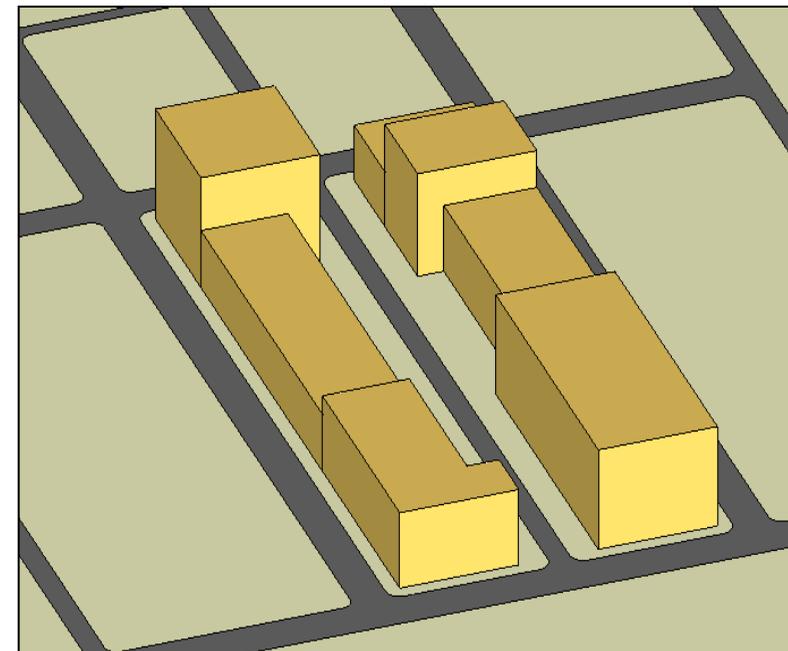


Figure 2.2

## S-2

Development above right-of-ways (air rights) does not occur.

Encourage building types appropriate to lot size and block structure. Pedestrian and vehicular bridges over alley right-of-ways may be acceptable with an integrated design.

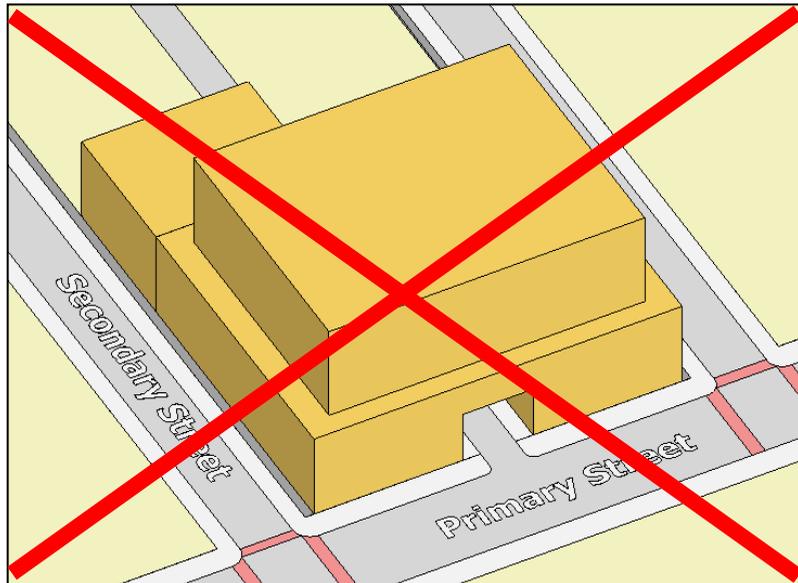


Figure 2.3

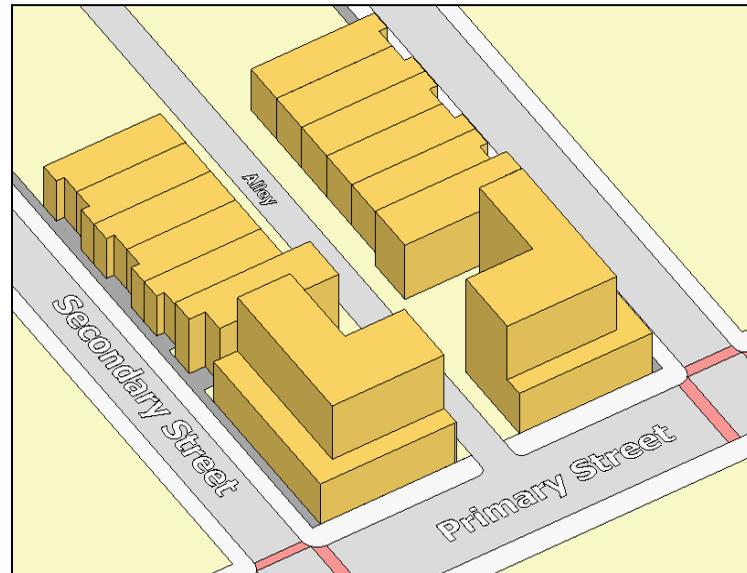


Figure 2.4

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**Plan Reference**

The study additionally confirmed that the planned traffic calming measures will both enhance the entrance to adjoining neighborhoods and sufficiently discourage traffic on local through streets. (SB/NE6 46)

**NOTE****Streets have reduced design speeds.**

Traffic speed plays an essential role in any successful pedestrian-oriented environment. Since people tend to drive at speeds that feel safe on a given road, the actual design of the road plays just as important a role as the posted speed limits in determining the speed of traffic flow. There are very few examples of successful pedestrian streets that accommodate high-speed traffic flow.

**STREET DESIGN STANDARDS: NWRAC-MU****S-3****Streets have reduced lane widths.**

Urban street standards, attempting to balance the needs of cars, people, bicycles, and transit, require narrower travel lanes and “tighter” dimensional standards than typical ‘suburban’ standards for several reasons: the need to fit multi-modal travel lanes within existing rights-of-way; the need to discourage excessive high-speed automobile flow in areas where pedestrians and bicycles share the street; the need to decrease the pedestrian crossing distance; and, the opportunity to provide wider sidewalks within the public right-of-way.



Figure 2.5

**S-4****Traffic calming is utilized rather than barricading streets.**

Encourage the re-opening of existing street closures; discourage such closures in the future. Instead of street closures, a variety of other ‘traffic calming’ devices should be utilized to inhibit through-traffic on local streets.

A technique well suited for local neighborhood streets is the ‘mini-roundabout’. The roundabout slows traffic and adds a distinct urban identity with landscape elements at intersections. Another traffic calming technique is the ‘speed table’, which is an elevated portion of the roadway that encourages cars to slow down and creates a more seamless pedestrian crossing.

On-street parking, practical for a number of reasons, also serves as an effective traffic-calming device.

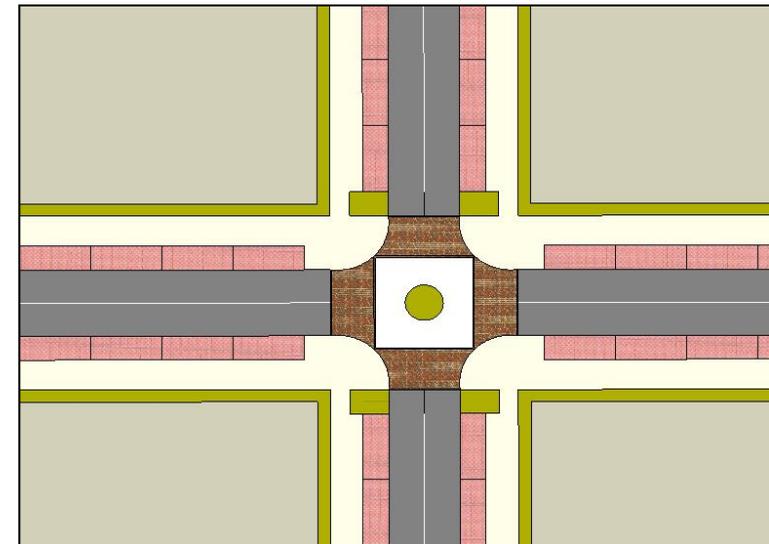


Figure 2.6

## S-5

**On-street parking is maximized on all streets.**

Abundant parallel parking throughout the NWRAC-MU zoning districts is important for several reasons: it helps to satisfy the ever-growing need for more parking spaces without incurring the higher costs of structured parking; it contributes to pedestrian-friendly design by providing a buffer between pedestrians and fast-moving traffic; it contributes to an active street-life by depositing passengers/future pedestrians at various points along the streets who then walk to nearby destinations. It can also provide a significant revenue source for the city that could contribute to the costs of an improved public realm.

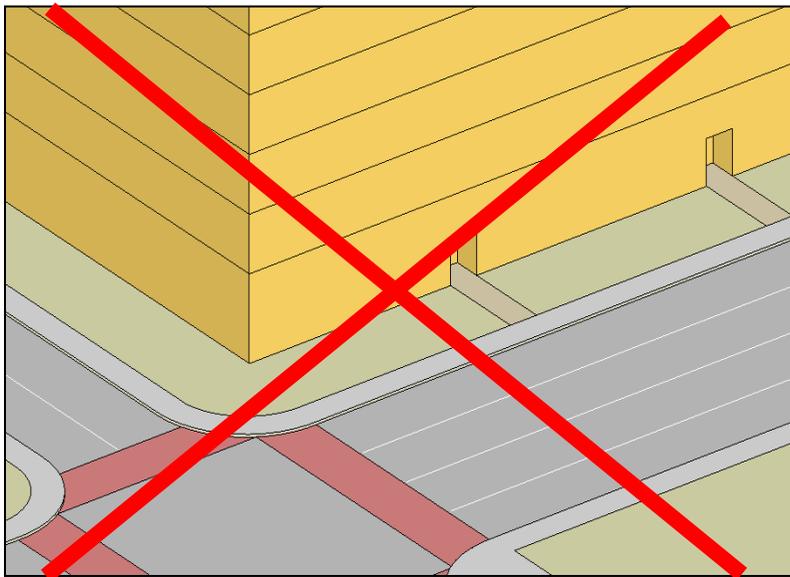


Figure 2.7

## S-6

**Adequate bike lanes are provided where appropriate, subject to a planned bicycle network.**

A well-connected system of bike lanes is critical to making an area bicycle-friendly. Bike lanes need to be properly sized and located to truly create a safe, desirable biking environment, which can also reduce car traffic.

The provision of a bike lane is dependent upon the ROW width. Where suitable, the preferred bike lane width is as follows:

Alongside a travel lane with on-street parking: a = 5 feet

Alongside a travel lane without on-street parking: a = 4 feet

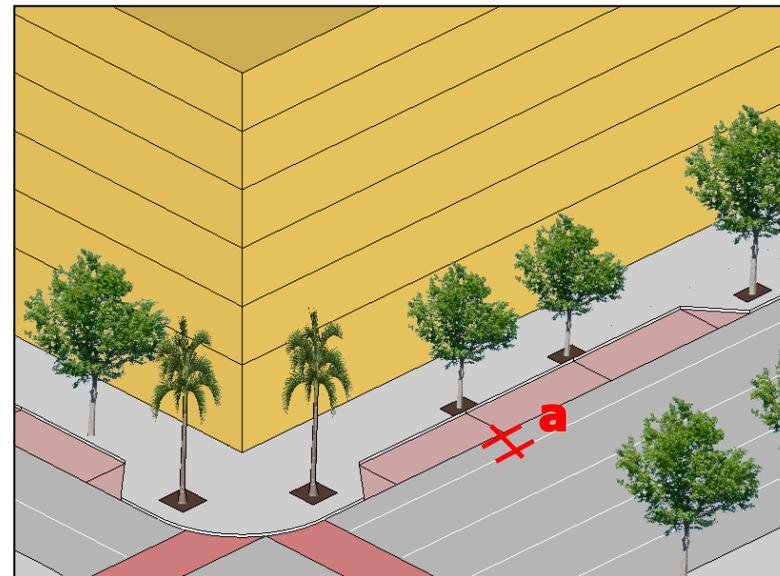


Figure 2.8

**CODE ISSUE**

Coordinate with Broward County Bikeways Program.

**Plan Reference**

The reconstruction of Sistrunk Boulevard and its conversion from a four lane street with limited on-street parking to a three lane street (two east bound and one west bound) with dedicated street parking will calm traffic and also restore Sistrunk Boulevard (historic "Sixth Street") to the local neighborhood shopping street that it once was (NPFHIP 30)

**NOTE**

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## STREET DESIGN STANDARDS: NWRAC-MU

### S-7

Curb radii are reduced at street intersections to a preferred maximum of 15 feet, or a preferred maximum of 20 feet at major arterial roadways.

Decreasing the curb radius standard in urban areas accomplishes two important things: it decreases the crossing distance for pedestrians and provides traffic calming by compelling motorists to slow down when turning, providing a safer crossing for pedestrians.



Figure 2.9

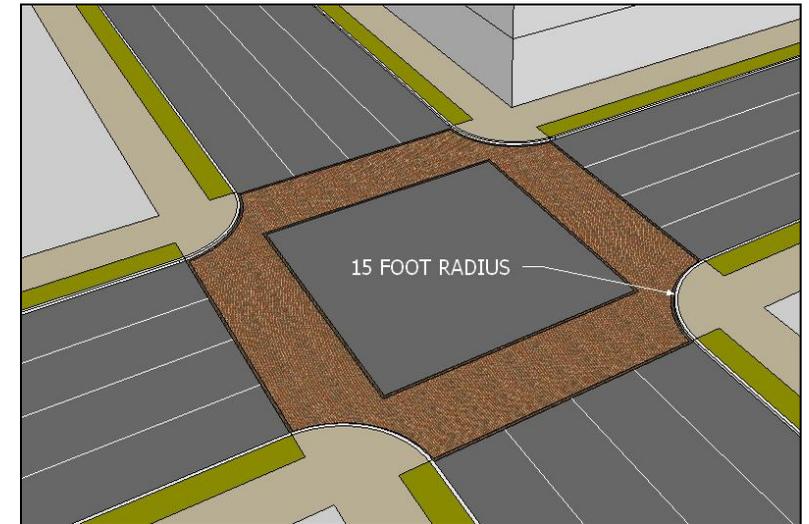


Figure 2.10

S-8

County "Corner Chord" requirements are eliminated to the greatest extent possible.

The triangular easement required by current County corner chord regulations creates excessive building setbacks at affected street corners. While this type of design is generally intended for suburban conditions and is incompatible with the NWRAC-MU zoning districts (where the option for corners built-out to the property lines is highly desirable) an integrated design that enhances the pedestrian experience with active uses may be appropriate at certain locations where available sidewalk space is at a premium.

The necessary utility infrastructure can be located underground, within an adjacent building (with external access), or at the base or top of signal posts. These methods are common in many cities.



Figure 2.11

S-9

All utility lines (electrical, telephone, cable, etc.) are buried in locations allowing for tree planting and proper root growth.

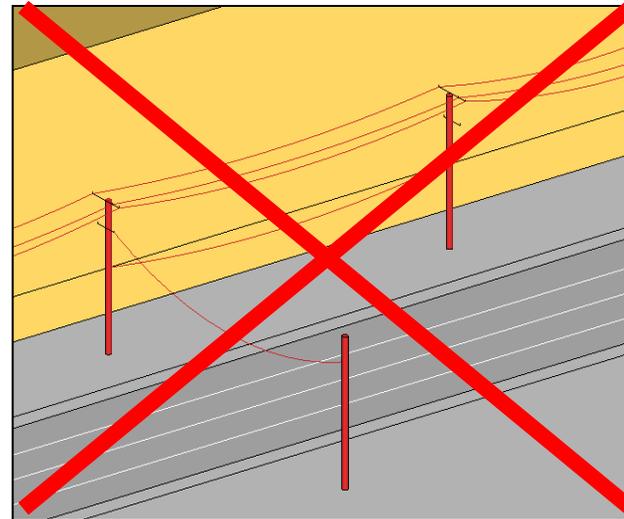


Figure 2.12



Figure 2.13

NOTE

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Plan Reference

Street trees will be planted along the entire length of the corridor, providing welcome shade and relief from the sun, and creating visual cohesiveness, connectivity, and sense of place. (SB/NE6 46)

STREET DESIGN STANDARDS: NWRAC-MU

S-10

Shade trees are maximized on all right-of-ways, located between the sidewalk and the street, with palms or ornamental trees providing a visual marker for intersections.

Street trees that are located between the sidewalk and automobile traffic provide a physical and psychological buffer that encourages a feeling of pedestrian safety. Framing the sidewalk (with buildings on one side, trees on the other) can provide consistent shade for pedestrians. Shade trees are preferable to palms where pedestrian comfort is desired. Trees also reduce the visual width of the street and frame the roadway. Both shade and palm trees can effectively achieve this effect.

Note: Palm and ornamental trees along streets are also acceptable in some areas, such as major traffic arterials where a strong “framing” from the perspective of the automobile is desired, or when existing or proposed physical conditions may prevent the proper growth of shade trees, as determined by the Development Review Committee (DRC). Palms and ornamentals may also be added to complement shade trees in a variety of configurations.

Trees located directly adjacent to buildings are prohibited; they provide little shade, have limited size and growth potential, and are mostly limited to palms.



Figure 2.14



Figure 2.15

Important factors in tree selection include: desired shade canopy, sidewalk width, underground utility lines, maintenance, and, most importantly, the creation of a unified street image. All trees shall satisfy the following standards at the time of planting.

### Tree Planting Dimensions

#### **Shade Trees:**

- min. 20 ft in height
- min. 8 ft spread
- min. 6 ft ground clearance
- max. 30 lineal ft spacing
- \*min. 15 ft canopy clearance (face of building to face of trunk)

#### **Palm Trees:**

- min. 18 ft in height
- min. 8 ft of wood
- max. 20 lineal ft spacing
- min. 6 ft canopy clearance (face of building to face of trunk)

#### **Ornamental Trees:**

- min. 12 ft in height
- min. 6 ft spread
- min. 6 ft ground clearance
- max. 20 lineal ft spacing
- min. 6 ft canopy clearance (face of building to face of trunk)

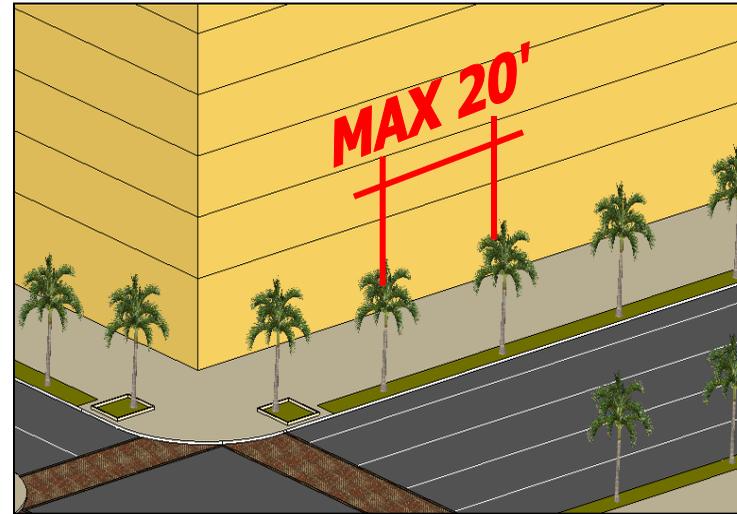


Figure 2.16

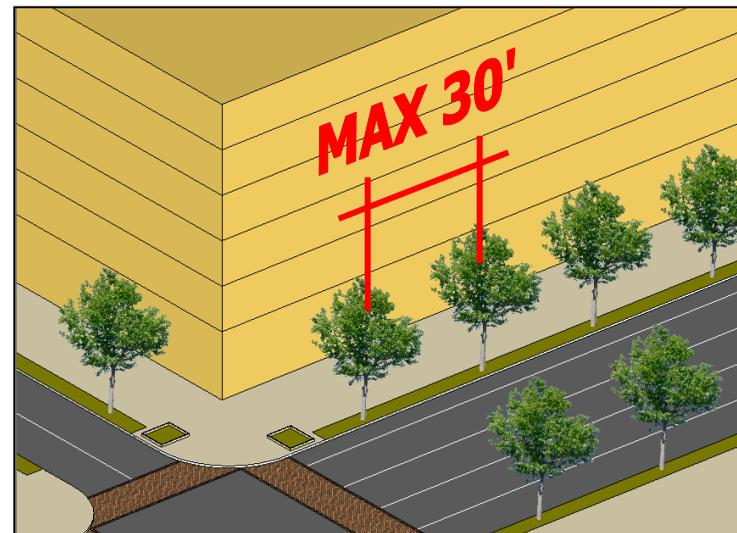


Figure 2.17

#### **NOTE**

\* The minimum canopy clearance for shade trees may be reduced based upon tree species and available planting width as approved by the Department.

#### **NOTE**

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## STREET DESIGN STANDARDS: NWRAC-MU

### S-11

**Landscaping (other than street trees) plays a supporting role rather than dominant role in the overall street design.**

Other elements should be used to enhance the street environment and should be part of a consistent and coordinated system including lighting poles, benches, waste receptacles, bicycle racks and other elements.



Figure 2.18



Figure 2.19



Figure 2.20

## S-12

**Numerous and wide curb cuts are avoided to the greatest extent possible.**

While curb cuts may be unavoidable, they are generally discouraged on primary streets. Where possible, curb cuts leading to drop-offs, parking garages and drive-through services should be located off of service alleys or secondary streets (streets which are removed from the significant pedestrian-oriented activity).

Multiple access points serving the same development should also be consolidated into the fewest number of curb cuts as possible, and the width and number of lanes of curb cuts should be minimized.

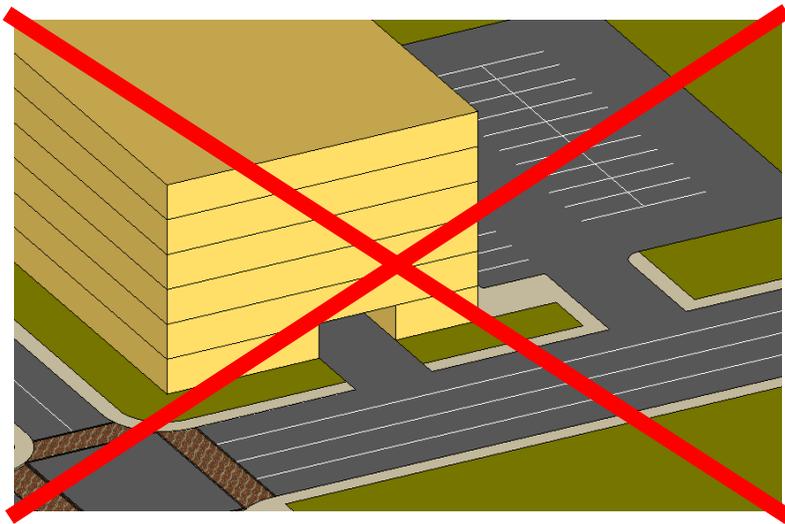


Figure 2.21

## S-13

**Drive-thrus are avoided in most cases.**

Discourage drive-thru configurations that detract from streets' spatial definition, are visible from public rights-of-way, or that add curb cuts to primary or secondary streets.

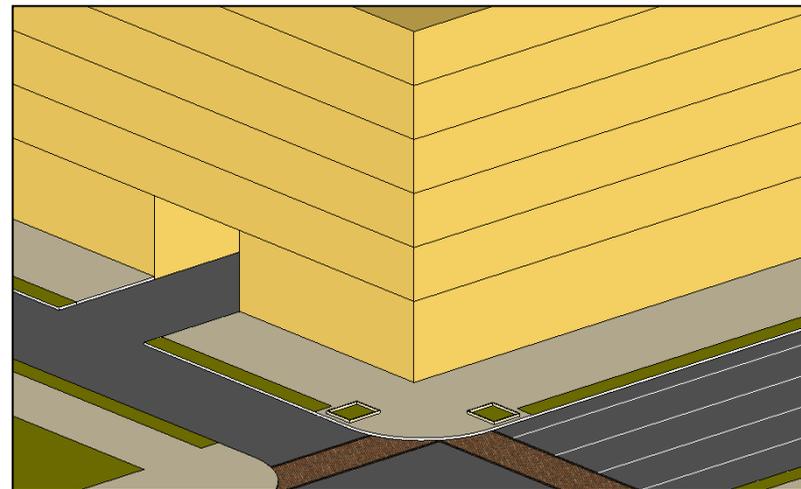


Figure 2.22

## NOTE

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## STREET DESIGN EXAMPLES: NWRAC-MU

Important concepts regarding street design are referenced in the South Andrews Avenue Master Plan and Development Guide and used as the basis for the NWRAC-MU Street Design Examples.

Improvements to the existing streets will be an important factor in channeling vehicular traffic, enhancing the pedestrian experience, and providing additional convenient, curbside parking. A successful streetscape program is also helpful in establishing a distinctive image and identity for an area.

The common goal for all of the streets in the study area is that they become more pedestrian friendly. The addition of on-street parking in as many locations as possible adds to the potential supply of pedestrians as well as serving as a protective buffer between the sidewalk and the moving lanes. Other improvements such as a consistent treatment of landscaping, paving materials, lighting, street furniture, and public art will help to create a coherent visual environment and a distinctive character for the South Andrews area.



Figure 3.1, Northwest/Progresso/Flagler Heights Implementation Plan  
Sistrunk Boulevard at 10<sup>th</sup> Avenue (looking west)

The street design examples contained herein illustrate design standards to achieve the goals of the Master Plan and do not represent fully engineered solutions. Other alternatives are acceptable as long as they satisfy the fundamental design standards as indicated in this document.

The City has the flexibility to work with the NWRAC-MU street design recommendations to make them compatible with changing or unforeseen conditions and ongoing studies.

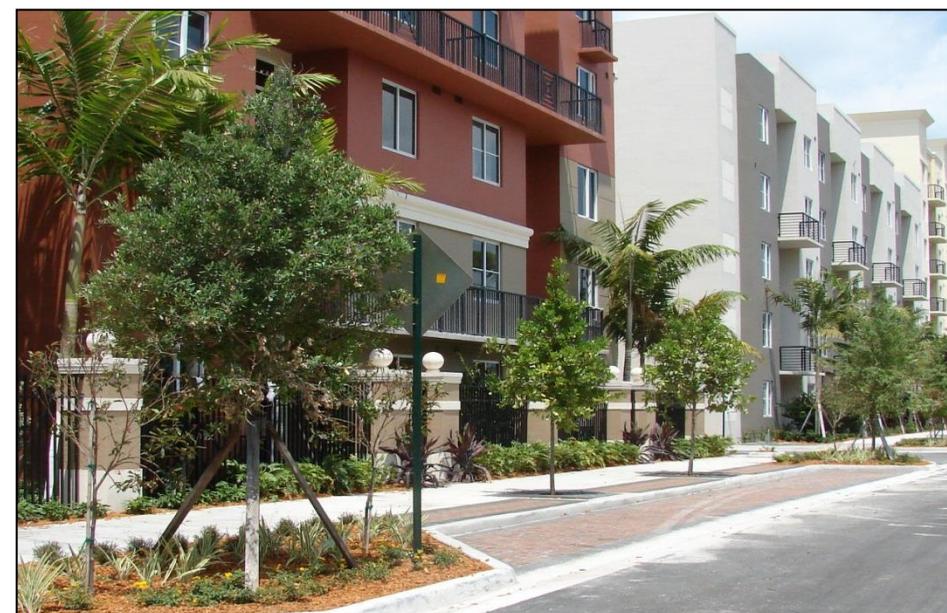


Figure 3.2, Example of street design with large shade trees in bulb outs, and small shade trees / ornamental trees in sidewalk

# EXISTING STREET DESIGN CONDITIONS



Figure 3.3, Sistrunk Boulevard/NW 6th Street Prior to Corridor Improvements



Figure 3.4, Sistrunk/NW 6th Street Prior to Corridor Improvements



Figure 3.5, Sistrunk Boulevard/NW 6th Street after Corridor Improvements



Figure 3.6, Sistrunk Boulevard/NW 6th Street after Corridor Improvements

**NOTE**

It should be noted that any secondary streets included within the study area boundaries are required to be improved as part of development/redevelopment applications. Where these streets transition further into the areas outside of the study area a suitable design to accommodate this transition shall be coordinated with Engineering Division and the Department of Transportation and Mobility.

**STREET DESIGN EXAMPLES: NWRAC-MU****General ROW Design**

Right-of-ways within the NWRAC-MU zoning districts vary in width, number of travel lanes and overall design.

For those areas of Sistrunk Boulevard that have not been improved as part of the Sistrunk Boulevard/NE 6<sup>th</sup> Street Improvement Plans shall be designed consistently with the existing and established improvements as design and intended by that plan.

All street design examples provided in this document include a cross sections representing the preferred design solution, however it must be noted that due to unforeseen or unanticipated design obstacles, such as, but not limited to utilities and easements certain modifications to these standard designs may be necessary. However, any design alteration must meet the intent and spirit of the overall design solution presented within this document.

**Sistrunk Boulevard:**

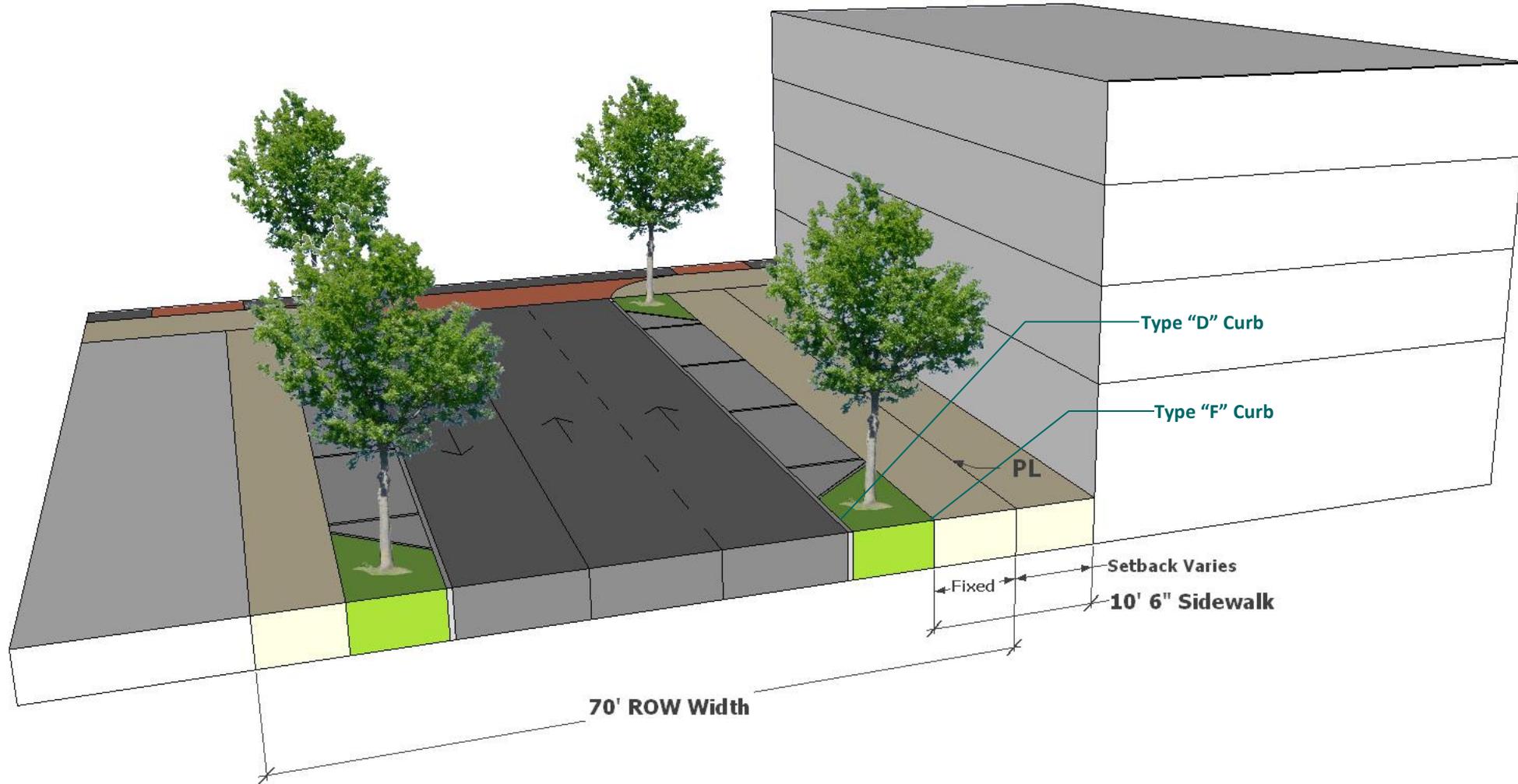
- All improvements associated with the Sistrunk Boulevard/NE 6<sup>th</sup> Street Improvement Plan that have been implemented at the time of development shall remain as they currently exist subject to providing a minimum sidewalk width of ten (10) feet six (6) inches. Additional width as needed to provide for the minimum sidewalk width shall be dedicated to the pedestrian realm.
- All areas of Sistrunk Boulevard that have not been improved as part of the Sistrunk Boulevard/NE 6<sup>th</sup> Street Improvement Plan at the time of development shall be development with improvements consistent with the existing street cross-sections for those areas of the corridor that have been established as part of that plan.

**All Other ROWs:**

- All other right-of-ways shall be improved at the time of development subject to the street cross-section examples contained in this document and as listed as primary or secondary streets.
- Minimum on-street parking width shall be eight (8) feet
- The remaining portion of the right-of-way, from the curb of the parking space to the dimension necessary to provide the minimum sidewalk width shall be dedicated to the pedestrian realm and shall include shade trees as indicated.

# STREET DESIGN EXAMPLES: NWRAC-MU

## Sistrunk Boulevard



### NOTE on Street Design

- A consistent sidewalk width of ten (10) feet six (6) inches shall be maintained for the entire length of the Sistrunk Boulevard corridor. Any additional width necessary to provide this minimum dimension shall be dedicated to the pedestrian realm.
- Those areas of Sistrunk Boulevard that have not been improved as part of the Sistrunk Boulevard/NE 6<sup>th</sup> Street Improvement Plan shall be designed and improved at the time of development in accordance with that plan.

### NOTE

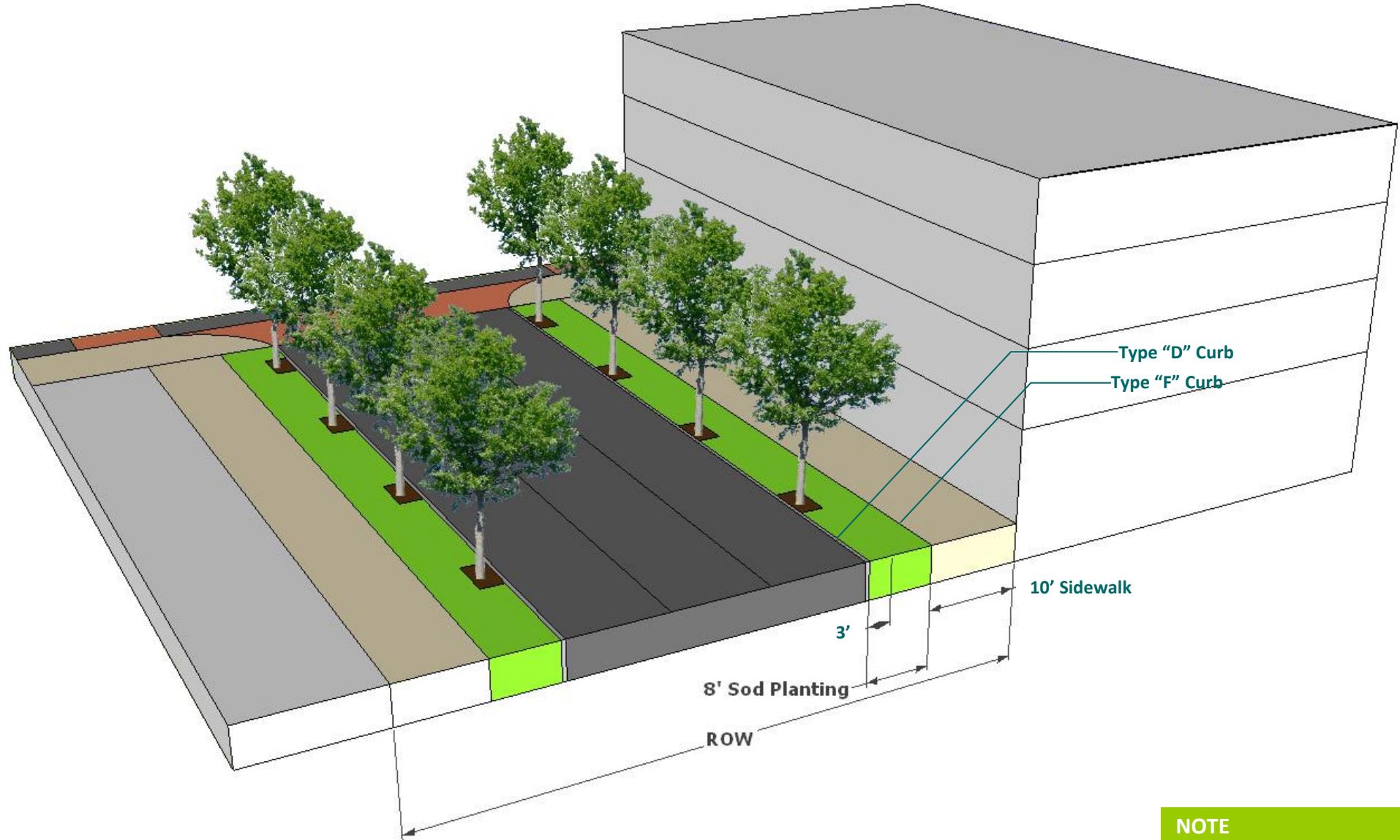
Sub-grade under sidewalk with trees to be constructed with approved structural soil system.

**NOTE on Street Design**

A consistent sidewalk width of ten (10) feet six (6) inches shall be maintained for the entire length of the Sistrunk Boulevard corridor. Any additional width necessary to provide this minimum dimension shall be dedicated to the pedestrian realm.

**STREET DESIGN EXAMPLES: NWRAC-MU**

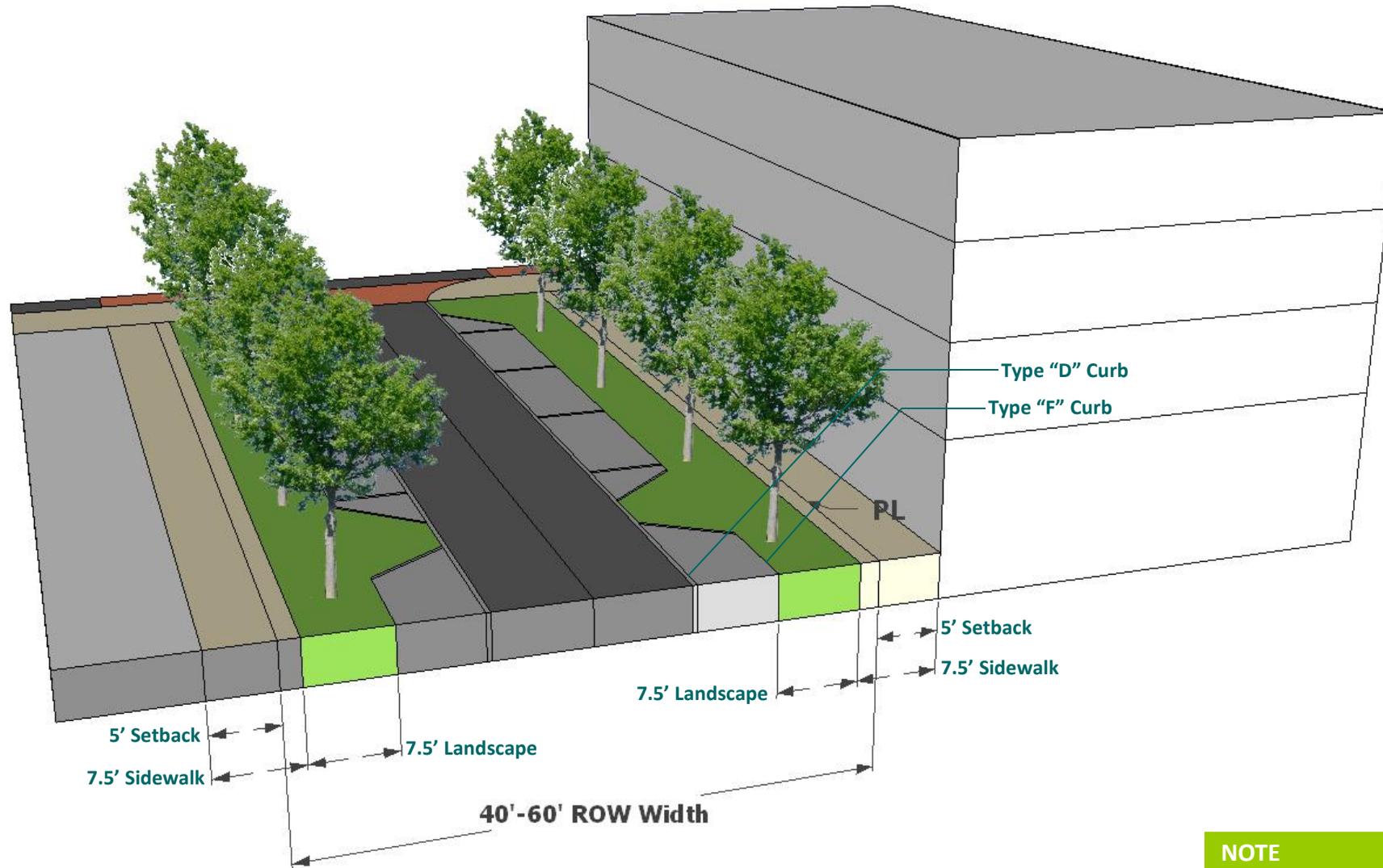
**NW 7<sup>th</sup> Avenue**



**NOTE**  
Sub-grade under sidewalk with trees to be constructed with approved structural soil system.  
Exhibit 2

# STREET DESIGN EXAMPLES: NWRAC-MU

## Secondary Streets



**NOTE on Street Design**

- All existing medians shall be preserved as they currently exist
- Maximum travel lane width shall be ten (10) feet
- Minimum on-street parking width shall be eight (8) feet
- The remaining portion of the right-of-way, from the curb of the parking space to the property line, plus the minimum five (5) ft building setback, shall be dedicated to the pedestrian realm, as outlined below:
  - Large shade trees shall be located in a bulb out, after every two parking spaces
  - Small shade trees or ornamental trees shall be located in a tree grate within the sidewalk, the trunk being a minimum of six (6) ft from the face of the building, and spaced at the intersection of every parking space

**NOTE**  
Sub-grade under sidewalk with trees to be constructed with approved structural soil system.

# BUILDING DESIGN STANDARDS: NWRAC-MU

## Street Classification

Streets in the NWRAC-MU zoning districts are classified according to various functional characteristics such as width, traffic volume, and suitability for human-scale, pedestrian-friendly street life. All streets are classified as primary or secondary.

The primary focus of street classification in the NWRAC-MU zoning districts is to promote development that reinforces the character of various streets according to the role they play in the urban fabric.

The form of development that occurs on any given street is in part determined by the street classification. The regulations for development arising from street classifications shall encourage the development of both sides of the street in a consistent manner.

The NWRAC-MU zoning districts establish development provisions intended to reinforce the qualities described for primary and secondary streets. For each street type, the right-of-way width and particular street section may vary depending on available space and other existing constraints.

**Primary Streets**

- |                          |                                 |
|--------------------------|---------------------------------|
| <b>Sistrunk Blvd</b>     | <b>NW 7<sup>th</sup> Avenue</b> |
| <b>Sunrise Boulevard</b> | <b>Broward Boulevard</b>        |
| <b>Andrews Avenue</b>    | <b>Progresso Drive</b>          |

Primary streets are characterized by active commercial and retail frontage at the ground floor, taller and more intensive buildings fronting the street, and a consistent streetwall. Primary Streets typically feature a full complement of pedestrian amenities, including wide sidewalks, on street parking, and a well-developed streetscape, which may include open space for public use. Primary Streets are the principal urban streets and are intended to be well used by vehicles and pedestrians as primary transit routes.

While design solutions have been developed for the corridors of Sistrunk Boulevard and NE 7<sup>th</sup> Avenue, street cross-sections have not been developed for Sunrise Boulevard as only a small section of this corridor is effected by this document. As development/redevelopment occurs street design solutions will be reviewed on a case-by-case basis in conjunction with the applicant to determine best possible design solution and to ensure the entire length of Sunrise Boulevard is considered. Further, Broward Boulevard is not included within the study boundaries. Should these corridors be included within the study boundary at a future date design solutions shall be developed to establish minimum requirements for development along these streets.

**Secondary Streets**

**All streets other than the primary streets listed in herein**

Secondary streets tend to be more residential in nature, and have smaller scale non-residential uses transitioning between the Primary Streets and the existing residential and commercial neighborhoods outside of the NWRAC-MU zoning districts. Secondary streets offer a combination of a mix of uses, but at less intensity and with less vehicular traffic while maintaining a pedestrian friendly environment.

Plan Reference

Overall the goal should be to protect the existing efficient street grid by avoiding street closures and cul de sacs, maintaining alleys, prohibiting super blocks, and generally avoiding widening of streets. (NPLFHIP 16)

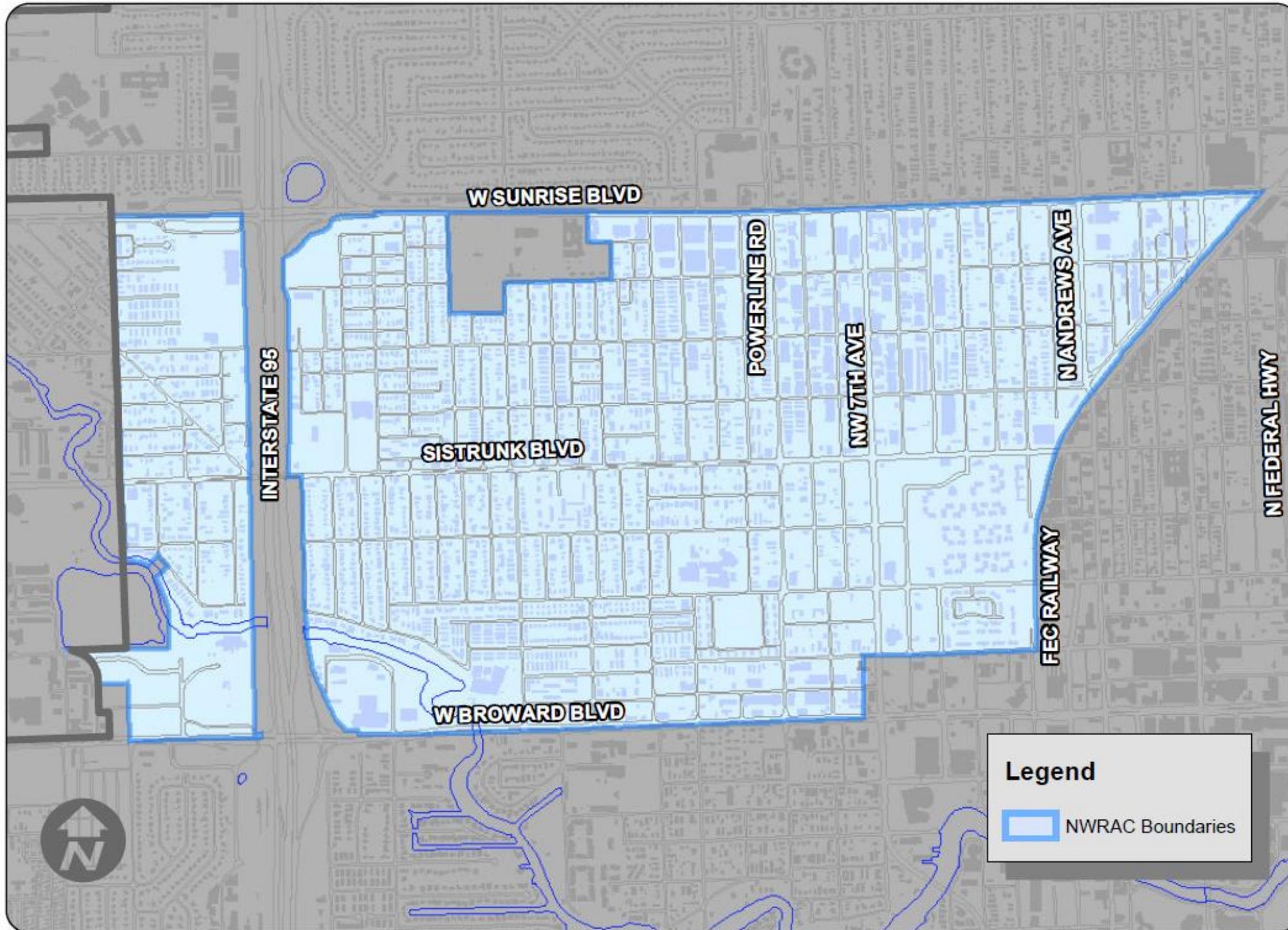


Figure 4.1

**Plan Reference**

The Sistrunk Boulevard and NW Seventh Avenue Initiative is the preferred location for new multi-family housing, retail and mixed use development. The goal is to reestablish this area as the “heart” of the Northwest as it was historically. Combining the planned reconstruction of Sistrunk Boulevard with the revitalization of NW Seventh Avenue from Sistrunk Boulevard to Broward Boulevard will not only energize the CRA area internally but will help to connect the CRA area to the jobs and amenities of Downtown Fort Lauderdale (NPLFHIP 29)

**NOTE**

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**BUILDING DESIGN STANDARDS: NWRAC-MU**

**Summary of Dimensional Standards**

**NWRAC-MUe and NWRAC-MUe**

**Permitted Height** up to 5 stories, but no higher than 65 ft  
**Max Height** up to 10 stories, but no higher than 110 ft\*

**NWRAC-MUw**

**Permitted Height** up to 3 stories, but no higher than 45 ft  
**Max Height** up to 5 stories, but no higher than 65 ft\*

**Build-to Line**

	<b>Front</b>	<b>Corner</b>	
Primary Street:	0 ft	0 ft	(5 ft front build-to line on 7 <sup>th</sup> Street)
Secondary Street:	5 ft	5 ft	

**Setbacks**

	<b>Side</b>	<b>Rear</b>
Primary Street:	0 ft*	0 ft*
Secondary Street:	0 ft*	0 ft*

\*side/rear yard setback = 15 ft when abutting existing residential

\*Structures exceeding the permitted height threshold of the NWRAC-MUe, NWRAC-MUe, and NWRAC-MUw shall be reviewed subject to the process for a Site Plan Level II permit, with City Commission review and approval.

**Shoulder Height**

	<b>Minimum</b>	<b>Maximum</b>
*For buildings with towers	2 stories or 25 ft	5 stories or 65 ft

(development abutting residential shall provide a maximum 4 stories or 45 ft for shoulder height)

**Min. Tower Stepback**

	<b>Front</b>	<b>Corner</b>	<b>Side</b>	<b>Rear</b>
Primary Street:	12 ft	12 ft	[Dependent on floorplate]	
Secondary Street:	15 ft	15 ft	[Dependent on floorplate]	

**Max. Floorplate / Min. Tower Separation**

<b>Commercial</b>		<b>Residential</b>	
32,000 s.f.	30 ft side and rear stepback	12,000 s.f.	30 ft side and rear stepback
20,000 s.f.	25 ft side and rear stepback	10,000 s.f.	25 ft side and rear stepback
16,000 s.f.	20 ft side and rear stepback	8,000 s.f.	20 ft side and rear stepback

## B-1

**Surface parking facilities are secondary to the pedestrian public realm experience with vehicular access provided from the secondary street or alley where possible.**

In general, surface parking along street frontages should be avoided. Parking lots create 'dead' spaces along pedestrian-oriented streets, where street life and street-space definition are lost. However, when unavoidable, surface lots should be located to the rear of the principal building with access and frontage of parking lots limited to Secondary Streets or alleys when feasible.

Surface parking areas should be fully screened from the street. This may be accomplished through the use of decorative walls or fencing in addition to any landscaping or any combination thereof subject to CPTED performance standards.



Figure 4.2

Surface parking lots located on a development site abutting the intersection of Andrews Avenue and any other Primary Street are discouraged from locating the vehicular entranceway on Andrews Avenue.

Along secondary street frontages a minimum of a 10-foot landscape buffer shall be required exclusive of sidewalk regulations.

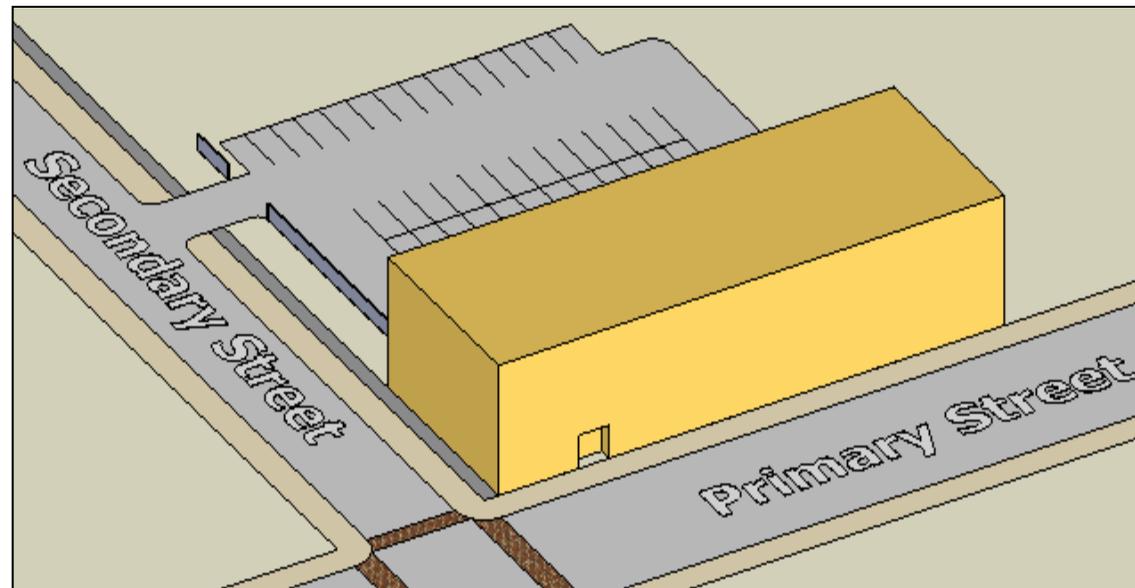


Figure 4.3

## NOTE

Parking regulations in the NWRAC-MU zoning districts are reduced from the general regulations as provided in ULDR Section 47-20, Parking and Loading regulations.

## Plan Reference

As along Sistrunk Boulevard, parking lots and structures will be behind the buildings and not visible from Seventh (NPFHIP 31)

Locate parking and service areas (and access) to the side or the rear of the property (SB/NE6 45)

## BUILDING DESIGN STANDARDS: NWRAC-MU

### B-2

**Structured parking design is well integrated into the overall building design.**

Access from Secondary Streets and alleys is encouraged.

Parking garages are encouraged to minimize visual exposure of parking by locating active space on the ground floor along the street.

Where structured parking must be exposed to the street, exceptionally creative solutions should be explored:

- Dramatic and/or elegant building form with a compelling street presence
- Consistent and integrated architectural details
- High quality, durable exterior materials
- Richer materials palette, more intensive details and lighting encouraged for the street level
- Landscaping, plazas, or active uses are encouraged to conceal or enhance rooftop parking areas.

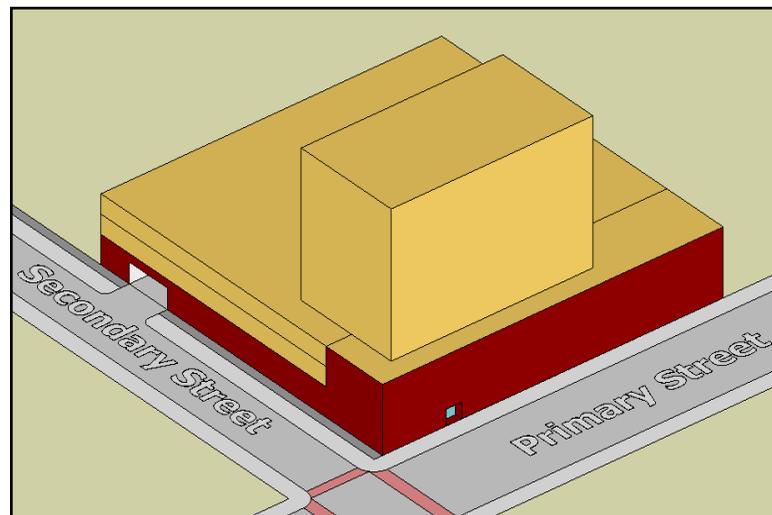


Figure 4.4



Figure 4.5

## B-3

**To create an interesting, active, street environment, main pedestrian entrances are oriented toward the street.**

When a building is located at the intersection of a Primary and Secondary Street, the main pedestrian entrance into the building should be located toward the Primary Street.

With the exception of certain types of residential development, the main pedestrian entrance along a Secondary Street is encouraged to be located along the street frontage.

Entrances along the street encourage pedestrian activity, accommodating building-users arriving by foot, from on-street parking, and from transit. In general, the more pedestrian entrances along a street, the more active and interesting the street becomes. If interior-block parking exists, there may also be secondary entrances from the parking area, or mid-block pedestrian passages from parking areas to the street.

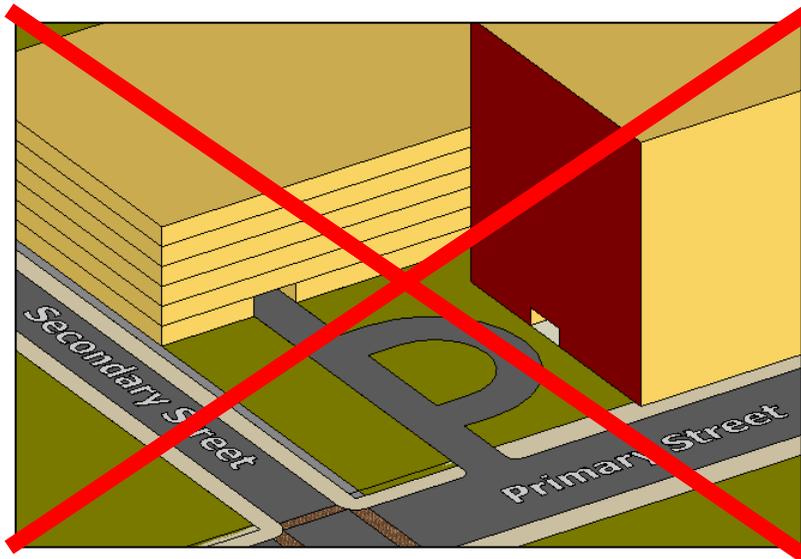


Figure 4.6

Buildings set back from the street behind surface parking lots are discouraged since they draw pedestrian life away from the streets and create unpleasant approaches to their entrances for people arriving at the building on foot.

Building entrances set back behind large 'motor court' drop-offs can also compromise the continuity of pedestrian street-life. Modest drop-off areas, without curb-cuts, are easily accommodated along streets (often through the removal of on-street parking at the building entrance location), or within an adjacent ground floor parking structure.

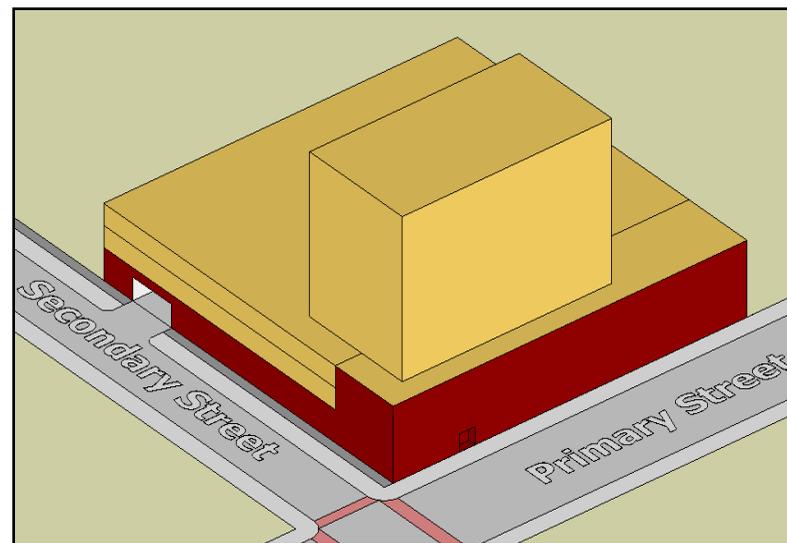


Figure 4.7

## Plan Reference

"buildings should locate close to the front property line (through a build-to line or maximum front setback, still allowing for a wide sidewalk) and parking should be located to the rear or side of the buildings." (SB/NE6 48)

## NOTE

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### Plan Reference

The integration of additional open space into the design of the proposed development is encouraged, provided that the visual character of the development remains compatible with that of the surrounding area, and consistent with the overall character of the corridor. (SB/NE6 2-24)

## BUILDING DESIGN STANDARDS: NWRAC-MU

### B-4

**Framing the street: Site open space, as required, is aggregated as usable pedestrian-oriented public space instead of a leftover 'green' perimeter. Courtyards and Plazas that are part of the development site are lined with active uses.**

Too often, open space site regulations result in unusable, suburban-style landscaped zones between the sidewalk and building. Dimensions and treatments often vary, resulting in a discontinuous, inefficient use of open space. As a result, the open space is 'wasted' rather than contributing to a vibrant public realm. The requirement to place buildings close to the public street rather than to surround buildings with yard areas will allow for the consolidation of open space into usable areas, which may consist of private courtyards and are encouraged to be public open spaces as a community amenity.

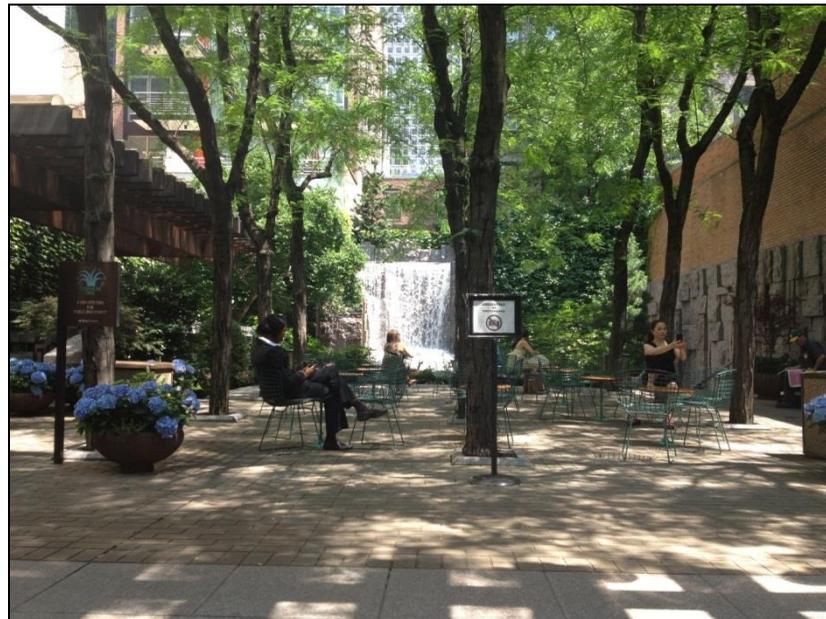


Figure 4.8

Open space should be consolidated and used to create pedestrian-friendly spaces, parks, and plazas; 'hard' surfaces mixed with landscaping should be encouraged to create usable, urban plazas.

Large, undifferentiated expanses of pavement or landscape areas intended primarily for ornamental use shall be discouraged. Other than for purposes of consolidation open space should not be located near existing open space. Open space should also be used to mark significant intersections or as forecourts for civic buildings or other buildings with a high degree of public access.



Figure 4.9

## B-5

**Framing the street: buildings meet the front and corner build-to-lines to maintain a consistent streetwall.**

In general the building streetwall should meet the build-to-lines except in cases of special entry features, architectural articulation, or in the instance of well-defined public spaces. When all the buildings along a street follow this principle the street forms a well-defined continuous corridor that encourages walkability and activity along its length.

**Primary Street:** The building frontage abutting a Primary Street should be brought to the build-to-line consistent with the established street cross-sections as illustrated within this document.

**Secondary Street:** The building frontage abutting a Secondary Street should be built to a zone consisting of 5 to 10 feet from the property line.

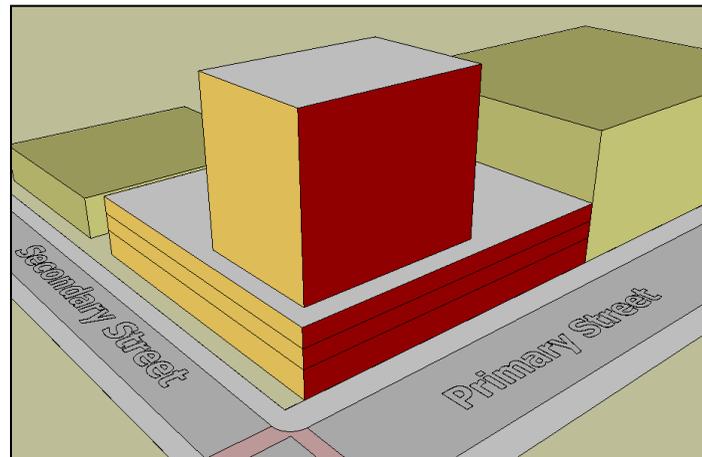


Figure 4.10

## B-6

**Framing the street: buildings meet the side yard setback to maintain a consistent streetwall.**

**Side / Rear Yard Setbacks: 0 ft\***

\*15 ft when abutting existing residential

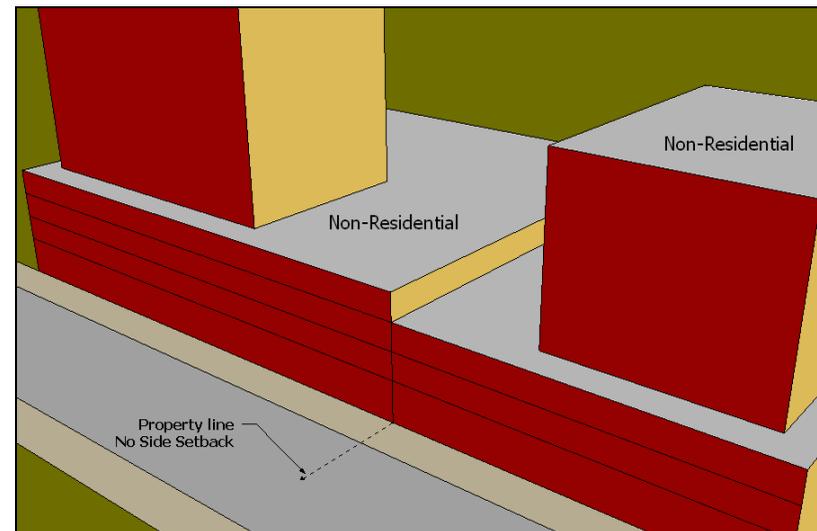


Figure 4.11

## Plan Reference

...buildings should locate close to the front property line (through a build-to line or maximum front setback, still allowing for a wide sidewalk) and parking should be located to the rear or side of the buildings, in order to promote a more pedestrian-friendly street environment. (SB/NE6 48)

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### Plan Reference

The height of a tower should be proportioned to the overall massing of your building, but generally should be no taller than one and one-third (1-1/3) of the maximum roof height. (SB/NE6 3-7)

## BUILDING DESIGN STANDARDS: NWRAC-MU

### B-7

**Framing the street: building streetwalls meet minimum and maximum shoulder heights.**

Consistent shoulder heights provide a defined streetwall and maintain a comfortable pedestrian scale.

#### Shoulder Height:

Minimum	Maximum
2 stories or 25 ft	5 stories or 65 ft

Maximum tower height shall be limited to 110-feet in height in the NWRAC-Mune and NWRAC-MUe zoning districts and 65-feet in the NWRAC-MUw subject to City Commission approval.

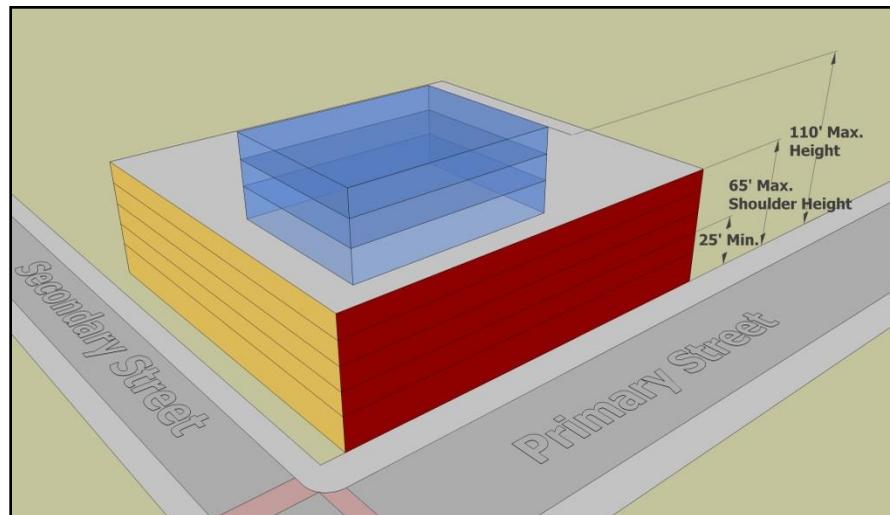


Figure 4.12

### B-8

**Framing the street: buildings exceeding a maximum streetwall length of 150 ft provide variation in the physical design and articulation of the streetwall.**

The principle of minimizing the impact of very long building frontages is desirable. Site-specific solutions need to ensure that the treatment and articulation along elevations provides attractive and pedestrian-friendly walking environments.

No structure on a development site shall exceed a maximum length of 150-feet along any right-of-way, unless it provides variation in the physical design and articulation of the streetwall through the following examples (other options may be approved subject to meeting the intent of the design standards):

- division into multiple buildings without superficial parapets
- a break/articulation of the façade
- significant change of massing/ façade design



Figure 4.13

## B-9

**Buildings do not exceed maximum height dimensions.**

Height may be permitted up to 5 stories and 65-feet when located in the NWRAC-MUone and NWRAC-MUe zoning districts, but no higher than 110 feet if approved by City Commission.

Height may be permitted up to 3 stories and 45-feet when located in the NWRAC-MUw, but no higher than 5 stories and 65-feet if approved by City Commission

Height shall be subject to the following limitations:

**Max. Height:**

110 feet – NWRAC-Mune and NWRAC-MUe (subject to City Commission approval)

65 feet (5 Stories) – NWRAC-MUw (subject to City Commission approval)

**Max. Floorplate:**

Commercial 32,000 s.f.

Residential 12,000 s.f.

**Min. Tower Separation:**

40 ft (depending on floorplate)

**Min. First Floor Height:**

Fifteen (15) feet

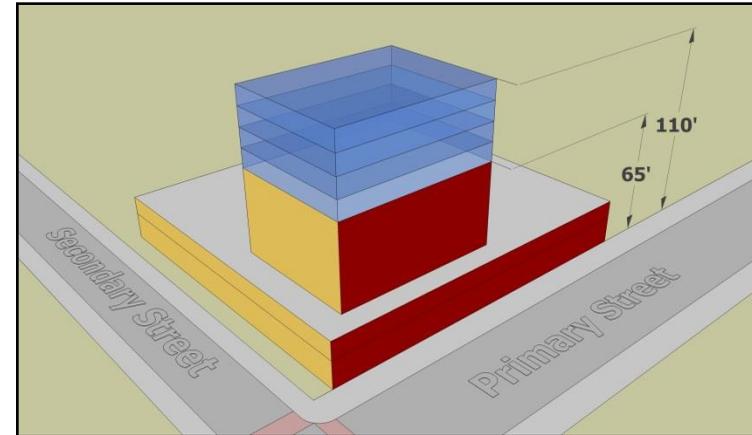


Figure 4.14

**Plan Reference**

The building height limits provide an appropriate progression—from low rise buildings in the MX-R district, to mid-rise in the MX-R/C district, and taller buildings in the UV-M district—to transition into the higher rise buildings anticipated in the Flagler Heights area and Downtown. (SB/NE6 2-12)

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**NOTE**

Preferred floorplate s.f. does not include open balcony areas if open on three sides.

Maximum floorplate area below shoulder height is not specified.

## BUILDING DESIGN STANDARDS: NWRAC-MU

**B-10**

**Towers do not exceed minimum stepback dimensions and maximum floorplate area.**

Reducing tower floorplate areas and setting minimum stepback dimensions will dramatically change the visual impact of tall buildings on the skyline, the street environment, and on views from nearby buildings.

Varying floorplate areas will encourage more slender towers (allowing more than one tower per project in some cases) and discourage massive, bulky, 'wall'-type buildings with larger floorplates, thereby providing more light and air to streets/open spaces below.

<b>Min. Tower Stepback</b>	<b>Front</b>	<b>Corner</b>	<b>Side</b>	<b>Rear</b>
Primary Street:	12 ft*	12 ft*	[Dependent on floorplate]	[Dependent on floorplate]
Secondary Street:	15 ft	15 ft	[Dependent on floorplate]	[Dependent on floorplate]

**Max. Floorplate / Min. Tower Stepback****Commercial**

32,000 s.f.	30 ft side and rear stepback
20,000 s.f.	25 ft side and rear stepback
16,000 s.f.	20 ft side and rear stepback

**Residential**

12,000 s.f.	30 ft side and rear stepback
10,000 s.f.	25 ft side and rear stepback
8,000 s.f.	20 ft side and rear stepback

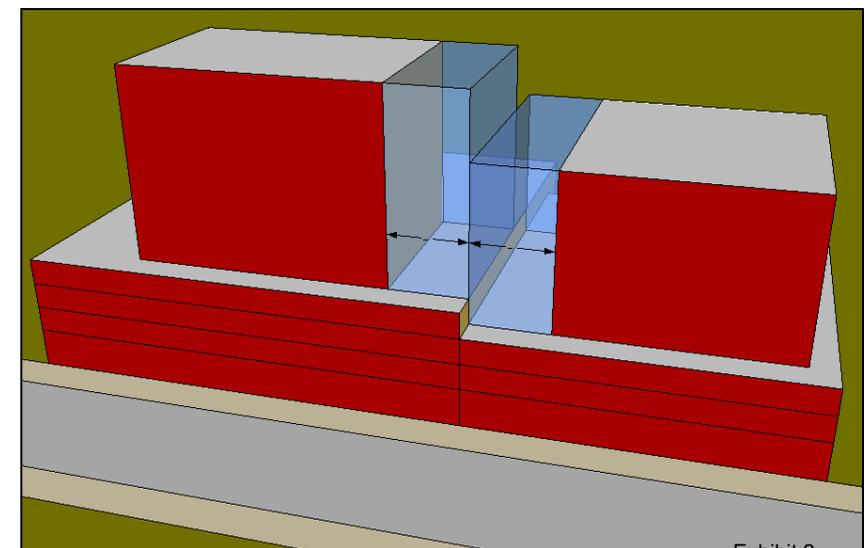


Figure 4.15

## B-11

Where buildings abut existing residential development a transition zone shall be established.

**Min. Yard Setback**

15-feet

**Max Shoulder Height**

45 ft

**Min. Tower Stepback**

15-feet

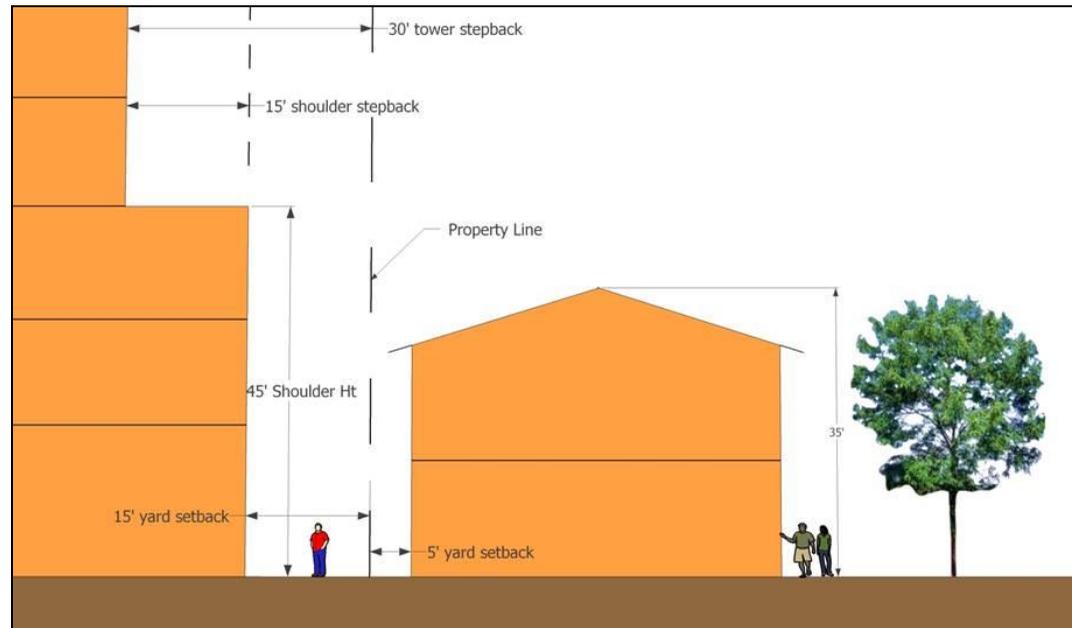


Figure 4.16

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### Plan Reference

Front setback requirements represent a desired build-to line. Some variation is encouraged, but using a relatively consistent front setback from structure to structure helps to define the street edge, reinforce a pedestrian scale, and create a sense of place (SB/NE6 2-12)

## BUILDING DESIGN STANDARDS: NWRAC-MU

### B-12

Where buildings with towers are located with frontages on multiple streets, the towers are oriented towards the “Primary Street”.

By placing the tower of a building closer to the primary street, the character of the street is better maintained.

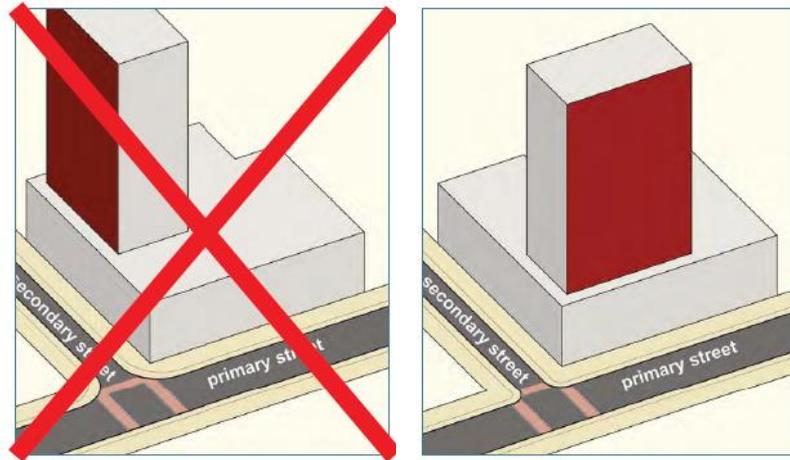


Figure 4.17

### B-13

Towers contribute to the overall skyline composition.

Buildings that propose tower elements should be designed to contribute to the overall skyline composition of Fort Lauderdale. Buildings should have architectural/sculptural elements designed to be seen from the appropriate distances.

Potential negative impacts from proposed towers elements should be reduced to the greatest extent possible.



Figure 4.18

## B-14

**Original and self-confident design: A range of architectural styles exist, each having a strong identity, and striving for the highest quality expression of its chosen architectural style.**



Figure 4.19



Figure 4.20

## Plan Reference

All façades that front on Sistrunk Boulevard or NE 6th Street shall be articulated architecturally along the length and height of the façade to emphasize a human scale. (SB/NE6 2-15)

Consideration was also given to whether a single theme should be applied to the entire corridor or whether each sub-district should have its own “style,” integrated to the larger context by a few common, cohesive elements. A variation of the latter approach was taken at the behest of the community, making the public realm components the unifying element throughout the corridor, but applying a more performance-oriented approach to the development and design of structures. Thus, no architectural style is dictated. However, specific urban design and architectural elements will be required to achieve the desired pedestrian orientation and street-level vitality. (SB/NE6 49)

### Plan Reference

The guidelines express only general design principles. They are intended to promote attractive, high-quality building designs, rather than to dictate a specific architectural style or a fixed set of specific design solutions. (SB/NE6 3-1)

## BUILDING DESIGN STANDARDS: NWRAC-MU

### B-15

**Buildings are of high quality design and construction with an emphasis on durable materials, well thought-out details and careful workmanship.**

Encourage high quality materials for the entire building, with a special emphasis on detailing and durability for the first 2 floors. Encourage richer materials, more intensive details and lighting to enhance pedestrian views at the first 2 floors.

Encourage durable exterior materials such as: stone, masonry, metal paneling, pre-cast concrete panels and details, and glass. Avoid less durable materials, such as EIFS, vinyl or aluminum siding, molded plastic or fiberglass details and moldings.



Figure 4.21



Figure 4.22



Figure 4.23

## B-16

Buildings are site responsive, reflect local character, and have architectural features and patterns that provide visual interest from the perspective of the pedestrian.



Figure 4.25



Figure 4.24



Figure 4.26

## Plan Reference

Where arcades are used, the columns should be spaced between ten and twenty feet (10'-20') apart to ensure adequate visibility to the commercial units. The depth of the arcade should not exceed ten feet (10'), from the back face of the columns to the outer wall of the building's ground floor. The use of large, heavy piers is discouraged; instead, use slim columns to maximize light and visual surveillance into the arcade from the street.

Canopies may be rigid, retractable, or made of flexible fabric, but in all cases should be attached directly to the building and designed in a way that complements its architecture. (SB/NE6 3-6)

Plan Reference

For walls, the use of cast-in-place or pre-cast concrete is encouraged. Concrete block and brick may also be used, with an appropriate surface finish (stucco or plaster). Other acceptable choices include wood (painted or stained), stone and cast stone, non-reflective glass, and architectural metals (preferably with shop-applied color finishes such as powder coating or anodizing). (SB/NE6 3-16)

B-17

**Creative façade composition: a rich layering of architectural elements are provided throughout the building, with special attention to details below the shoulder level.**

Encourage differentiation of the street level by a change in façade composition such as, but not limited to:

- Variety of window types and scale
- Changes in material
- Recess lines
- Roof gardens
- Expression of building openings
- Balconies
- Awnings
- Overhangs
- Sunscreens
- Low garden walls



Figure 4.27

B-18

**The first floor of nonresidential buildings are flush with the adjacent sidewalk, have a minimum height of fifteen (15) feet, and a high percentage of clear glazing:**

- Primary Streets: min. 60 %**
- Secondary Streets: min. 50%**

Large expanses of blank walls and use of tinted or reflective glass is discouraged. Opaque, smoked, or decorative glass should only be used for accents.

Ground floor window tops are encouraged to be no lower than 9' above the sidewalk. Restaurants are encouraged to provide clear visual and physical connections to outdoor seating.



Figure 4.28

**B-19**

**Buildings with historic value are preserved and utilized for adaptive re-use.**

Avoid design of a single building that is meant to imitate the look of multiple older buildings or mimic older buildings in a 'fake historic' style.

- Entire structure should be maintained
- Historic fabric should be restored
- Significant interior spaces maintained
- Existing scale and massing should be respected
- Sensitive, respectful rooftop & adjacent additions are permitted



Figure 4.29

**B-20**

**Architecture responds to the unique nature of the South Florida environment.**

- Solar orientation
- Wind direction
- Rain



Figure 4.30



Figure 4.31

**Plan Reference**

Historic heritage of Sistrunk Boulevard, Progresso Village, and the African American culture. (NPFHIP 14)

**NOTE**

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Individual graphics do not represent the accumulation of all design standards, but rather, each graphic focuses on a specific point presented as part of the referenced design standard.

### Plan Reference

All façades that front on Sistrunk Boulevard or NE 6th Street shall be articulated architecturally along the length and height of the façade to emphasize a human scale. Features such as doors, windows, porches, columns, pilasters, cornices, balconies, roof decks, canopies, and arcades, can be used to express the hierarchy of the façade elements, provided their use is appropriate to the architectural style of the structure. (SB/NE6 2-15)

## BUILDING DESIGN STANDARDS: NWRAC-MU

### B-21

**Pedestrian shading devices, of various types, are provided along the façade of buildings.**

Pedestrian comfort and visual interest can be achieved through consistent use of a variety of shading devices in conjunction with street trees. These elements may project beyond building setback lines, as permissible. Some options include:

- Awnings
- Arcades
- “Eyebrow” overhangs
- Miscellaneous shade structures



Figure 4.32



Figure 4.33

## B-22

**Active and 'extroverted' ground floors with retail are located in strategic locations.**

Active ground-floor retail should be focused along the Primary Streets and scattered in strategic neighborhood locations, such as along the edge of a neighborhood 'square'. Ground floor retail is not required for all new development; rather, it should be encouraged in market-supported areas that contribute to a well-planned, interconnected, active streetscape.

Where ground floor retail is not appropriate, other 'extroverted' program elements should be located on the ground floor or wherever possible such as residential common areas. These uses should have transparent and open facades and avoid blank walls wherever possible.



Figure 4.34



Figure 4.35

**Plan Reference**

The Sistrunk Boulevard and NW Seventh Avenue Initiative is the preferred location for new multi-family housing, retail and mixed use development. The goal is to reestablish this area as the "heart" of the Northwest as it was historically. (NPFHIP 29)

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# BUILDING DESIGN STANDARDS: NWRAC-MU

## B-23

**In residential buildings, ground floor units have individual entrances.**

Multiple residential entrances create increased and well-distributed pedestrian activity, and increased security (actual and perceived) on the street by adding activity and “eyes on the street”, especially in residential areas with little or no retail. Multiple entrances also create a more human-scaled, regular rhythm along the street.

Minimum ground floor elevation of 2’ above public sidewalk level is encouraged for individual ground floor entrances to provide safety and privacy.



Figure 4.37



Figure 4.36



Figure 4.38

## B-24

**Balconies and bay windows animate residential building facades.**

While balconies and bay windows add to the quality of residential units, they also contribute to the visual variety of the streetscape. Highly articulated building facades can break up the potential monotony of large-scale buildings. Balconies, in particular, take advantage of Fort Lauderdale's year-round climate by lining the streetwalls with people and living spaces.

Balconies and bay windows may project beyond building setback lines (to be coordinated with City Staff on a case by case basis, and subject to potential conflicts.)

When possible, depth of balconies should provide outdoor space that is usable and accessible by residents. "False" balconies are discouraged.



Figure 4.39

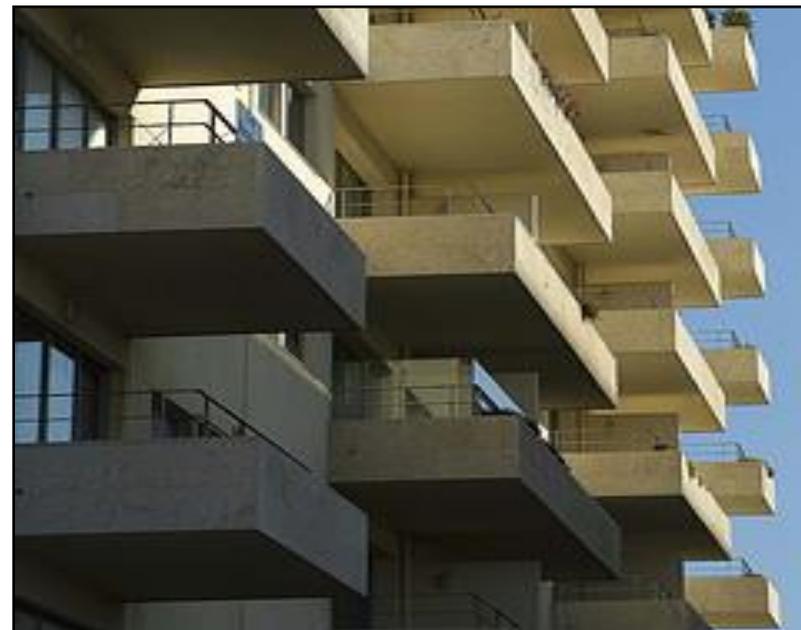


Figure 4.40

**Plan Reference**

Encouraging the use of architectural features such as towers, balconies, arcades, etc. (VI-3 SAMP)

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# BUILDING DESIGN STANDARDS: NWRAC-MU

## B-25

The “fifth façade” of a building is treated as part of the total design.

Encourage green roofs as visual amenities that provide a combination of usable, landscaped spaces (recreation & open space benefits) and sustainable roof treatments (environmental benefits).

Mechanical equipment, exhaust fans, generators and other similar noise-producing equipment should be muffled and directed away from streets, public spaces, and adjacent properties



Figure 4.41



Figure 4.42

## B-26

**Lighting is utilized to enhance safety without contributing to excessive light pollution or glare.**

Minimize “light trespass” (light shining in windows) by precluding unshielded floodlights, high wattage pedestrian lights, wall packs, and other unshielded light sources that are improperly located and poorly aimed.

Minimize “light pollution” (uncontrolled light traveling into atmosphere) that contributes to “sky glow” by avoiding unshielded light sources and excessively high lighting levels that are improperly located and aimed.

Promote appropriate light “temperature” (ie. color):  
yellow light (low pressure sodium) discouraged  
white light (metal halide/other) encouraged



Figure 4.43

## B-27

**Noise pollution as a result of building design is mitigated.**

Mechanical equipment, exhaust fans, generators and other similar noise-producing equipment should be muffled and directed away from streets, public spaces, and adjacent properties.



Figure 4.44

## NOTE

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# SECTION 2

Reserved

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# Northwest Regional Activity Center-Mixed Use (NWRAC-MU) Draft Amendments

## Sec. 47-13.1.3. List of Districts – Northwest Regional Activity Center

### A. Northwest Regional Activity Center – Mixed Use (NWRAC-MU)

1. North Regional Activity Center – Mixed Use northeast (NWRAC-MU<sub>ne</sub>)
2. North Regional Activity Center – Mixed Use east (NWRAC-MU<sub>e</sub>)
3. North Regional Activity Center – Mixed Use west (NWRAC-MU<sub>w</sub>)

## **Sec. 47-13.2.1. - Intent and purpose of each district.**

- A. *Downtown Regional Activity Center (RAC)*. This land use designation applies to the geographic area containing a mixture of large scale business, cultural, educational, governmental and residential uses which are in close proximity to mass transit resources (airport, port, rail and bus terminal). The purpose is to foster an active downtown within which one can work, live, entertain and shop without commuting to other districts in the city. The various RAC districts are described below.
1. *RAC-CC City Center District* is the city's high-intensity downtown zoning district, and is intended to be applied to the central downtown core area as a means of accommodating a wide range of employment, shopping, service, cultural, higher density residential and other more intense land uses. The RAC-CC zoning district will permit mixed use development including high intensity commercial uses, as well as downtown residential housing. Commercial retail uses will be required on the ground floor of buildings on those streets where pedestrian activity is encouraged. In order to ensure that development along the boundaries of the RAC-CC district will be compatible with adjacent zoning districts, properties abutting the edges of the RAC-CC district will be subject to regulations that provide a transition from the very intense and dense uses found within the central urban core.
  2. *RAC-AS Arts and Sciences District* is the city's downtown arts and sciences cultural district. It is located in those areas where cultural, civic entertainment, institutional and other complementary high-activity land uses draw patrons from the surrounding region.
  3. *RAC-UV Urban Village District* is intended to support the RAC-CC district by providing a mix of uses including institutional, office, commercial and residential. This area will encourage housing for the Downtown RAC. The RAC-UV regulations require ground floor retail, service and arts activity on the main street where pedestrians are encouraged. Also, residential uses will be permitted above business uses and encouraged to be located abutting the public street/sidewalk to promote an urban character.

4. *RAC-RPO Residential and Professional Office District* is intended to promote the preservation and enhancement of existing low-density residential neighborhoods south of the downtown area while providing for the continued development of neighborhood-serving commercial land uses, and professional and office uses similar to those which typically complement nearby governmental, judicial and medical centers.
5. *RAC-TMU Transitional Mixed-Use District* is intended to provide three transition areas between the high intensity RAC-CC, district and the lower intensity residential neighborhoods which abut the RAC. The area is intended to support the city center by allowing a wide range of employment, shopping, service, cultural and higher density residential neighborhoods. This area includes the expansion area where the downtown's urban core was expanded so as to provide a transition area surrounding the central urban core in order to protect the adjacent areas. There are three (3) TMU areas identified along the perimeter of the higher intensity RAC districts.
  - a. The *East Mixed Use (EMU)* is located east of the RAC-CC district, and includes residential areas on either side of Las Olas Boulevard and commercial business uses along Federal Highway and Las Olas Boulevard. Regulations within the EMU are designed to provide for a transition from intense uses permitted within the RAC-CC district to those established neighborhoods east of the EMU.
  - b. The *West Mixed Use (WMU)* is located north of the RAC-AS district and encompasses portions of the Sailboat Bend neighborhood fronting on NW 7 Avenue. Regulations within the WMU are designed to blend with adjacent neighborhoods such as City View, Dorsey Riverbend and Regal Trace and promote mixed use development to support the RAC-CC district, as well as create a "gateway" to the RAC-CC district.
  - c. The *Southwest Mixed Use (SMU)* is located south of the RAC-AS district, along the New River to S.W. 7th Street. Development in this area is intended to preserve marine related uses, as well as promote mixed use development to support the RAC-CC district while blending with the Tarpon River community.

B. *South Regional Activity Center (SRAC).*

1. *South Regional Activity Center (SRAC).* This land use designation applies to the geographical area containing a mixture of professional office, small to medium scale businesses, cultural and residential uses. The purpose is to foster an active pedestrian friendly environment while maintaining the established eclectic atmosphere of the area.
  - a. SRAC-SA is intended to promote an active urban environment with a mix of uses characteristic of the traditional character of the South Andrews neighborhood. To this end, the district will allow residential and mixed-use development to create a true urban area complete with both daytime and

evening activity. This will be accomplished by requiring the following: high quality buildings with minimal setbacks and oriented to provide light and air at the street level, active occupied spaces at the ground floor and enhanced streetscape consisting of tree-lined streets encouraging an active and comfortable pedestrian environment. Landscaping should be consolidated into useable park-like areas consisting of plazas and open space. On-site parking will be designed in such a way that the vehicle will be as imperceptible as possible and interference with pedestrian pathways minimized. Crime Prevention through Environmental Design (CPTED) principles shall be incorporated in the design of the streets, parking areas and public areas in a manner that makes the area less attractive to criminal activities. SRAC-SA has been further refined to distinguish between SRAC-SA east (SRAC-SAe) and SRAC-SA west (SRAC-SAw) zoning district.

- i. The SRAC-SAw zoning district is intended to be an area of more intensive uses consisting of heavy non-residential business uses, wholesale, warehousing, storage operations and establishments conducting activities of the same general character as well as those uses intended to meet the shopping and service needs of the community. Residential uses are permitted and encouraged to promote a diverse character. The SRAC-SAw zoning district is located west of those properties abutting SW 1st Avenue and follows the zoning line of the previous Heavy Commercial/Light Industrial Business District (B-3) zoning district of the area to the east portion of the FEC corridor.
- ii. The SRAC-SAe zoning district is intended to meet the shopping and service needs of the community as well as limited wholesale uses. Residential uses are permitted and encouraged to promote a diverse character. The SRAC-SAe zoning district is generally located within the same zoning boundaries of the previous Community Business District (CB) zoning district of the area.

C. Northwest Regional Activity Center (NWRAC). *Northwest Regional Activity Center (NWRAC)*. This land use designation applies to the geographical area containing a mixture of small to medium scale businesses, cultural and residential uses. The purpose is to foster an active pedestrian friendly environment while maintaining the established historic and eclectic atmosphere and cultural diversity of the area through long-term sustainable redevelopment and adaptive reuse.

1. *NWRAC-MU Northwest Regional Activity Center Mixed Use* is intended to promote and enhance the existing commercial and residential character of the main corridors of the NWRAC by providing a wide range of employment, shopping, services, cultural and residential opportunities through allowing a mix of residential and non-residential uses. These areas include higher densities along the corridors transitioning to the lower densities and intensities of the surrounding zoning districts subject to adopted regulations.

Sec. 47-13.10.

List of permitted and conditional uses, Regional Activity Center-City Center (RAC-CC); Regional Activity Center-Arts and Science (RAC-AS); Regional Activity Center-Urban Village (RAC-UV); Regional Activity Center-Residential Professional Office (RAC-RPO); Regional Activity Center-Transitional Mixed Use (RAC-TMU); South Regional Activity Center-South Andrews east (SRAC-SAe); South Regional Activity Center-South Andrews west (SRAC-SAw); Northwest Regional Activity Center-Mixed Use northeast (NWRAC-MU<sub>ne</sub>), Northwest Regional Activity Center-Mixed Use east (NWRAC-MU<sub>e</sub>) & Northwest Regional Activity Center-Mixed Use west (NWRAC-MU<sub>w</sub>) collectively known as NWRAC-MU.

Use Type	Zoning District								
	RAC-CC	RAC-AS	RAC-UV	RAC-RPO	RAC-TMU	SRAC-SAe	SRAC-SAw	<u>NWRAC-MU<sub>e</sub> and NWRAC-MU<sub>w</sub></u>	<u>NWRAC-MU<sub>ne</sub></u>
<b>Key:</b>									
<b>P - Permitted</b>									
<b>C - Conditional</b>									
<b>Automotive</b>									
<del>Automotive Parts &amp; Supplies Store, including installation in wholly enclosed buildings, permitting wholesale sales</del>								<u>P</u>	
Automotive Parts & Supplies Store (installation in wholly enclosed buildings including wholesale sales in the SRAC-SAw, NWRAC-MU <sub>ne</sub> , NWRAC-MU <sub>e</sub> and NWRAC-MU <sub>w</sub> zoning districts)	<u>P</u>		<u>P</u>	<u>P</u>	<u>P</u>	<u>P</u>	<u>P</u>	<u>P</u>	<u>P</u>
Automotive Repair Shop, Major Repair, see Section 47-18.4							<u>P</u>		<u>P</u>
Automotive Repair Shop, Minor Repair, see Section 47-18.4	<u>P</u>								<u>P</u>
<del>Automotive Sales, Rental, new or used vehicles, see Section 47-18.3, abutting Federal Highway only</del>	<u>P</u>		<u>P</u>		<u>P</u>				
Automotive Sales, Rental, new or used vehicles, see Section 47-18.3 (only permitted when abutting Federal Highway in the RAC-CC, RAC-UV and RAC-TMU zoning districts)	<u>P</u>		<u>P</u>		<u>P</u>		<u>P</u>		<u>P</u>
<del>Automotive Service Station, see Section 47-18.5, abutting Federal Highway and Broward Boulevard only</del>					<u>P</u>				
<del>Automotive Service Station, see Section 47-18.5, abutting Federal Highway and Andrews Avenue only</del>				<u>P</u>					
Automotive Service Station, see Section 47-18.5, abutting Federal Highway only	<u>P</u>		<u>P</u>						

Use Type	Zoning District									
	RAC-CC	RAC-AS	RAC-UV	RAC-RPO	RAC-TMU	SRAC-SAe	SRAC-SAw	NWRAC-MUe and NWRAC-MUw	NWRAC-MUe	
<b>Key:</b>										
<b>P - Permitted</b>										
<b>C - Conditional</b>										
Automotive Service Station, see Section 47-18.5 (only permitted when abutting Federal Highway and Broward Boulevard in RAC-TMU; abutting Federal Highway and Andrews Ave in RAC-RPO; abutting Federal Highway in RAC-CC and RAC-UV zoning districts)	P		P	P	P	P	P	P	P	P
Car Wash, Automatic, see Section 47-18.7 (Car Wash, Outdoor Hand-wash permitted as conditional use in SRAC-SAw and NWRAC-MUe, NWRAC-MUe and NWRAC-MUw zoning districts)	P							P	C	C
Motorcycle/Moped Sale, wholesale sales permitted								P		P
Recreation Camper and Trailers, Sales and Rental, new or used, wholesale sales permitted, see Section 47-18.27								P		
Taxi Lot/Operations								P		
Tire sales, including Retreading and Service (wholesale sales permitted in SRAC-Saw )	P				P			P		P
<b>Boats, Watercraft and Marinas</b>										
Charter and Sightseeing Boat, see Section 47-23.8	P	C			C			C	C	C
Hotel Marina, see Section 47-23.8	P	C			C			C		
Marina, see Section 47-23.8	P	C		C	C	C	C	C	C	C
Marine Parts and Supplies Store	P	P	P	P	P	P	P	P	P	P
Marine Service Station, see Section 47-18.20	P	C			C			C		
Sailmaking								P		
Shipyards								C		
Watercraft Repair, major repair, see Section 47-18.37								P		
Watercraft Repair, minor repair, see Section 47-18.37 (only allowed within the NWRAC-MUe)									C	C
Watercraft Sales and Rental, new or used, see Section 47-18.36 (restricted to S.W. 7th Avenue and accessory uses in RAC-AS), (only allowed within the NWRAC-MUe and NWRAC-MUe)	P	P						P	C	C
<b>Commercial Recreation</b>										
Amphitheater		C							P	P

EXHIBIT 5  
CASE #s T14012 ULDR Text Amendment and 3Z13 Rezoning

Use Type	Zoning District									
	RAC-CC	RAC-AS	RAC-UV	RAC-RPO	RAC-TMU	SRAC-SAe	SRAC-SAw	NWRAC-MUe and NWRAC-MUw	NWRAC-MUe	
<b>Key:</b>										
<b>P - Permitted</b>										
<b>C - Conditional</b>										
Billiard Parlor	P		P				P	P	<u>P</u>	<u>P</u>
Bingo Hall								P	<u>P</u>	<u>P</u>
Bowling Alley					P			P	<u>P</u>	<u>P</u>
Indoor Firearms Range, see Section 47-18.18								C		
Indoor Motion Picture Theater (fewer than 5 screens in RAC-UV; and SRAC-SAe and NWRAC-MUe, NWRAC-MUe and NWRAC-MUw zoning districts)	P	P	P	P	P	P	P	P	<u>P</u>	<u>P</u>
Performing Arts Theater, less than 300 seats (unlimited seating permitted in SRAC-SAw zoning district)	P	P	P	P	P	P	P	P	<u>P</u>	<u>P</u>
Professional Sports Clubs, including facilities such as arenas, stadiums, athletic fields and skating centers	P									
<b>Food and Beverage Sales and Service</b>										
Bakery Store	P	P	P	P	P	P	P	P	<u>P</u>	<u>P</u>
Bar, Cocktail Lounge, Nightclub	P	P	P	P	P	P	P	P	<u>P</u>	<u>P</u>
Cafeteria	P	P	P	P	P	P	P	P	<u>P</u>	<u>P</u>
Candy, Nuts Store	P	P	P	P	P	P	P	P	<u>P</u>	<u>P</u>
Catering Service	P		P		P		P		<u>P</u>	<u>P</u>
Delicatessen	P	P	P	P	P	P	P	P	<u>P</u>	<u>P</u>
Food and Beverage, Drive-Thru and Carryout (no Drive-Thru permitted in the RAC-CC zoning district) (no Carryout permitted RAC-UV zoning district)	P		P	P	P	P	P	P	<u>P</u>	<u>P</u>
Fruit and Produce Store	P		P	P	P	P	P	P	<u>P</u>	<u>P</u>
Grocery/Food Store ( <u>Convenience Store prohibited in the NWRAC-MUe, NWRAC-MUe and NWRAC-MUw zoning districts</u> )	P	P	P	P	P	P	P	P	<u>P</u>	<u>P</u>
Ice Cream/Yogurt Store	P	P	P	P	P	P	P	P	<u>P</u>	<u>P</u>
Liquor Store	P		P	P	P	P	P			
Meat and Poultry Store	P		P	P	P	P	P	P	<u>P</u>	<u>P</u>
Restaurant	P	P	P	P	P	P	P	P	<u>P</u>	<u>P</u>
Seafood Store	P		P	P	P	P	P	P	<u>P</u>	<u>P</u>
Supermarket	P		P	P	P	P	P	P	<u>P</u>	<u>P</u>
<u>Wine Specialty Store</u>									<u>P</u>	<u>P</u>
<b>Light Manufacturing</b>										
Apparel, Textile, Canvas and related uses								P		

EXHIBIT 5  
CASE #s T14012 ULDR Text Amendment and 3Z13 Rezoning

Use Type	Zoning District									
	RAC-CC	RAC-AS	RAC-UV	RAC-RPO	RAC-TMU	SRAC-SAe	SRAC-SAw	NWRAC-MUe and NWRAC-MUw	NWRAC-MUe	
<b>Key:</b>										
<b>P - Permitted</b>										
<b>C - Conditional</b>										
Contractor's yards							P			
Processing and assembly of previously prepared materials							P			
<b>Lodging</b>										
Bed and Breakfast Dwelling, see Section 47-18.6	P	P	P	P	P	P	P	<u>P</u>	<u>P</u>	
Hotel, see Section 47-18.16	P	P	P	P	P	P	P	<u>P</u>	<u>P</u>	
<b>Manufacturing*</b>										
<b>*Permitted only when contiguous to or separated by no more than a 60-foot public right-of-way from a railroad right-of-way in the RAC-CC, RAC-AS, RAC-UV, RAC-RPO, &amp; RAC-TMU zoning districts.</b>										
Apparel, Textile, Canvas and related uses	P		P	P	P		P			
Contractor's yards	P		P	P	P		P			
Industrial Machinery and Equipment	P		P	P	P					
Processing and assembly of previously prepared materials	P		P	P	P		P			
<b>Public Purpose Facilities</b>										
Active and Passive Park	P		P							
Bus Terminal, Railroad Station, Transportation Terminal	P						P			
Civic and Private Club Facility	P	P	P	P	P	P	P	<u>P</u>	<u>P</u>	
College, University	C		P					<u>P</u>	<u>P</u>	
Communication Towers, Structures, and Stations, see Section 47-18.11							C	<u>C</u>	<u>C</u>	
Conservation Area	P		P							
Courthouse	P							<u>P</u>	<u>P</u>	
Cultural, Educational and Civic Facility	P				P			<u>P</u>	<u>P</u>	
Detention Center, Jail	C									
Fire Facility	P									
Government Facility	P		P	P	P	P	P	<u>P</u>	<u>P</u>	
Helistop, see Section 47-18.14	C									
Hospital, Medical and Public Health Clinic	C					C	P	<u>C</u>	<u>C</u>	
House of Worship	P		P	P	P	P	P			
Indoor Firearms Range, see Section 47-18.18	C									
Indoor and Outdoor Recreational Facility	P									
Library	P	P	P	P	P	P	P	<u>P</u>	<u>P</u>	
Museum and Art Gallery	P	P	P	P	P	P	P	<u>P</u>	<u>P</u>	

Use Type	Zoning District									
	RAC-CC	RAC-AS	RAC-UV	RAC-RPO	RAC-TMU	SRAC-SAe	SRAC-SAw	NWRAC-MUe and NWRAC-MUw	NWRAC-MUe	
<b>Key:</b>										
<b>P - Permitted</b>										
<b>C - Conditional</b>										
Police and Fire Substation	P	P	P	P	P	P	P	<u>P</u>	<u>P</u>	
Post Office Branch/Substation	P		P	P	P	P	P	<u>P</u>	<u>P</u>	
Public Maintenance and Storage Facility	P									
Public/Private Meeting Rooms	P							<u>P</u>	<u>P</u>	
Public/Private Recreation		P	P	P	P	P	P	<u>P</u>	<u>P</u>	
Railroad Freight							P			
Radio Station (digital)/Broadcast								<u>P</u>	<u>P</u>	
School, including <u>trade/business school (trade school not permitted in RAC-CC and RAC-TMU) (permitted in RAC-UV only when contiguous to or separated by on more than a 60-foot public right-of-way from a railroad right-of-way)</u>	P		P	P	P	P	P	<u>P</u>	<u>P</u>	
Social Service Facilities, see Section 47-18.31	C	C	C	C	C		C			
Social Service Residential Facility, see Section 47-18.32	C	C	C	C	C	C	C	<u>C</u>	<u>C</u>	
<b>Residential Uses</b>										
Single-Family Dwelling, Standard			P	P	P	P	P			
Cluster Dwellings, see Section 47-18.9			P	P	P	P	P			
Coach Homes, see Section 47-18.10	P	P	P	P	P	P	P			
Multi-family Dwelling	P	P	P	P	P	P	P	<u>P</u>	<u>P</u>	
Rowhouse, see Section 47-18.28	P	P	P	P	P	P	P			
Townhouse, see Section 47-18.33			P	P	P	P	P	<u>P</u>	<u>P</u>	
Two-Family/Duplex Dwellings			P	P	P	P	P			
Zero Lot Line Dwelling, see Section 47-18.38			P	P	P	P	P			
<b>Retail Sales (*Including Wholesale Sales)</b>										
Antiques Store	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>	
Apparel/Clothing, Accessories Store	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>	
Apothecary		P								
Art Galleries, Art Studio, Dealer	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>	
Arts & Crafts Supplies Store	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>	
Bait and Tackle Store	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>	
Bicycle Shop	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>	
Book Store	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>	

Use Type	Zoning District								
	RAC-CC	RAC-AS	RAC-UV	RAC-RPO	RAC-TMU	SRAC-SAe	SRAC-SAw	NWRAC-MUe and NWRAC-MUw	NWRAC-MUe
<b>Key:</b>									
<b>P - Permitted</b>									
<b>C - Conditional</b>									
Building Supplies, Materials and Equipment with Outdoor Storage, see Section 47-19.9 (In the RAC-CC this use is only permitted as Wholesale Sales and only when contiguous to or separated by no more than a 60-foot public right-of-way from a railroad right-of-way.)	P								
Camera, Photographic Supplies Store	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>
Candle Shop		P						<u>P</u>	<u>P</u>
Card & Stationery Store	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>
Cigar, Tobacco Store	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>
Computer/Software Store	P		P	P	P	P	*P	<u>P</u>	<u>P</u>
Consignment, Thrift Store	P		P	P	P	P	*P	<u>P</u>	<u>P</u>
Cosmetics, Sundries Store	P		P	P	P	P	*P	<u>P</u>	<u>P</u>
Department Store	P		P	P	P	P	*P	<u>P</u>	<u>P</u>
Pharmacy	<u>P</u>	<u>P</u>	<u>P</u>	<u>P</u>	<u>P</u>	<u>P</u>	<u>*P</u>		
Fabric, Needlework, Yarn Shop	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>
Firearms Store (In the SRAC-SAw zoning district this use is only permitted as Wholesale Sales.)							P		
Flooring Store	P		P	P	P	P	*P	<u>P</u>	<u>P</u>
Florist Shop	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>
Furniture Store	P		P	P	P	P	*P	<u>P</u>	<u>P</u>
Gasoline Sales on restored premises originally designed for this purpose		P							
General Store in character with historic district		P							
Gifts, Novelties, Souvenirs Store	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>
Glassware, China, Pottery Store	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>
Hardware Store	P		P	P	P	P	*P	<u>P</u>	<u>P</u>
Hobby Items, Toys, Games Store	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>
Holiday Merchandise, Outdoor Sales, see Section 47-18.15	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>
Home Improvement Center (abutting railroad track only in RAC-CC zoning district)	P				P		*P		
Household Appliances Store	P		P	P	P	P	*P	<u>P</u>	<u>P</u>
Jewelry Store	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>

Use Type	Zoning District								
	RAC-CC	RAC-AS	RAC-UV	RAC-RPO	RAC-TMU	SRAC-SAe	SRAC-SAw	NWRAC-MUe and NWRAC-MUw	NWRAC-MUe
<b>Key:</b>									
<b>P - Permitted</b>									
<b>C - Conditional</b>									
Lawn and Garden Center, Outdoor Display (abutting railroad track only in RAC-CC zoning district)	P		P	P	P	P	*P	<u>P</u>	<u>P</u>
Linen, Bath, Bedding Store	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>
Luggage, Handbags, Leather Goods Store	P		P	P	P	P	*P	<u>P</u>	<u>P</u>
Lumber Yards							*P		
Medical Supplies Store	P		P	P	P	P	*P	<u>P</u>	<u>P</u>
Mobile Vendor, see Section 47-18.22	P		P				P	<u>P</u>	<u>P</u>
Music, Musical Instruments Store	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>
Newspapers, Magazines Store	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>
Nursery, Plants, Flowers							*P	<u>P</u>	<u>P</u>
Office Supplies, Equipment Store	P		P	P	P	P	*P	<u>P</u>	<u>P</u>
Optical Store	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>
Paint, Wallpaper Store	P		P	P	P	P	*P	<u>P</u>	<u>P</u>
Party Supply Store	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>
Pawn Shop							*P		
Pet Store	P		P	P	P	P	*P	<u>P</u>	<u>P</u>
Pharmacy	<u>P</u>	<u>P</u>	<u>P</u>	<u>P</u>	<u>P</u>	<u>P</u>	<u>*P</u>	<u>P</u>	<u>P</u>
Plumbing Equipment Sales (Only permitted as Wholesale Sales and only when contiguous to or separated by no more than a 60-foot public right-of-way from a railroad right-of-way.)	*P								
Pump and Well Sales (Only permitted as Wholesale Sales and only when contiguous to or separated by no more than a 60-foot public right-of-way from a railroad right-of-way.)	*P								
Restaurant and Hotel Equipment Sales (Only permitted as Wholesale Sales and only when contiguous to or separated by no more than a 60-foot public right-of-way from a railroad right-of-way.)	*P								
Security Systems			P	P		P	P	<u>P</u>	<u>P</u>
Shoe Store	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>
Shopping Center				P	P	P	*P	<u>P</u>	<u>P</u>
Silversmith		P							

Use Type	Zoning District								
	RAC-CC	RAC-AS	RAC-UV	RAC-RPO	RAC-TMU	SRAC-SAe	SRAC-SAw	NWRAC-MUe and NWRAC-MUw	NWRAC-MUe
<b>Key:</b>									
<b>P - Permitted</b>									
<b>C - Conditional</b>									
Sign Sales (Only permitted as Wholesale Sales permitted and only when contiguous to or separated by no more than a 60-foot public right-of-way from a railroad right-of-way.)	*P								
Sporting Goods Store	P		P	P	P	P	*P	<u>P</u>	<u>P</u>
Swimming Pools, Hot Tubs & Spas, supplies and service (In the RAC-CC this use is permitted as Wholesale Sales when contiguous to or separated by no more than a 60-foot public right-of-way from a railroad right-of-way.)	*P				P		*P		
Tapes, Videos, Music CD's Store	P	P	P	P	P	P	*P	<u>P</u>	<u>P</u>
Woodcraft		P							
<b>Services/Office Facilities (*Including Wholesale Service)</b>									
Auction House		P	*P		P		*P		
Check Cashing Store	P		*P	P	P	*P	*P		
Child Day Care Facilities, Corporate/Employee Sponsors, see Section 47-18.8							*P	<u>P</u>	<u>P</u>
Child Day Care Facilities, Large, see Section 47-18.8	P		P	P	P		*C	<u>C</u>	<u>C</u>
Contractors			*P				*P	<u>P</u>	<u>P</u>
Copy Center	P		*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Dry Cleaner, see Section 47-18.12 (laundering plant permitted only in RAC-UV, SRAC-SAw zoning districts)	P		* <u>P</u>	P	P	*P	*P	<u>P</u>	<u>P</u>
<del>Dry Cleaning, Laundering Plant</del>			<del>*P</del>				<del>*P</del>		
Equipment Rental							*P		
Film Processing Store	P	P	*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Financial Institution, including Drive-Thru Banks	P		*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Formal Wear, Rental	P		*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Fortunetellers and Psychic Readers							*P		
Funeral Home			*P				*P	<u>P</u>	<u>P</u>
Hair Salon	P	P	*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Health and Fitness Center	P	P	*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Helistop, see Section 47-18.14	C								

Use Type	Zoning District								
	RAC-CC	RAC-AS	RAC-UV	RAC-RPO	RAC-TMU	SRAC-SAe	SRAC-SAw	NWRAC-MUe and NWRAC-MUw	NWRAC-MUe
<b>Key:</b>									
<b>P - Permitted</b>									
<b>C - Conditional</b>									
Instruction: Fine Arts, Sports and Recreation, Dance, Music, Theater	P	P	*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Interior Decorator	P	P	*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Laundromat, see Section 47-18.19	P		*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Mail, Postage, Fax Service	P	P	*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Massage Therapist	P		*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Medical/Dental Office/Clinic	P		*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Mover, Moving Van Service							*P		
Nail Salon	P	P	*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Nursing Home, see Section 47-18.23	P				P	*C	*C	<u>C</u>	<u>C</u>
Parking Facility, see Section 47-20	P	P	*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Personnel Services, including Labor Pools	P		*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Pest Control			*P				*P		
Pet Boarding Facility, Domestic Animals Only							*P		
Photographic Studio	P	P	*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Professional Office	P	P	*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Publishing Plant			*P				*P	<u>P</u>	<u>P</u>
Security Systems	P		*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Senior Citizen Center, see Section 47-18.30	P		*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Shoe Repair, Shoe Shine	P	P	*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Tailor, Dressmaking Store, Direct to the Customer	P	P	*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Tanning Salon	P	P	*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Tattoo Artist	P	P	*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Taxidermist			*P				*P		
<del>Trade/Business School</del>	<del>P</del>		<del>*P</del>	<del>P</del>	<del>P</del>	<del>*P</del>	<del>*P</del>		
Travel Agency	P	P	*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Veterinary Clinic, see Section 47-18.35			*P	P	P	*P	*P	<u>P</u>	<u>P</u>
Warehouse Facility			*P						
Watch and Jewelry Repair	P	P	*P	P	P	*P	*P	<u>P</u>	<u>P</u>
<b>Storage Facilities</b>									
<b>*Permitted only when contiguous to or separated by no more than a 60-foot public right-of-way from a railroad right-of-way in the RAC-CC, RAC-AS, RAC-UV, RAC-RPO, &amp; RAC-TMU zoning districts.</b>									
Automotive Wrecking and Salvage Yards, Junk Yards, see Outdoor Storage of good and materials, see Section 47-19.9	*P								
Self Storage Facility, see Section 47-18.29	*C						P		

EXHIBIT 5  
CASE #s T14012 ULDR Text Amendment and 3Z13 Rezoning

Use Type	Zoning District									
	RAC-CC	RAC-AS	RAC-UV	RAC-RPO	RAC-TMU	SRAC-SAe	SRAC-SAw	NWRAC-MUe and NWRAC-MUw	NWRAC-MUe	
<b>Key:</b>										
<b>P - Permitted</b>										
<b>C - Conditional</b>										
Warehouse Facility	*P		*P				P			
<b>Accessory Uses, Buildings and Structures (See Section 47-19)</b>										
Accessory uses to Hotels, see Section 47-19.8	P	P	P	P	P	P	P	<u>P</u>	<u>P</u>	
Catering Services (accessory to restaurant, bakery or performing arts theatre in RAC-CC zoning district)	P	P					P	P	<u>P</u>	<u>P</u>
Child Day Care - Corporate/Employee Sponsors when accessory to professional office, see Section 47-18.8	P	P	P	P	P	P	P	P	<u>P</u>	<u>P</u>
Concessions, accessory to parks, including refreshment stands, pro shops, souvenir shops	P								<u>P</u>	<u>P</u>
Electronic Installation, when accessory to electronic sales, only in wholly enclosed building	P					P			<u>P</u>	<u>P</u>
Film Processing, when accessory to a permitted use (when accessory to a pharmacy or copy center in RAC-TMU zoning district)						P	P	P	<u>P</u>	<u>P</u>
Outdoor Dining and Sidewalk Cafés, see Section 47-19.9	P	P	P	P	P	P	P	P	<u>P</u>	<u>P</u>
Outdoor storage, see Section 47-19.9								P		
Parks Maintenance and Administrative Facility	P									
Public Restrooms, when accessory to a park	P									
Utility Facilities incidental to other uses	P									
Video Games Arcade, when accessory to a shopping center	P					P		P	<u>P</u>	<u>P</u>
Warehouse Facilities								P		
Watercraft Rental Facility, see Chapter 8, Article V, Division 3 of Volume I of this Code, and Section 47-23.7	P									
Watercraft Sales and Rental, new or used when accessory to a Marina		P							<u>C</u>	<u>C</u>
<b>Urban Agriculture See Section 47-18.41</b>										

Secs. 47-13.22 – 47-13.28~~29~~. Reserved

**Sec. 47-13.29. Design Standard Applicability.**

EXHIBIT 5  
CASE #s T14012 ULDR Text Amendment and 3Z13 Rezoning

A. Sections 47-13.30 through 47-13.60 shall be read in conjunction with the following adopted design standards. Should a conflict between the requirements of the ULDR and the adopted design standards be found, the design standards shall take precedence.

1. SRAC-SA Illustrations of Design Standards

2. NWRAC-MU Illustrations of Design Standards

B. Development shall be subject to, but shall not be limited by the following to meet the intent of the design standards as indicated in Section 47-13.29 above:

1. Development shall be required to meet all design standards including but not limited to the following:

Building orientation,  
Architectural requirements,  
Open Space,  
Vehicular and pedestrian access,  
Building materials,  
Active ground floor uses,  
Façade.

2. *Parking Facilities.*

a. Off-street parking regulations are as provided in Section 47-20, Parking and Loading Requirements.

b. *Parking garage.* The minimum design standards for a parking garage are:

i. Sloped garage ramps facing public right-of-ways shall have ornamental grating or other architectural features which screen the sloped ramp from view of the right-of-way.

ii. Parked vehicles shall be screened from view from abutting public rights-of-way, excluding alleys. Screening may be provided by intervening buildings, architectural detailing such as ornamental grating, or landscaping.

iii. Pedestrian walkways shall be provided between a parking garage and any principal or accessory building it serves and to abutting public rights-of-ways and public spaces.

iv. When a parking garage is provided for a principal structure on the same plot, the design of the parking garage shall complement and contain architectural features consistent with the principal structure.

3. *Landscaping.* Development shall meet the following landscape requirements:

- a. VUA landscaping: Surface parking lots shall meet the landscape requirements for vehicular use areas as provided in Section 47-21, Landscaping and Tree Preservation Requirements.
- b. Location of Street Trees. The requirements for street trees, as provided herein, may be located within the public rights-of-way, as provided by the entity with jurisdiction over the abutting right-of-way.
- c. All other landscape requirements in accordance with the SRAC-SA Design Standards and the NWRAC-MU Design Standards.

4. Signage.

- a. Development shall be required to meet the signage requirement applicable in the Community Business (CB) zoning district as provided in Section 47-22, Sign Requirements.

5. Streetscape.

- a. Streetscape improvements are required to be made as a part of a development. The required streetscape improvements shall be required to be made to that portion of the right-of-way abutting the proposed development site. If a development is located on two Primary Streets or a Primary Street and a Secondary Street, street improvements shall be required to be made to both rights-of-way. These streetscape improvements may include but are not limited to the following:

- i. Street Trees.
- ii. Sidewalk.
- iii. Parking.
- iv. Medians.
- v. Curb and gutter.
- vi. Landscaping.
- vii. Street furniture.
- viii. Transit Stop.
- ix. Traffic control devices.

- b. Each applicant shall be responsible for making the streetscape improvements in accordance with the adopted design standards and the applicable to the abutting right-of-way.

If a right-of-way is not under City of Fort Lauderdale jurisdiction and the authority with jurisdiction will not permit the improvement, or if, as determined by the City Manager, the streetscape improvement cannot reasonably be made at the time the development is constructed, the department shall estimate the cost of the streetscape improvement and the sum shall be paid by the applicant to the City to be held and earmarked for such streetscape improvement to be made in the future. If the streetscape improvement is unable to be made within 5 years of

development approval, the sum shall be refunded to the applicant including interest accrued at a rate accrued on similar City funds.

c. Modification to the required streetscape improvements may be permitted based on the preservation of natural barriers, avoidance of interference with overhead lines or other obstructions as approved by the City's Landscape Planner or may be modified based on an alternative design found to achieve the underlying intent of the streetscape design as indicated in the adopted design standards.

d. Applicant shall be required to execute maintenance agreement providing for the repair, replacement and maintenance of required off-site improvements in form approved by the City Engineer, to be recorded in the public records of Broward County at applicant's expense. The City Engineer is authorized to execute said agreement on behalf of City.

6. Accessory structures.

a. Fencing. Chain-link fencing shall not be permitted abutting any Primary or Secondary street. In all other areas of the RAC, all chain-link fencing shall be black vinyl coated. Temporary fencing may be permitted pursuant to Section 47-19.5.B.

**Sec. 47-13.30. - Table of Dimensional Requirements for the SRAC Districts.**

REQUIREMENTS	SRAC-SAe & SRAC-SAw	
<b>Max. Height</b> (Note A)	110 ft (10 stories) max.	
<b>Min. Lot Size</b>	None	
<b>Min. Lot Width</b>		
<b>Max. FAR</b>		
<b>Density</b>	50 du/acre	
	<b>Primary Street</b>	<b>Secondary Street</b>
<b>Front &amp; Corner Yard Build-to Line</b>	0 ft max.	5 ft min. - 10 ft max.
<b>Side &amp; Rear Yard Setback</b>		
When abutting existing residential zone or use	10 ft min.	10 ft min.
All others	None	None
<b>(*) Shoulder Height</b>	25 ft (2 stories) min.	25 ft (2 stories) min.
	75 ft (6 stories) max.	75 ft (6 stories) max.
<b>(*) Front &amp; Corner Stepback</b> (Note B)	12 ft min.	15 ft min.
<b>(*) Tower Design Standards</b>	<b>Floorplate Max.</b>	<b>Side/Rear Stepback</b>
<b>Residential</b>	≤8,000 sf	20 ft min.
	8,001 sf—10,000 sf	25 ft min.
	10,001—12,000 sf	30 ft min.
<b>Non-Residential</b>	≤16,000 sf	20 ft min.
	16,001 sf—20,000 sf	25 ft min.
	20,001—32,000 sf	30 ft min.

**Note A:** Subject to Site Plan Level II permit, with City Commission approval, for heights greater than one hundred ten (110) feet, up to one hundred fifty (150) feet, and proposed tower(s) cannot exceed the following standards:

<b>Max. Floorplate:</b>	<b>Min. Tower Separation:</b>
Commercial 20,000 sf	25 ft side and rear setback
Residential 10,000 sf	25 ft side and rear setback

**Note B:** Any portion of a structure over 7-stories (75-feet in height) shall meet the minimum step back requirements. Structures located on Andrews Avenue or at the corner of Andrews Avenue and any other Street are exempt from the step back requirements along those street frontages.

(\*) May be modified if alternative design is found to achieve the underlying intent of the design standard as provided in the SRAC-SA Design Standards

**Sec. 47-31.31. Table of Dimensional Requirements for the NWRAC-MU District**

REQUIREMENTS (Note A)	NWRAC-MUe and NWRAC-MUe	NWRAC-MUw
<b>Max. Height</b> (Note B)	65 ft 110 ft (10 Stories) max	45 ft 65 ft (5 Stories) max
<b>Min Lot Size</b>	None	
<b>Min. Lot Width</b>		
<b>Max FAR</b>		
<b>Density</b>	None	
<b>Yard Requirements (Note B &amp; C)</b>	(*) 0 ft (Primary Street) 5 ft (Secondary Street & NW 7 <sup>th</sup> Ave)	
<b>When abutting residential</b>	15 ft	15 ft
<b>(**) Min. Shoulder Height</b>	25 ft (2 Stories) min	
<b>(**) Max. Shoulder Height</b>	65 ft (5 Stories) max	
<b>When abutting residential</b>	45 ft max	
<b>(**) Tower Stepback</b>		
<b>(**) Tower Floorplate/Separation</b>	Floorplate Max	Side/Rear Stepback Min
<b>Non-Residential</b>	≤16,000 sf	20 ft min
	16,001 – 20,000 sf	25 ft min
	20,001 – 32,000 sf	30 ft min
<b>Residential</b>	≤ 8,000 sf	20 ft min
	8,001 – 10,000 sf	25 ft min
	10,001 – 12,000 sf	30 ft min
	10,001 – 12,000 sf	30 ft min

**Note A:** Dimensional requirements are subject to the intent of the NWRAC-MU Illustration of Design Standards. In the event of a conflict the NWRAC-MU Illustration of Design Standards shall take precedence.

**Note B:** Subject to Site Plan Level II permit, with City Commission approval, for heights greater than sixty-five (65) feet in the NWRAC-MUe and NWRAC-MUe up to one hundred ten (110) feet and heights greater than forty-five (45) feet in the NWRAC-MUw up to sixty-five (65) feet subject to towers meeting all other dimensional requirements.

**Note C:** A side yard is not required unless abutting residential property

(\*) Yard setback is based on the adopted street cross-sections and shall correspond to those as indicated in the NW-RAC Illustrations of Design Standards

(\*\*) May be modified if alternative design is found to achieve the underlying intent of the standards as provided in the NWRAC-MU Illustration of Design Standards

**Sec. 47-13.31. – SRAC special regulations applicable to all SRAC zoning districts.**

A. ~~Applicability. The following regulations shall apply to development permitted within a SRAC zoning district as shown on the List of Permitted and Conditional Uses – Section 47-13.10~~

~~B. *Density.* Density within the entire SRAC is limited in accordance with the number of units as provided in the adopted Comprehensive Plan.~~

- ~~1. Density within the entire SRAC may be increased as provided in the Comprehensive Plan.~~
- ~~2. Dwelling units are allocated at the time of development plan approval. Upon expiration of a development plan the allocation of dwelling units shall terminate and such units shall be made available for future development.~~
- ~~3. The allocation of dwelling units shall be subject to all provisions of the ULDR applicable at the time of development permit approval when the dwelling units are allocated and subject to any conditions imposed by Broward County on the approval of the land use plan amendment that permits additional dwelling units including but not limited to conditions requiring affordable housing, student station fees or any other fees required to be collected as a condition of the allocation of dwelling units.~~
- ~~4. The dwelling units that are allocated to the SRAC as provided in the City's Comprehensive Plan are available in accordance with the ULDR applicable at the time a development plan application is submitted on a first come, first serve basis.~~
- ~~5. Dwelling units shall be allocated in the SRAC land use district in accordance with regulations adopted by City for the dwelling units allocated and no development shall be permitted unless and until the City adopts a zoning regulation addressing the allocation of the dwelling units identified in the City's Plan.~~

~~C. *Streetscape Regulations and Classification.*~~

- ~~1. The purpose of the streetscape regulations is to create a safe, comfortable and visually interesting experience for the pedestrian, thereby encouraging more street level activity by creating a well landscaped street corridor defined by a consistent streetwall.~~

~~The regulations are intended to accomplish streetscape goals by requiring or encouraging the following:~~

- ~~a. Delineation of a streetwall through the limitation of space between buildings along the street.~~
- ~~b. Enhancing pedestrian and vehicular safety through traffic calming measures and regulation of sidewalk width and quality as well as vehicular crossings and the location of off-site parking;~~
- ~~c. Providing sufficient light and air through setback regulations, while providing weather protection from rain and sunshine through the use of natural shade trees, canopies and awnings;~~

- d. ~~Sidewalks that are safe to travel by regulating the width and quality of sidewalk and vehicular crossings, and the location of off-site parking;~~
  - e. ~~The provision of an interesting experience for pedestrian activity by locating non-residential, active uses on the first floor, principal building access to be oriented toward the street, requiring transparent glazing, architectural features or both on the first floor fronting of a building to front toward the street, and encouraging consolidated open areas along the street front along with street furniture.~~
  - f. ~~Permitting a mix of housing, retail and business uses that will create an active urban environment.~~
  - g. ~~Light and view to those occupying a building above the 6th floor or 75 feet by requiring stepbacks at this height.~~
2. ~~*Street Classifications.* In the SRAC all streets are classified as primary or secondary. This classification is made according to various functional characteristics of the street such as width, traffic volume, and suitability for human scale, pedestrian friendly street life. The form of development that occurs on any given street is in part determined by the street classification. The regulations for development arising from street classifications encourage development of both sides of the street in a consistent manner.~~
- a. ~~*Primary Streets:* Primary streets are characterized by active commercial and retail frontage at the ground floor, taller and more intensive buildings fronting the street, and a consistent streetwall. Primary Streets typically feature a full complement of pedestrian amenities, including wide sidewalks, on street parking, and a well developed streetscape, which may include open space for public use. Primary Streets are the principal urban streets and are intended to be well used by vehicles and pedestrians and to be the primary transit routes. In the SRAC, the Primary streets are:
    - i. ~~South Andrew's Avenue.~~
    - ii. ~~Davie Boulevard.~~
    - iii. ~~South East 17th Street.~~~~
  - b. ~~*Secondary Streets:* Secondary streets are more residential in nature, and have smaller scale non-residential uses transitioning between the more urban areas and the existing residential and commercial neighborhoods. Secondary streets offer a combination of a mix of uses, but at less intensity and with less vehicular traffic while maintaining a pedestrian friendly environment. Secondary streets are streets other than Primary Streets listed in subsection (a) above.~~
3. ~~The SRAC establishes development provisions intended to reinforce the qualities described for primary and secondary streets. For each street type, the right of-~~

~~way width and particular street section may vary depending on available space and other existing constraints.~~

**Secs. 47-13.32 – 47-13.42. Reserved**

**47-13.43. General Regulations**

A. The following regulations shall apply to all development permitted within the zoning districts as indicated herein and as specified in Section 47-13.10, List of Permitted and Conditional Uses:

1. South Regional Activity Center-South Andrews east (SRAC-S Ae)
2. South Regional Activity Center-South Andrews west (SRAC-S Aw)
3. Northwest Regional Activity Center-Mixed Use (NWRAC-MU)

B. Definitions

1. *Floorplate*: The gross square footage (GSF) for any floor of a tower. This does not include balconies that are open on three sides.
2. *NWRAC-MU Design Standards*: NWRAC-MU Illustrations of Design Standards as part of the creation of the NWRAC zoning districts adopted as part of this ordinance on mm/dd/yyyy incorporated as if fully set out herein and on file with the Department.
3. *Pedestal*: The portion of a building extending from the ground to the shoulder.
4. *Shoulder*: The portion of a building below the horizontal setback between a tower and a pedestal.
5. *SRAC-SA*: The overall area comprised of both the SRAC-S Aw and SRAC-S Ae zoning districts.
6. *SRAC-SA Design Standards*: SRAC-SA Illustrations of Design Standards as part of the creation of the SRAC-SA zoning districts adopted as part of this ordinance on January 4, 2011, incorporated as if fully set out herein and on file with the Department.
7. *Setback*: The horizontal dimension that defines the distance between the face of the tower and the face of the pedestal.
8. *Streetscape*: Exterior public space beginning at the face of a building extending into the adjacent right-of-way, which includes travel lanes for vehicles and bicycles, parking lanes for cars, and sidewalks or paths for pedestrians. Streetscape may also include, but not be limited to, landscaped medians and plantings, street trees, benches, and streetlights as well as fences, yards, porches, and awnings.

9. Streetwall: The building façade adjacent to the street, along or parallel to the lot-line.
10. Story: The complete horizontal section of a building, having one continuous or practically continuous floor.
11. Tower: The portion of a building extending upward from the pedestal.

C. Density. Density within the applicable zoning districts is limited in accordance with the number of units as provided in the adopted Comprehensive Plan.

1. Density may be increased as provided in the Comprehensive Plan.
2. Dwelling units are allocated at the time of development plan approval. Upon expiration of a development plan the allocation of dwelling units shall terminate and such units shall be made available for future development.
3. The allocation of dwelling units shall be subject to all provisions of the ULDR applicable at the time of development permit approval when the dwelling units are allocated.
4. The dwelling units allocated as provided in the City's Comprehensive Plan are available in accordance with the ULDR applicable at the time a development plan application is submitted on a first come, first serve basis.
5. Dwelling units shall be allocated in accordance with regulations adopted by City for the dwelling units allocated and no development shall be permitted unless and until the City adopts a zoning regulation addressing the allocation of the dwelling units identified in the City's Plan.

D. Streetscape Regulations and Classification.

2. The purpose of the streetscape regulations is to create a safe, comfortable and visually interesting experience for the pedestrian, thereby encouraging more street level activity by creating a well landscaped street corridor defined by a consistent streetwall.

The regulations are intended to accomplish streetscape goals by requiring or encouraging the following:

- a. Delineation of a streetwall through the limitation of space between buildings along the street.
- b. Enhancing pedestrian and vehicular safety through traffic calming measures and regulation of sidewalk width and quality as well as vehicular crossings and the location of off-site parking;

- c. Providing sufficient light and air through stepback regulations, while providing weather protection from rain and sunshine through the use of natural shade trees, canopies and awnings;
- d. Sidewalks that are safe to travel by regulating the width and quality of sidewalk and vehicular crossings, and the location of off-site parking;
- e. The provision of an interesting experience for pedestrian activity by locating non-residential, active uses on the first floor, principal building access to be oriented toward the street, requiring transparent glazing, architectural features or both on the first floor of a building fronting toward the street, and encouraging consolidated open areas along the street front along with street furniture.
- f. Permitting a mix of housing, retail and business uses that will create an active urban environment.
- g. Light and view to those occupying a building above a specified height by requiring stepbacks at this height.

**Secs. 47-13.44 – 47-13.50. Reserved.**

**Sec. 47-13.4651. - SRAC-SA special regulations.**

A. *Applicability.* In addition to the regulations provided for in Section 47-13.43, General Regulations ~~development in SRAC zoning districts~~, the following additional regulations shall apply to all development permitted within the SRAC-SAw and SRAC-SAe zoning districts as shown on the List of Permitted and Conditional Uses, Section 47-13.10.6. As used herein, the SRAC-SA Design Standards shall refer to the SRAC-SA: Illustrations of Design Standards on file with the Department and incorporated herein as if fully set out in those sections of the ULDR that refer to the SRAC-SA Design Standards.

1. *Street Classifications.* All streets are classified as primary or secondary. This classification is made according to various functional characteristics of the street such as width, traffic volume, and suitability for human-scale, pedestrian-friendly street life. The form of development that occurs on any given street is in part determined by the street classification. The regulations for development arising from street classifications encourage development of both sides of the street in a consistent manner. The SRAC-SA Design Standards establishes development provisions intended to reinforce the qualities described for primary and secondary streets. For each street type, the right-of-way width and particular street section may vary depending on available space and other existing constraints.

a. *Primary Streets:* Primary streets are characterized by active commercial and retail frontage at the ground floor, taller and more intensive buildings fronting the street, and a consistent streetwall. Primary Streets typically feature a full complement of pedestrian amenities, including wide sidewalks, on street parking, and a well-developed streetscape, which

may include open space for public use. Primary Streets are the principal urban streets and are intended to be well used by vehicles and pedestrians and to be the primary transit routes. In the SRAC, the Primary streets are:

- i. South Andrews Avenue.
- ii. Davie Boulevard.
- iii. South East 17th Street.

b. *Secondary Streets:* Secondary streets are more residential in nature, and have smaller scale non-residential uses transitioning between the more urban areas and the existing residential and commercial neighborhoods. Secondary streets offer a combination of a mix of uses, but at less intensity and with less vehicular traffic while maintaining a pedestrian friendly environment. Secondary streets are streets other than Primary Streets listed in subsection (a) above.

~~B. *Definitions.* For the purpose of sections 47-13.30 through 47-13.47, the following terms are defined as follows:~~

- ~~1. *Floorplate:* The gross square footage (GSF) for any floor of a tower. Does not include balconies that are open on three sides.~~
- ~~2. *Pedestal:* The portion of a building extending from the ground to the shoulder.~~
- ~~3. *Shoulder:* The portion of a building below the horizontal setback between a tower and a pedestal.~~
- ~~4. *SRAC-SA:* The overall area comprised of both the SRAC SAw and SRAC SAe zoning districts.~~
- ~~5. *SRAC-SA Design Standards:* The Illustrations of Design Standards as part of the creation of the SRAC-SA zoning districts adopted as part of this ordinance on January 4, 2011 and incorporated as if fully set out herein.~~
- ~~6. *Setback:* The horizontal dimension that defines the distance between the face of the tower and the face of the pedestal.~~
- ~~7. *Streetscape:* Exterior public space beginning at the face of a building extending into the adjacent right of way, which includes travel lanes for vehicles and bicycles, parking lanes for cars, and sidewalks or paths for pedestrians. Streetscape may also include, but not be limited to, landscaped medians and plantings, street trees, benches, and streetlights as well as fences, yards, porches, and awnings.~~
- ~~8. *Streetwall:* The building façade adjacent to the street, along or parallel to the lot line.~~
- ~~9. *Story:* The complete horizontal section of a building, having one continuous or practically continuous floor.~~

~~10. Tower: The portion of a building extending upward from the pedestal.~~

~~C. All dimensional requirements shall be as follows provided in Section 47-13.30~~

~~D. A development shall be required to meet all other standards provided in the SRAC-SA Plan including but not limited to the following:~~

~~Building orientation,  
Architectural requirements,  
Open Space,  
Vehicular and pedestrian access,  
Building materials,  
Active ground floor uses,  
Façade.~~

~~E. Parking Facilities:~~

~~1. Off street parking regulations are as provided in Section 47-20, Parking and Loading Requirements, except as provided herein:~~

~~a. SRAC SAe and SRAC SAw district parking and loading requirements are as provided in Table 3, Section 47-20, Parking and Loading Requirements. For residential uses, the general parking requirement shall apply. For non-residential uses, the parking and loading requirements for Transition Mixed Use Districts for the Downtown RAC shall apply.~~

~~2. Parking garage. The minimum design standards for a parking garage are:~~

~~a. Sloped garage ramps facing public right-of ways shall have ornamental grating or other architectural features which screen the sloped ramp from view of the right-of way.~~

~~b. Parked vehicles shall be screened from view from abutting public rights-of way, excluding alleys. Screening may be provided by intervening buildings, architectural detailing such as ornamental grating, or landscaping.~~

~~c. Pedestrian walkways shall be provided between a parking garage and any principal or accessory building it serves and to abutting public rights-of ways and public spaces.~~

~~d. When a parking garage is provided for a principal structure on the same plot, the design of the parking garage shall complement and contain architectural features consistent with the principal structure.~~

~~F. Landscaping. Development within the SRAC SAe and SRAC SAw districts shall meet the following landscape requirements:~~

- ~~1. *VUA landscaping:* Surface parking lots within the SRAC SAe and SRAC SAw districts shall meet the landscape requirements for vehicular use areas as provided in Section 47-21, Landscaping and Tree Preservation Requirements.~~
- ~~2. *Location of Street Trees.* The requirements for street trees, as provided herein, may be located within the public rights-of-way, as provided by the entity with jurisdiction over the abutting right-of-way.~~
- ~~3. All other landscape requirements in accordance with the Plan.~~

~~G. *Signage.*~~

- ~~1. Development shall be required to meet the signage requirement applicable in the Community Business (CB) zoning district as provided in Section 47-22, Sign Requirements.~~

~~H. *Streetscape.*~~

- ~~1. Streetscape improvements as described in the SRAC SA Plan are required to be made as a part of a development within the SRAC SA districts. The required streetscape improvements shall be required to be made to that portion of the right-of-way abutting the proposed development site. If a development is located on two Primary Streets or a Primary Street and a Secondary Street, street improvements shall be required to be made to both rights-of-way. These streetscape improvements may include but are not limited to the following:~~

- ~~a. Street Trees.~~
- ~~b. Sidewalk.~~
- ~~c. Parking.~~
- ~~d. Medians.~~
- ~~e. Curb and gutter.~~
- ~~f. Landscaping.~~
- ~~g. Street furniture.~~
- ~~h. Transit Stop.~~
- ~~i. Traffic control devices.~~

- ~~2. Each applicant shall be responsible for making the streetscape improvements in accordance with the SRAC SA Design Standards applicable to the abutting right-of-way based on the right-of-way's width and the median as described in the Plan.~~

~~If a right-of-way is not under City of Fort Lauderdale jurisdiction and the authority with jurisdiction will not permit the improvement, or if, as determined by the City Manager, the streetscape improvement cannot reasonably be made at the time the development is constructed, the department shall estimate the cost of the streetscape improvement and the sum shall be paid by the applicant to the City to be held and earmarked for such streetscape improvement to be made in the future. If the streetscape improvement is unable to be made within 5 years of~~

~~development approval, the sum shall be refunded to the applicant including interest accrued at a rate accrued on similar City funds.~~

- ~~3. Modification to the required streetscape improvements may be permitted based on the preservation of natural barriers, avoidance of interference with overhead lines or other obstructions as approved by the City's Landscape Planner or may be modified based on an alternative design found to achieve the underlying intent of the design standard as provided in the SRAC-SA Design Standards.~~
- ~~4. Applicant shall be required to execute maintenance agreement providing for the repair, replacement and maintenance of required off-site improvements in form approved by the City Engineer, to be recorded in the public records of Broward County at applicant's expense. The City Engineer is authorized to execute said agreement on behalf of City.~~

~~I. Accessory structures.~~

- ~~1. Fencing. Chain-link fencing shall not be permitted abutting any Primary or Secondary street. In all other areas of the RAC, all chain-link fencing shall be black vinyl coated. Temporary fencing may be permitted pursuant to Section 47-19.5.B.~~

**Sec. 47-13.52. - NWRAC-MU special regulations.**

~~A. Applicability. In addition to the provided for in Section 47-13.43 General Regulations, the following additional regulations shall apply to all development permitted within the NWRAC-MU zoning districts as shown on the List of Permitted and Conditional Uses, Section 47-13.10.6. As used herein, the NWRAC-MU Design Standards shall refer to the NW-RAC: Illustrations of Design Standards on file with the Department and incorporated herein as if fully set out in those sections of the ULDR that refer to the NWRAC-MU Design Standards.~~

~~1. Street Classifications. In the NWRAC-MU all streets are classified as primary or secondary. This classification is made according to various functional characteristics of the street such as width, traffic volume, and suitability for human-scale, pedestrian-friendly street life. The form of development that occurs on any given street is in part determined by the street classification. The regulations for development arising from street classifications encourage development of both sides of the street in a consistent manner and in character with the established residential areas of the NWRAC-MU or development that is compatible with zoning districts outside of the NWRAC-MU. The NWRAC-MU Design Standards establishes development provisions intended to reinforce the qualities described for primary and secondary streets. For each street type, the right-of-way width and particular street section may vary depending on available space and other existing constraints.~~

~~a. Primary Streets: Primary streets are characterized by an active commercial ground floor, which includes walk-up residential, commercial and retail uses with taller and more intensive buildings fronting the street~~

creating a consistent streetwall. Primary Streets typically feature a full complement of pedestrian amenities, including wide sidewalks, on street parking, and a well-developed streetscape, which may include open space for public use. Primary Streets are the principal urban streets and are intended to be well used by vehicles and pedestrians and to be the primary transit routes. In the NWRAC, the Primary streets are:

- i. 7<sup>th</sup> Avenue.
- ii. 6<sup>th</sup> Street (Sistrunk Boulevard).
- iii. Sunrise Boulevard.
- iv. Broward Boulevard.
- v. Andrews Avenue.
- vi. Progresso Drive.

b. *Secondary Streets:* Secondary streets are more residential in nature, and have smaller scale compatible non-residential uses transitioning from the higher intensity primary streets that are more urban in nature to the existing residential and commercial neighborhoods, which are lower in scale and intensity. Secondary streets offer a combination of a mix of uses, but at less intensity and with less vehicular traffic while maintaining a pedestrian friendly environment. Secondary streets are: streets other than Primary Streets listed in subsection (a) above.

2. All dimensional requirements shall be as provided in Section 47-13.32

3. Dumpsters shall be located to allow access from existing alleys and away from existing residential property to the greatest extent possible.

**Sec. 47-13.53 – 47-13.59. Reserved.**

**Sec. 47-13.4760. - Permit approval.**

A. ~~Review process. SRAC-SA zoning districts: Except as provided in Section 47-13.30 and Section 47-24, Table 1. Development Permits and Procedures, development within the following SRAC-S Ae and SRAC-S Aw zoning districts shall be reviewed as a Site Plan Level II permit. In addition to any other notice of consideration of an application for development permit in the SRAC-S Ae and SRAC-S Aw zoning districts pursuant to Section 47-27 of the ULDR, posting of a sign notice as provided in Section 47-27.4.A.3. shall be required prior to DRC review of an application for Site Plan Level II permit.~~

1. SRAC-SA zoning districts.

a. In addition to any other notice of consideration of an application for development permit in the SRAC-S Ae and SRAC-S Aw zoning districts pursuant to Section 47-27 of the ULDR, posting of a sign notice as provided in Section 47-27.4.A.3. shall be required prior to DRC review of an application for Site Plan Level II permit.

2. NWRAC-MU zoning district.

- B. *Criteria.* An application for a development permit ~~in the SRAC-SA zoning district~~ within the zoning districts specified in Section 47-13.60.A shall be reviewed for compliance with the criteria applicable to the proposed development as provided in the ULDR. The development shall also be reviewed to determine whether it is consistent with the principals and standards provided in the ~~SRAC-Plan~~ adopted design standards as indicated in Section 47-13.29. In the event compliance with the ULDR would not permit consistency with the ~~SRAC-Plan~~ adopted design standards, the ~~SRAC-Plan~~ adopted design standards shall govern.
- C. *Effect of other ULDR provisions.* Unless otherwise provided in the provisions applicable to development within the zoning districts specified in Section 47-13.60.A ~~in the SRAC-SA district~~, the provisions of the ULDR with general applicability to development within the City shall apply as requirements for the development of property ~~within the SRAC-SA district~~ and shall not be modified unless specifically provided herein.
- D. *Effective Date of Approval.* Approval of a Site Plan Level II development within the zoning districts specified in Section 47-13.60.A ~~within the SRAC-SA~~ shall not be final until thirty (30) days after preliminary DRC approval and then only if no motion is approved by the City Commission seeking to review the application pursuant to the process provided in Section 47-26.A.2 of the ULDR.
- E. *Appeal.* An appeal of a denial of an a SRAC-SA development application within the zoning districts specified in Section 47-13.60.A shall be to the Planning and Zoning Board pursuant to Section 47-26.B.

**Section 47-20.2 Table 3**

**TABLE 3. PARKING AND LOADING ZONE REQUIREMENTS – RAC AND CENTRAL BEACH DISTRICTS**

Northwest Regional Activity Center—NWRAC-MU Districts

<u>Use</u>	<u>Standard Requirements</u>	
	<u>Parking Space Requirements</u>	<u>Loading Zone Requirements</u>
<u>Residential Use</u>	See Parking requirements for uses as provided in Table 1.  Bicycle Parking – 1 per 10 dwelling units	N/A
<u>Nonresidential Use</u>	Exempt for development between 0 and 2,500 gross square feet in area. All development greater than 2,500 gross square feet shall be required to provide 60% of the parking space requirements for uses as provided in Table 1.  Bicycle Parking – 1 per 20 parking spaces provided	See Loading requirements for uses as provided in Table 2.

## Addendum – Case T14012

### SECTION 47-24. - DEVELOPMENT PERMITS AND PROCEDURES

Sec. 47-24.1. - Generally.

TABLE 1. DEVELOPMENT PERMITS AND PROCEDURES

Permit	Department	Development Review Committee	Planning & Zoning Board (Local Planning Agency)	Historic Preservation Board	City Commission	Board of Adjustment	Criteria for Review
CENTRAL BEACH AREA DISTRICTS - see Section 47-12 and other regulations provided in this Table 1.	—	—	—	—	—	—	1. Adequacy Review Sec. 47-25.2 2. Neighborhood Compatibility Review Sec. 47-25.3
SITE PLAN—LEVEL I DEPARTMENT							
1. Sidewalk cafe	DP		A		CRR/PZ		1. Adequacy Review Sec. 47-25.2 2. Outdoor Uses, Sidewalk Cafe Sec. 47-19.9;

2.	Mobile vendor	DP		A		CRR/PZ		1. Adequacy Review Sec. 47-25.2 2. Mobile Vendor, Sec. 47-18.22;
3.a	Residential—less than 5 units	DP		A		CRR/PZ		Adequacy Review Sec. 47-25.2
3.b	In SRAC-SA zoning districts Less than 5 units and equal to or less than 110 ft. in height.	DP		A		CRR/PZ		1. Adequacy Review 47-25.2 2. SRAC-SA Design Standards;
4.a	New nonresidential construction— 5,000 square feet or less	DP		A		CRR/PZ		Adequacy Review Sec. 47-25.2
4.b	In SRAC-SA zoning districts equal to or less than 5,000 square feet and equal to or less than 110 feet in height	DP		A		CRR/PZ		1. Adequacy Review 47-25.2 2. SRAC-SA Design Standards;
5.	Modification of waterway lot widths in RS-4.4 & RS-8 Districts	DP		A		CRR/PZ		1. Adequacy Review Sec. 47-25.2 2. Modificatio

							n of Lot Width, Sec. 47-23.10;
6.	Change of use— different operation but does not involve development which requires a Site Plan Level II or higher permit— See Sec. 47-3.5.B.a	DP		A		CRR/PZ or Dept.	Nonconforming Uses, Section 47-3
7.	Reuse of nonconforming structure	DP		A		CRR/PZ or Dept.	1. Adequacy Review Sec. 47-25.2, and 2. Neighborhood Compatibility Review Sec. 47-25.3 3. Nonconforming Uses, Section 47-3;
8.	Continuation of nonconforming status	DP		A		CRR/PZ or Dept.	Nonconforming Uses, Section 47-3
9.	Approval of off-site parking	DP		A		CRR/PZ or Dept.	Parking and Loading Sec. 47-20.18
10.	Construction	DP				CRR/A	Section 47-

	staging area							19.2 FF.
SITE PLAN—LEVEL II DEVELOPMENT REVIEW COMMITTEE								
11.	New nonresidential construction—greater than 5,000 sq. ft.	R	DP	A		CRR/PZ		Adequacy Review Sec. 47-25.2
12.	a . When communication towers are permitted	R	DP	A		CRR/PZ		Adequacy Review Sec. 47-25.2 & 47-18.11
	b . When communication towers are conditional	R	R	DP		CRR/A		Adequacy Review Sec. 47-25.2 & 47-18.11
13.	Nonresidential use within 100 feet of residential property	R	DP	A		CRR/PZ		1. Adequacy Review Sec. 47-25.2, and 2. Neighborhood Compatibility Review Sec. 47-25.3;
14.	Residential—5 units or more	R	DP	A		CRR/PZ		Adequacy Review Sec. 47-25.2
15.	Multifamily residential	R	DP	A		CRR/PZ		1. Adequacy

	development at a higher density than the density of any abutting existing residential property or vacant residentially zoned property that is outside of the Multifamily Residential Zoning District							Review Sec. 47-25.2, and 2. Neighborhood Compatibility Review Sec. 47-25.3;
16.	Redevelopment proposals if existing and proposed improvements together meet the criteria of site plan level II review if proposed as new development and includes one (1) or more of the following:	R	DP	A		CRR/PZ		Adequacy Review Sec. 47-25.2
	a. A modification which alters the site improvements by more than twenty-five percent (25%) of the area of the development site.							
	b. A new drive or relocation of an							

	.	existing drive giving vehicular access from a public road to the development site.							
	c	An addition which exceeds twenty-five percent (25%) of the gross floor area of the existing structure(s) on the development site.							
	d	A change in group occupancy category as defined by the Florida Building Code, Broward County Edition which increases traffic generation by more than fifty percent (50%) of the traffic generated by the existing use based on Broward County traffic generation							

	rates.							
17.	Allocation of reserve units (maximum of 2 dwelling units)	R	DP	A		CRR/PZ		1. Adequacy Review Sec. 47-25.2, and 2. Neighborhood Compatibility Review Sec. 47-25.3, and 3. Flexibility Rules, Section 47-28;
18.	Change in use— See Sec. 47-3.5.B.b - Site Plan Level II threshold is met.	R	DP	A		CRR/PZ or DRC		1. Adequacy Review Sec. 47-25.2, and 2. Neighborhood Compatibility Review Sec. 47-25.3 3. Nonconforming Use, Section 47-3;
19.	For any use in the Downtown RAC which is within 100 feet of residential property outside	R	DP	A		CRR/PZ		1. Adequacy Review Sec. 47-25.2, and 2. Neighborhood

	of the RAC, or within the RAC-TMU(EMU,SMU,WMU) except on the New River waterfront as provided in 32, or on the New River waterfront corridor within RAC-CC and RAC-AS as provided in 33, below							od Compatibilit y Review Sec. 47-25.3 3. Nonconfor ming Use, Section 47- 3;
20.	All development within the RAC-TMU (EMU, SMU, WMU), except as otherwise provided in 20 and 32, below	R	DP	A		CRR/PZ		1. Adequacy Review Sec. 47-25.2, and 2. Neighborhood Compatibilit y Review Sec. 47-25.3 3. RAC Requiremen t, Section 47-13;
21.	All development within the RAC-TMU (EMU, SMU, WMU) that is greater in density than 25 dwelling units per net acre	R	DP	A		CRR/PZ		1. Adequacy Review Sec. 47-25.2, and 2. Neighborhood Compatibilit y Review Sec. 47-25.3 3. RAC Requiremen

								t, Section 47-13;
22.a.	Any use within the downtown RAC which is contiguous to residential property outside of the RAC	R	DP	A		CRR/PZ		1. Adequacy Review Sec. 47-25.2, and 2. Neighborhood Compatibility Review Sec. 47-25.3 3. RAC Requirement, Section 47-13;
b.	Any Site Plan Level II development within Downtown RAC which has previously been approved by or subject of an agreement with the City Commission (See Sec. 47-13.20.M.1)	R	DP	A		CRR/PZ or DRC		1. RAC Requirement, Sec. 47-13 2. Adequacy Review, Sec. 47-25.2 3. Neighborhood Compatibility review Sec. 47-25.3 when applicable.;
c.	Any Site Plan Level II development within Downtown RAC where one or more requirements of the ULDR or City's	R	DP	A		CRR/PZ or DRC		1. RAC Requirement, Sec. 47-13 2. Adequacy Review, Sec. 47-25.2

	Comprehensive Plan misapplied or failed to apply. See Sec. 47-13.20.M.2.							3. Neighborhood Compatibility review Sec. 47-25.3 when applicable.;
d.	Allocation of flexibility units within Downtown RAC	R	DP	A		CRR/PZ or DRC		1. RAC Requirement, Sec. 47-13 2. Adequacy Review, Sec. 47-25.2 3. Neighborhood Compatibility review Sec. 47-25.3 when applicable. Flexibility Rules, Sec. 47-28
23.a	Residential development 5 units or more and nonresidential development greater than 5,000 square feet within the SRAC-SA zoning districts less than or equal to one hundred and ten (110) feet	R	DP	A		CRR		1. Adequacy Review 47-25.2 2. SRAC-SA Design Standards;

	in height.							
23.b	Residential development 5 units or more and nonresidential development greater than 5,000 square feet within the SRAC-SA zoning districts greater than one hundred ten (110) feet in height up to one hundred fifty (150) feet in height	R	R			DP		1. Adequacy Review 47-25.2 2. SRAC-SA Design Standards;
<u>24.a.</u>	<u>Residential development five (5) units or less and nonresidential development less than five thousand (5,000) square feet within the NWRAC-MUe &amp; NWRAC-MUe zoning districts less than or equal to sixty five (65) feet in height and within NWRAC-MUw zoning district less than or equal to forty five (45) feet in height.</u>	<u>DP</u>		<u>A</u>		<u>CRR/PZ</u>		
<u>24.b.</u>	<u>Residential development five</u>	<u>R</u>	<u>DP</u>	<u>A</u>		<u>CRR</u>		

	<u>(5) units or more and nonresidential development greater than five thousand (5,000) square feet within the NWRAC-MUe &amp; NWRAC-MUe zoning districts less than or equal to sixty five (65) feet in height and within NWRAC-MUw zoning district less than or equal to forty five (45) feet in height.</u>						
24.c.	<u>Development within the NWRAC-MUe &amp; NWRAC-MUe zoning districts greater than sixty five (65) feet in height and within NWRAC-MUw zoning district greater than forty five (45) in height.</u>	R	R			DP	
SITE PLAN—LEVEL III							
2425. a.	Parking reduction	R	R	DP		CRR or A	1. Adequacy Review Sec. 47-25.2
b.	Parking Reduction in Northwest-	R	DP	A		CRR/PZ or	2. Parking and Loading

	Progresso-Flagler Heights Community Redevelopment Area					DRC		Requirements, Section 47-20;
<u>2526.</u>	Modification of yards in RMM-25, RMH-25 and RMH-60 Districts	R	R	DP		CRR or A		1. Adequacy Review Sec. 47-25.2 2. See Modification of Yards, Sec. 47-23.11;
<u>2627.</u>	Any use within the CF, CF-H, CF-S, CH-HS, P, T and U districts which is greater in height, FAR, gross floor area of the maximum within the specific zoning district (except for the T district when located within an airport boundary at which time the height of any use shall be regulated by FAA standards)	R	R	DP		CRR or A		1. Adequacy Review, Sec. 47-25.2 2. Neighborhood Compatibility Review, Sec. 47-25.3;
<u>2728.</u>	Waterway uses, except for uses in the RAC-CC, RAC-UV, RAC-AS, RAC-TMU and all Central Beach	R	R	DP		CRR or A		1. Adequacy Review Sec. 47-25.2, and 2. Neighborhood

	Districts							Compatibility Review, Sec. 47-25.3, and 3. Waterway Use, Sec. 47-23.8;
<del>28</del> <u>29</u> .	Allocation of flexibility units to residential land use to allow bonus density for affordable housing on residential land use parcels or for special residential facilities	R	R	DP		CRR or A		1. Adequacy Review Sec. 47-25.2 2. Flexibility Rules, Section 47-28;
<del>29</del> <u>30</u> .	Any use within the Community Business (CB) District which is greater than 10,000 square feet in gross floor area	R	R	DP		CRR or A		1. Adequacy Review Sec. 47-25.2, and 2. Neighborhood Compatibility Review Sec. 47-25.3;
<del>30</del> <u>31</u> .	Within the RMH-60 District, a hotel with greater than 87 sleeping rooms per net acre, up to a maximum of 120 sleeping rooms	R	R	DP		CRR/A		1. Adequacy Review Sec. 47-25.2, and 2. Neighborhood Compatibility

	per net acre							y Review Sec. 47- 25.3;
<del>31</del> <u>32</u>	Density bonus in RML-25, RMM-25, RMH-25 Districts	R	R	DP		CRR/A		1. Adequacy Review Sec. 47-25.2, and 2. Neighborhood Compatibility Review Sec. 47-25.3 3. Density Bonus Requirements, Sec. 47- 23.12;
<del>32</del> <u>33</u>	Zero lot line and cluster residential development	R	R	DP		CRR/A		1. Adequacy Review Sec. 47-25.2, and 2. Neighborhood Compatibility Review Sec. 47-25.3 3. Cluster Dwellings, Sec. 47-18.9 4. Zero Lot Line Dwellings, Sec. 47- 18.38;
<del>33</del> <u>34</u>	Within the RS-4.4 and RS-8	R	R	DP		CRR/A		1. Adequacy

	Districts—for greater FAR or lot coverage than as limited by Section 47-5							Review Sec. 47-25.2, and 2. Neighborhood Compatibility Review Sec. 47-25.3;
<u>3435.</u>	Any use within the RAC-TMU (EMU, SMU, WMU) on land abutting the New River	R	R	DP		CRR/A		1. Adequacy Review Sec. 47-25.2, and 2. Neighborhood Compatibility Review Sec. 47-25.3 3. RAC Requirement, Section 47-13;
<u>3536.</u>	Any use within the RAC-CC or RAC-AS on the New River which deviates from the New River Corridor Requirements, as provided in Section 47-13, Downtown RAC districts	R	R	DP		CRR/A		1. Adequacy Review Sec. 47-25.2, and 2. Neighborhood Compatibility Review Sec. 47-25.3 3. RAC Requirement, Section [47-13];

3637.	All development within the SRAC-SA zoning districts greater than one hundred and ten (110) feet in height up to one hundred and fifty feet (150) feet in height.	R	R			DP		1. Adequacy Review 47-25.2 2. SRAC-SA Design Standards;
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Permit	Department	Development Review Committee	Planning & Zoning Board (Local Planning Agency)	Historic Preservation Board	City Commission	Board of Adjustment	Criteria for Review
EXEMPTION FROM ZONING FOR PUBLIC PURPOSE USES	R	R	R		DP		Public Purpose Use Requirements , Sec. 47-18.26
CONDITIONAL USES a. Any use listed as a conditional use within a zoning district. b. Within the RS-4.4 and RS-8 districts, any single family lot which is	R	R	DP		CRR/A		1. Adequacy Review Sec. 47-25.2, and 2. Neighborhood Compatibility Review Sec. 47-25.3, and 3. Conditional Use Permit Sec. 47-24.3 4. For Industrial

<p>twice the minimum lot size, or greater.</p> <p>c. Within the RMH-60 zoning district, any use which is greater than 150 feet in height, up to 300 feet in height.</p> <p>d. Any industrial use which is within 300 feet of residential property.</p>						<p>Uses—Section 47-7;</p>
<p>REZONING</p> <p>a. Change in zoning designation or change to text of the ULDR.</p> <p>*b. Allocation of commercial uses on residential land use parcel.</p> <p>*c. Allocation of commercial uses on industrial or</p>	R	*R	R/Approval  DP/Denial		DP/A	<ol style="list-style-type: none"> <li>1. Adequacy Review Sec. 47-25.2, and</li> <li>2. Rezoning Criteria, Sec. 47-24.4, and</li> <li>3. For Flex Commercial Acreage Section 47-28</li> <li>4. For X-Exclusive Use district Section 47-9;</li> </ol>

employment center land use parcel. *d. X-Use District.							
SUBDIVISION APPROVAL/PLAT REQUIREMENTS	R	R	R		DP		1. Adequacy Review Sec. 47-25.2, and 2. Plat/Subdivision Criteria Sec. 47-24.5;
VACATION OF RIGHTS-OF-WAY	R	R	R		DP		1. Adequacy Review Sec. 47-25.2, and 2. Vacation of ROW Requirements , Sec. 47-24.6;
VACATION OF EASEMENTS	R	R			DP		1. Adequacy Review Sec. 47-25.2, and 2. Vacation of Easements Requirements , Sec. 47-24.7;

Permit	Department	Development Review Committee	Planning & Zoning Board (Local Planning)	Historic Preservation Board	City Commission	Board of Adjustment	Criteria for Review
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			ng Agenc y)				
<p>COMPREHENSIVE PLAN AMENDMENTS</p> <p>a. Text or map amendments to the City's adopted comprehensive plan.</p> <p>b. Increase of residential density on residential land use parcel.</p> <p>c. Allocation of residential units on commercial or office park land use and employment center.</p>	R	R	R		DP		<p>1. Adequacy Review Sec. 47-24.2, and</p> <p>2. Comprehensive Plan Amendments Criteria, F.S. ch. 163 and F.A.C. Rule 9J-5;</p>
<p>CONCURRENCY FINDING OF ADEQUACY</p>	R	DP					<p>1. Adequacy Review Sec. 47-25.2</p> <p>2. Concurrency Finding of Adequacy Requirements, Sec. 47-24.9;</p>
<p>DEVELOPMENT OF REGIONAL</p>	R	R	R		DP		<p>1. Adequacy Review Sec. 47-25.2, and</p>

IMPACT (DRI)							2. Development of Regional Impact Review Criteria, F.S. ch. 380;
HISTORIC DESIGNATION (OF A DISTRICT)	R		R	R	DP		1. Adequacy Review Sec. 47-25.2, and 2. Historic Designation Requirements, Sec. 47-24.11;
HISTORIC DESIGNATION (OF A LANDMARK SITE, BUILDING OR STRUCTURE)	R			R	DP		1. Adequacy Review Sec. 47-25.2, and 2. Historic Designation Requirements, Sec. 47-24.11;
CERTIFICATE OF APPROPRIATENESS (GENERAL)	R			DP	A		Historic Designation/Certificate of Appropriateness Requirements, Sec. 47-24.11
SAILBOAT BEND HISTORIC DISTRICT— CERTIFICATE OF APPROPRIATENESS	DP			HPBR/DP	CRR/A		Sailboat Bend Historic District, Section 47-17
SAILBOAT BEND HISTORIC DISTRICT— MODIFICATION OF YARDS	R			DP	A		Sailboat Bend Historic District, Section 47-17

VARIANCE/SPECIAL EXCEPTION	R					DP	1. Adequacy Review Sec. 47-25.2, and 2. Variance/Special Exception Requirements, Sec. 47-24.12;
INTERPRETATION OF ULDR	DP					A	1. Adequacy Review Sec. 47-25.2, and 2. Interpretation of ULDR Requirements, Sec. 47-24.12;

LEGEND:	
DRC	Development Review Committee
PZ	Planning and Zoning Board
Dept.	Department
DP	Development Permit issued
R	Review and recommendation requirement
A	Appeal by applicant of a denial
CRR	City commission request for review
CRR/PZ	City commission request for review of planning and zoning board action
CRR/PZ or Dept.	City commission request for review of planning and zoning board action or of department action

CRR/PZ or DRC	City commission request for review of planning and zoning board action or of Development Review Committee action
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